



## *The County of San Diego*

# Planning Commission Hearing Report

---

<b>Date:</b>	July 9, 2025	<b>Case/File No.:</b>	Alpine Car Wash Major Use Permit; PDS2022-MUP-22-008; PDS2022-ER-22-03-001
<b>Place:</b>	County Operations Center (COC) Hearing Room 5520 Overland Avenue San Diego, CA 92123	<b>Project:</b>	Major Use Permit for a Car Wash Facility
<b>Time:</b>	9:00 am	<b>Location:</b>	1250 Tavern Road, Alpine, CA
<b>Agenda Item:</b>	#1	<b>General Plan:</b>	General Commercial
<b>Appeal Status:</b>	Appealable to the Board of Supervisors	<b>Zoning:</b>	General Commercial (C-36)
<b>Applicant/Owner:</b>	Linus Klein/Growney USA LP	<b>Community:</b>	Alpine
<b>Environmental:</b>	CEQA § 15303 Exemption	<b>APN.:</b>	403-390-27-00

---

### **A. OVERVIEW**

The purpose of this report is to provide the Planning Commission with the information necessary to consider a proposed Major Use Permit (MUP) for the Alpine Car Wash Facility (Project), conditions of approval, and environmental findings prepared in accordance with the California Environmental Quality Act (CEQA). Planning & Development Services (PDS) staff recommends approval of the MUP, with the conditions noted in the attached MUP decision (Attachment B).

A MUP (Record ID: PDS2012-3300-12-019) was approved on June 14, 2013, to construct and operate a 4,374 square foot, two-story building that included a car wash tunnel, lube and oil change area, convenience store, equipment room, carport, office and associated storage areas on this site, but was never constructed. A preliminary grading and improvement plan were approved concurrently with the MUP, and the site has since been graded. This MUP (PDS2022-MUP-22-008) would allow the construction and operation of a new 3,000 square foot express car wash facility on this site. A total of 10 parking spots with associated vacuums at each, signage, a retaining wall, and landscaping would be installed. The proposed project would be located similar to the developmental footprint of the previously approved MUP.

This report includes a staff recommendation, a Project description, and analysis and discussion.

## B. REQUESTED ACTIONS

This is a request for the Planning Commission to evaluate the Project and determine if the required findings can be made and, if so, take the following actions:

1. Adopt the Environmental Findings included in Attachment D which includes a finding that the project is exempt from the California Environmental Quality Act (CEQA).
2. Grant Major Use Permit PDS2022-MUP-22-008, make the findings, and include the requirements and conditions as set forth in the Form of Decision (Attachment B).

## C. PROJECT BACKGROUND

The project site is currently vacant. The site was previously developed with a Shell gas station and lube facility that was closed and demolished in 2003. The underground storage tank was removed under a Department of Environmental Health and Quality (DEHQ) permit in 2004.

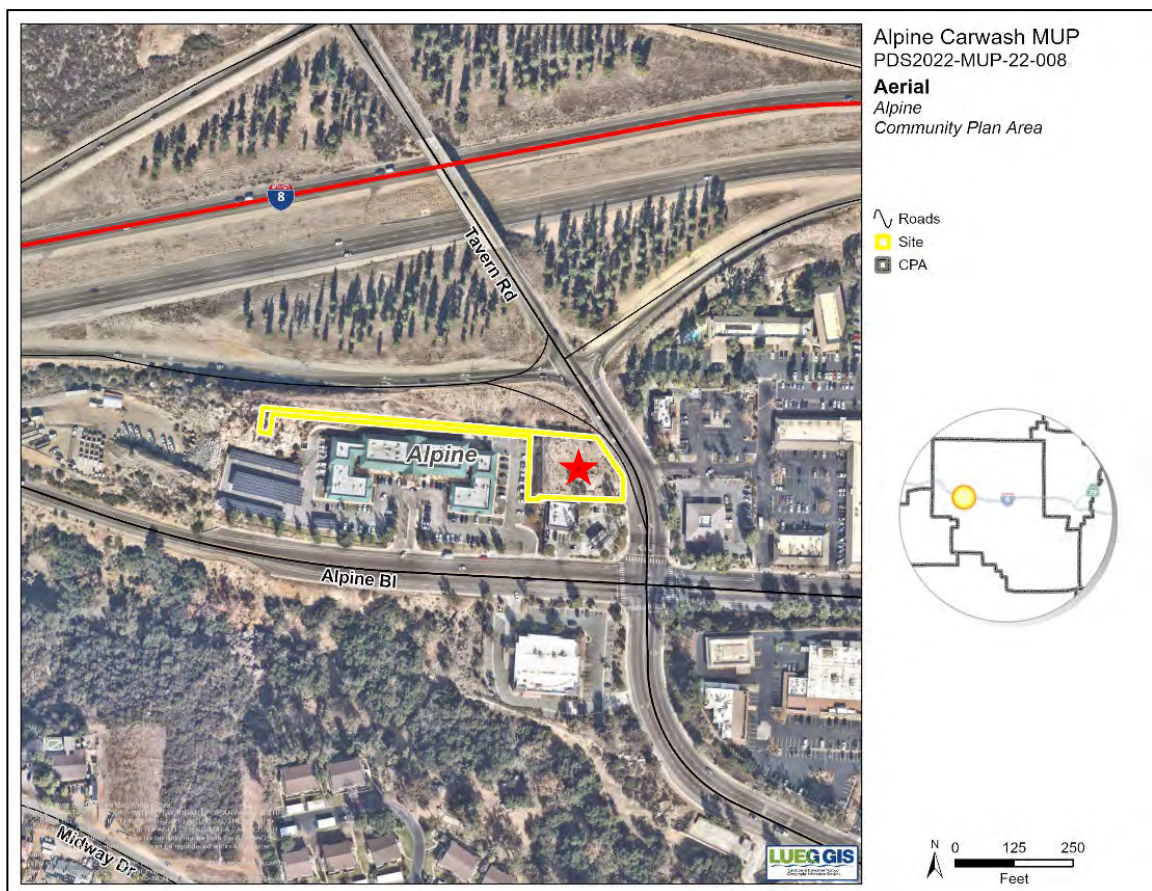


Figure 1: Aerial photograph of project site. Approx. location of proposed facility identified as red star.

On September 28, 2012, an application for a MUP (PDS2012-3300-12-019) was submitted for a car wash facility and was approved by the Planning Commission on June 14, 2013. The applicant

proposed the construction of a 4,374 square foot, two-story building which included a car wash tunnel, lube and oil change area, convenience store, equipment room, carport, office and storage. Parking would have been provided for 17 cars, and a noise attenuation wall, signage, and landscaping would have been installed. Construction never began on the project site, and the MUP Time Extension application (PDS2015-MUP-12-019TE) was formally withdrawn, following the approval of the original MUP on December 7, 2017.

This MUP (PDS2022-MUP-22-008) is for a new car wash facility that has been submitted by the current owner of the property, Linus Klein (Growney USA LP). The proposed project would constitute a similar use, with a smaller development footprint, than the previously approved MUP (PDS2012-3300-12-019).

**D. DEVELOPMENT PROPOSAL**

**1. Project Description**

The Project, submitted in June 2022, is a request for a MUP to construct and operate a 3,000-square-foot express car wash facility at 1250 Tavern Road, within the Alpine Community Plan area. The facility will operate daily from 7:00 AM to 7:00 PM, with an additional hour allocated for opening and closing, and will accommodate up to 30 cars per hour. The project will also include 10 parking spots with associated vacuums at each, signage, a retaining wall, and landscaping. The site is located within the Village General Plan Regional Category and General Commercial General Plan Land Use Designation and zoned C36 (General Commercial), which requires a MUP for the Automotive and Equipment Cleaning use under Zoning Ordinance Section 1430.

**2. Subject Property and Surrounding Land Uses**

The subject property is 0.6 acres in size and would include a car wash facility. The site is located within the General Commercial (C36) zone, along Tavern Road within the Alpine Community Plan area. The site is relatively flat and at a lower elevation than the Interstate 8 off-ramp which is adjacent to the north. The approximate elevation of the site is 1,700 feet above mean sea level. The project site was previously developed with a gas station which was removed in 2003 with the underground storage tank removed in 2004. The site was cleaned off at that time and is currently vacant.



*Figure 2: Site Photo – Looking south from Tavern Rd near Interstate 8 off-ramp towards project site.*



*Figure 3: Site Photo – Looking southeast from Interstate 8 off-ramp towards project site.*

The area surrounding the project site is characterized by commercial and freeway open space land uses. The properties immediately surrounding the project site are occupied by a gas station, pharmacy, convenience store, fast food and restaurants, gyms and a hardware store.



Table D-1: Surrounding Zoning and Land Uses

Location	General Plan	Zoning	Adjacent Streets	Description
North	Public/Semi-Public Facilities (P/SP)	Transportation and Utility Corridor (S94)	Interstate-8	Open Space and Tavern Road off-ramp
East	General Commercial (C-1)	General Commercial (C36)	Tavern Road	Shopping Center
South	General Commercial (C-1)	General Commercial (C36)	Alpine Blvd.	Gas Station
West	General Commercial (C-1)	Heavy Commercial (C37)	Alpine Blvd.	Alpine Community Commercial Center

## E. ANALYSIS AND DISCUSSION

The Project has been reviewed to ensure it conforms to all the relevant ordinances and guidelines, including, but not limited to, the San Diego County General Plan, the Zoning Ordinance, and CEQA. A discussion of the Project's consistency with applicable codes, policies, and ordinances, is described on the following pages.

### 1. Key Requirements for Requested Actions

- Is the proposed project consistent with the vision, goals, and policies of the General Plan?
- Does the project comply with the policies set forth under the Alpine Community Plan?
- Is the proposed project consistent with the County's Zoning Ordinance?
- Does the project comply with the California Environmental Quality Act (CEQA)?
- Does the project comply with the Multiple Species Conservation Program (MSCP)?

### 2. Analysis

#### Major Use Permit (MUP) Findings

The discussion below covers scale, bulk and coverage, availability of services, effects upon neighborhood character, and suitability of the site for the type of proposed use. Staff has analyzed the Project in relation to each of these. The Project is consistent with the required findings for a MUP regarding scale, bulk, and coverage, as it is proposed in a location and manner in which it would blend in with the surrounding uses of the area. Additionally, the project would be located in a similar, but smaller, development footprint as the previously approved MUP for a car wash and lube facility (PDS2012-3300-12-019). The Project area is 0.6 acres in size and surrounding commercial parcels

range in size from 0.5 to three acres. The proposed structure is 3,000 square feet representing lot coverage of ten percent. With driveways, parking and landscaping, the project is compatible with the surrounding land uses in terms of lot coverage. The project is a car wash facility that is expected to have an estimated maximum throughput of approximately 20 to 30 cars per hour and will operate between 7 a.m. and 7 p.m., with an extra hour added for opening and closing.

Service availability letters have been received from the Alpine Fire Protection District, Padre Dam Municipal Water District, and San Diego County Sanitation District demonstrating that all necessary public services and utilities are available.

The design of the project includes an articulated facade, community-consistent color scheme, a combination of stone and wood paneled siding, and landscaping that is compatible with the Alpine Design Guidelines and with other commercial structures in the vicinity. The project is located along Tavern Road (mobility element road). The project has been conditioned for Tavern Road to be re-stripped to prevent left turns into or out of the facility. Noise produced from the car wash will comply with the County's Noise Ordinance and was determined to be less than significant via provided acoustical studies. The previous use on the site was a gas station and lube/repair facility that was removed from the property in 2003. The surrounding area is comprised of similar sized parcels with a variety of commercial uses including a gas station, pharmacy, convenience store, fast food and restaurants, grocery and a hardware store.

### 3. General Plan Consistency

The proposed project is consistent with the following relevant General Plan policies, and actions as described in Table E-1.

Table E-1: General Plan Conformance

General Plan Policy	Explanation of Project Conformance
<b>POLICY LU-2.4 - Relationship of Land Uses to Community Character.</b> Ensure that the land uses and densities within any Regional Category or Land Use Designation depicted on the Land Use Map reflect the unique character and development objectives for a Community Plan Area, in addition to the General Plan Guiding Principles.	The project was reviewed and approved by the Alpine Community Planning group and the Alpine Design Review Board for conformance with the Alpine Design Guidelines and community character. The project is located within the Village regional category and the General Commercial land use designation. The Village regional category is an area where higher intensity land uses are planned or established. Surrounding land uses are commercial. The proposed project is also commercial and would fit in with the community character for the Alpine Village classification.

<p><b>POLICY LU-2.7 - Commercial Viability.</b> Ensure that new commercial centers maintain or enhance the viability of existing commercial areas.</p>	<p>The proposed car wash facility would enhance the existing commercial area by offering a service not already existing in the area.</p>
<p><b>POLICY LU-6.10 – Protection from Hazards.</b> Require that development be designed to protect property and residents from the risks of natural and man-induced hazards.</p>	<p>The project received approval from the Alpine Fire Protection District, with conditions. In addition, the water run-off from the project will be collected and discharged off-site in a similar manner as predevelopment. The minor alteration in runoff due to the proposed development of the car wash service is considered insignificant. Runoff will be collected in catch basins on the easterly edge of the site which plan to utilize a Modular Wetlands System with an associated storage pipe, as well as a proposed tree well for stormwater treatment and hydromodification.</p>
<p><b>POLICY LU-9.3 – Village and Community Core Guidelines and Regulations.</b> Support the development and implementation of design guidelines, Village-specific regulations for roads, parking, noise, and other planning and regulatory mechanisms that recognize the unique operations and character of villages, town centers, and transportation nodes. Ensure that new development be compatible with the overall scale and character of established neighborhoods.</p>	<p>The project has been reviewed and approved for consistency with the Alpine Design Guidelines for commercial development. The traffic study identifies that with the addition of 889 average daily trips (ADT), the road systems in the area would not be adversely affected by the development. Additionally, based on the noise data for the project, it has been determined that the noise generating mechanisms on site have been properly enclosed or shielded so as to conform with the performance standards for the zone.</p>
<p><b>POLICY LU-11.1 – Location and Connectivity.</b> Locate commercial, office, and industrial development in Village areas with high connectivity and accessibility from surrounding residential neighborhoods, whenever feasible.</p>	<p>The proposed project will be located at the Tavern Road off-ramp from Interstate 8, a main road serving the Alpine community that currently has a level of service (LOS) B. As such, the project is in a location that is accessible from surrounding residential neighborhoods.</p>

<p><b>POLICY LU-11.2 – Compatibility with Community Character.</b> Require that commercial, office, and industrial development be located, scaled, and designed to be compatible with the unique character of the community.</p>	<p>The project is at an elevation below that of Interstate 8. Landscaping will be included to provide screening. The project would be visible from the Interstate 8 off-ramp but not from Interstate 8. Scenic highways, corridors, vistas, or natural features would not be impacted by the construction of the project.</p>
<p><b>POLICY LU-13.1 – Adequacy of water supply.</b> Coordinate water infrastructure planning with land use planning to maintain an acceptable availability of a high quality sustainable water supply. Ensure that new development includes both indoor and outdoor water conservation measures to reduce demand.</p>	<p>The proposed project has been designed with a wastewater reclamation and recycling system for the car wash operation so as to greatly reduce the demand for and the overall water usage for the project. In addition, the plant palette that is to be used for landscaping was conditioned to meet the water conservation requirements as identified in the Water Efficient Landscape Design Manual and the Water Conservation in Landscaping Ordinance, the Parking Design Manual, the Grading Ordinance, the Alpine Community Plan, and Measure W-1.2 (Reduce Outdoor Water Usage) of the County's Climate Action Plan.</p>
<p><b>POLICY M-2.1 – Level of Service Criteria.</b> Require development projects to provide associated road improvements necessary to achieve level of service of "D" or higher on all Mobility Element roads except for those where a failing level of service has been accepted by the County pursuant to the criteria specifically identified in the accompanying text box (Criteria for Accepting a Road Classification with Level of Service E/F).</p>	<p>Tavern Road is a mobility element road with a current LOS classification of E. The LOS classification of E is acceptable with a marginal deficiency if the project addresses traffic congestion through operational improvements such as right-turn lanes. The proposed project has a projected ADT of 889 and will be conditioned for operational road improvements along the Tavern Road frontage. With the inclusion of these operational road improvements, the project will not have an adverse effect on the LOS for Tavern Road.</p>



<p><b>POLICY COS-4.1 – Water Conservation.</b> Require development to reduce the waste of potable water through the use of efficient technologies and conservation efforts that minimize the County's dependence on imported water and conserve groundwater resources.</p>	<p>The proposed project has been designed with a wastewater reclamation and recycling system for the car wash operation so as to greatly reduce the demand for and the overall water usage for the project. In addition, the plant palette that is to be used for landscaping was conditioned to meet the water conservation requirements as identified in the Water Efficient Landscape Design Manual and the Water Conservation in Landscaping Ordinance, the Parking Design Manual, the Grading Ordinance, the Alpine Community Plan, and Measure W-1.2 (Reduce Outdoor Water Usage) of the County's Climate Action Plan.</p>
<p><b>POLICY COS-11.1 – Protection of Scenic Resources.</b> Require the protection of scenic highways, corridors, regionally significant scenic vistas, and natural features, including prominent ridgelines, dominant landforms, reservoirs, and scenic landscapes.</p>	<p>The project is at an elevation below that of Interstate 8. Landscaping will be included to provide screening. The project would be visible from the Interstate 8 off-ramp but not from Interstate 8. Scenic highways, corridors, vistas, or natural features would not be impacted by the construction of the project</p>

#### 4. Community Plan Consistency

The proposed project is consistent with the following relevant Alpine Community Plan goals, policies, and actions as described in Table E-2.

<p><b>Goal 1-A: Village.</b> Preserve and enhance the Village character of Alpine defined by the current central hub of industrial, commercial, and higher density residential land use designations.</p>	<p>The project is located within the Village regional category and the General Commercial land use designation. The project is at the perimeter of the Alpine Village. The Village category is an area where higher intensity land uses are planned or established. Surrounding land uses are commercial. The proposed project is also commercial and would fit in with the community character for the Alpine Village classification.</p>
<p><b>Land Use Policy 2.1.c – Village.</b></p>	<p>The proposed project would be consistent with the Alpine Community Plan and the village</p>

Encourage projects that are in keeping with the Village character, country roads, and density of the existing village.	character concept and commercial uses. See response under Goal 1-A: Village above.
<b>Land Use Policy 2.1 – Commercial.</b> Existing and new commercial developments shall be consistent with the guidelines and standards of the Alpine Design Manual.	The project has been reviewed and approved by the Alpine Community Planning group and the Alpine Design Review Board. The proposed structure and landscaping conform to the Alpine Design Guidelines.

## 5. Zoning Ordinance Consistency

The Project complies with all applicable zoning requirements of the General Commercial (C36) zone with the incorporation of conditions of approval (See Table E-3).

*Table E-3: Zoning Ordinance Development Regulations*

<b>CURRENT ZONING REGULATIONS</b>		<b>CONSISTENT?</b>
Use Regulation:	C36	Yes – Upon issuance of a Major Use Permit
Animal Regulation:	R	N/A
Density:	7.26	N/A
Lot Size:	-	N/A
Building Type:	W	Yes
Maximum Floor Area:	-	N/A
Floor Area Ratio:	-	N/A
Height:	G (35')	Yes
Lot Coverage:	-	N/A
Setback:	O (50'-0'-25')	Yes
Open Space:	-	N/A
Special Area Regulations:	B	Yes

## 6. California Environmental Quality Act (CEQA) Completion

The project has been reviewed for compliance with the California Environmental Quality Act (CEQA) and it is recommended that the project qualifies for a Categorical Exemption under CEQA section 15303. Section 15303 includes the construction and location of limited numbers of new, small facilities or structures. Up to four commercial buildings not exceeding 10,000 square feet in floor area are allowed under this exemption on sites zoned for such use if not involving the use of significant amounts of hazardous substances where all necessary public services and facilities are available, and the surrounding area is not environmentally sensitive. The project includes one

building which has a floor area of 3,000. The car wash facility will not have significant amounts of hazardous substances on site. All public facilities are available, and the surrounding area is not environmentally sensitive.

**F. COMMUNITY PLANNING GROUP**

The Alpine Community Planning Group (CPG) recommended approval of the project on December 7, 2023 by a vote of 13-0-0-2. The Alpine Design Review Board (DRB) recommended approval of the project on May 2, 2022 by a vote of 4-0-0.

**G. PUBLIC INPUT**

The Project was submitted to PDS in November of 2022. At the time of application submittal and in accordance with Board Policy I-49, public notices were sent to property owners within a minimum radius of 600 feet of the project site until at least 20 different property owners were noticed. No written comments or phone calls were received from community members as a result of the public notices sent at the time of application. In addition, public notices for the Planning Commission hearing were sent to a total of one hundred property owners within a radius of 1,950 feet from the project site.

**H. RECOMMENDATIONS**

Staff recommends that the Planning Commission take the following actions:

1. Adopt the Environmental Findings included in Attachment D, which includes a finding that the project is exempt from CEQA.
2. Grant Major Use Permit, PDS2022-MUP-22-008, make the findings, and impose the requirements and conditions as set forth in the Form of Decision in Attachment B.

---

***Report Prepared By:***

Aidan Pulley, Project Manager  
619-972-8234  
Aidan.Pulley@sdcounty.ca.gov

---

***Report Approved By:***

Mark Slovick, Deputy Director  
858-495-5172  
Mark.Slovick@sdcounty.ca.gov

---

**AUTHORIZED REPRESENTATIVE:**

*Mark Slovick*

---

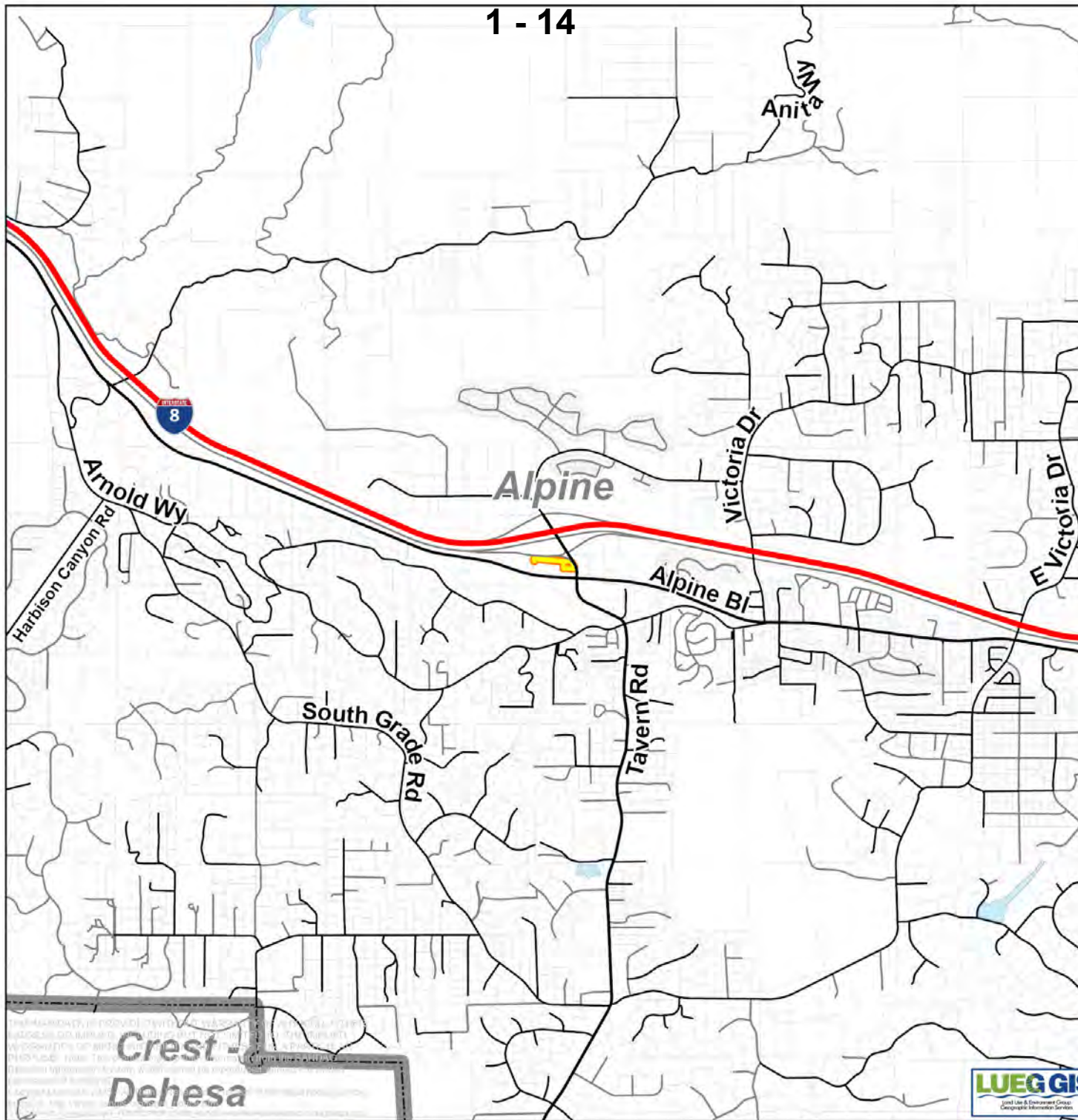
MARK SLOVICK, DEPUTY DIRECTOR

**ATTACHMENTS**

Attachment A – Planning Documentation  
Attachment B – Form of Decision Approving PDS2022-MUP-22-008  
Attachment C – Environmental Documentation  
Attachment D – Environmental Findings  
Attachment E – Public Documentation  
Attachment F – Service Availability Forms  
Attachment G – Ownership Disclosure

## **Attachment A – Planning Documentation**







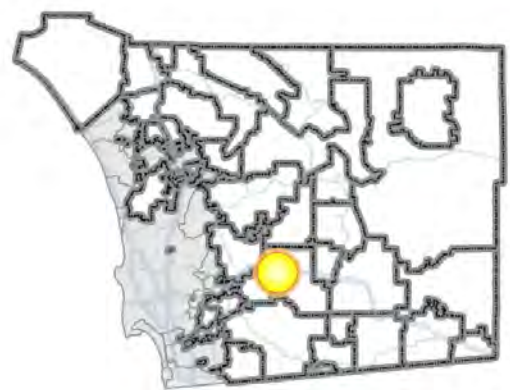
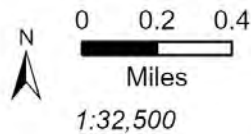


Alpine Carwash MUP  
PDS2022-MUP-22-008

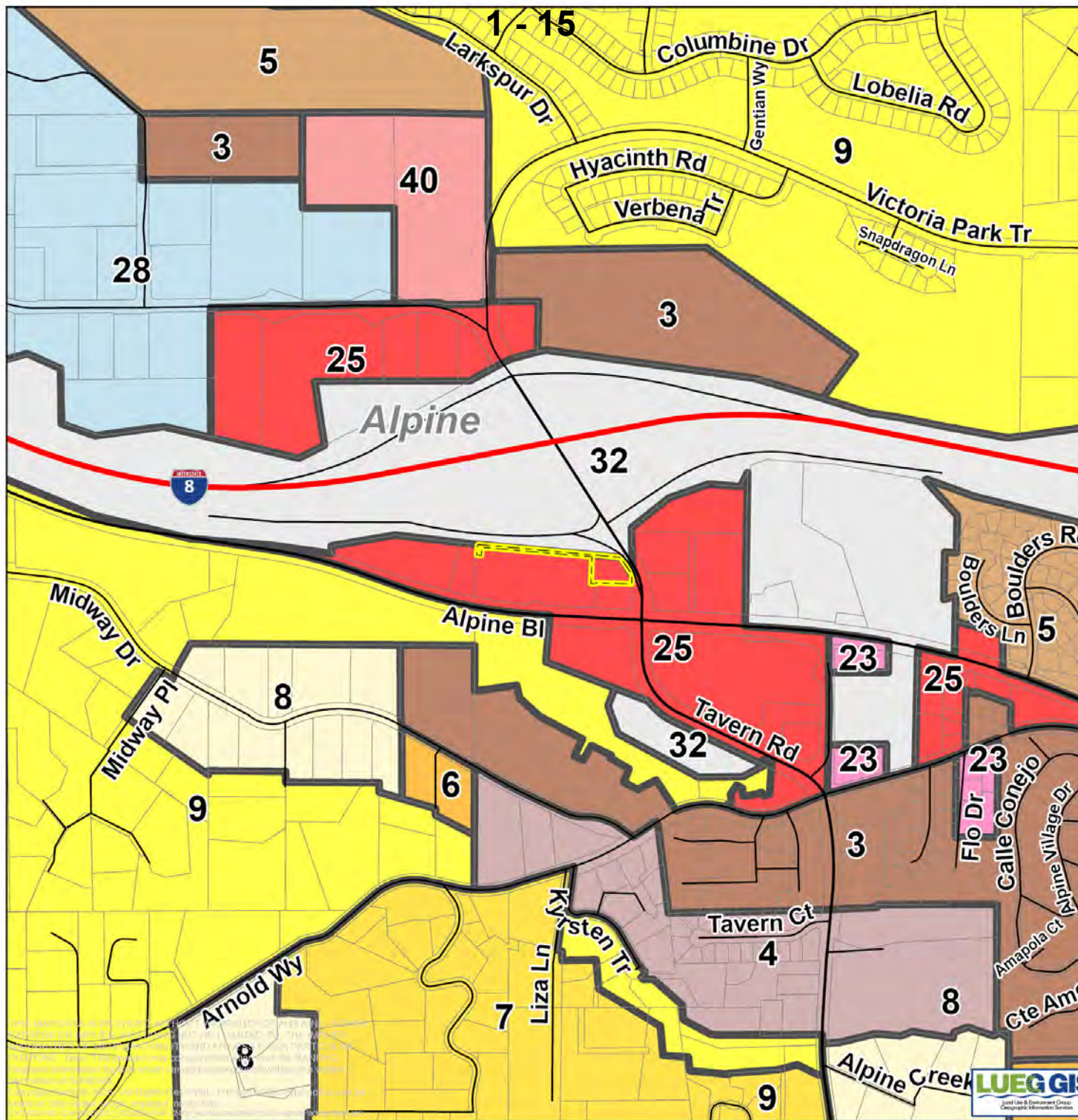
### Vicinity

Alpine  
Community Plan Area

-  Roads
-  Site
-  Parcels
-  CPA







Alpine Carwash MUP  
PDS2022-MUP-22-008

## General Plan

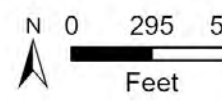
### Alpine

### Community Plan Area

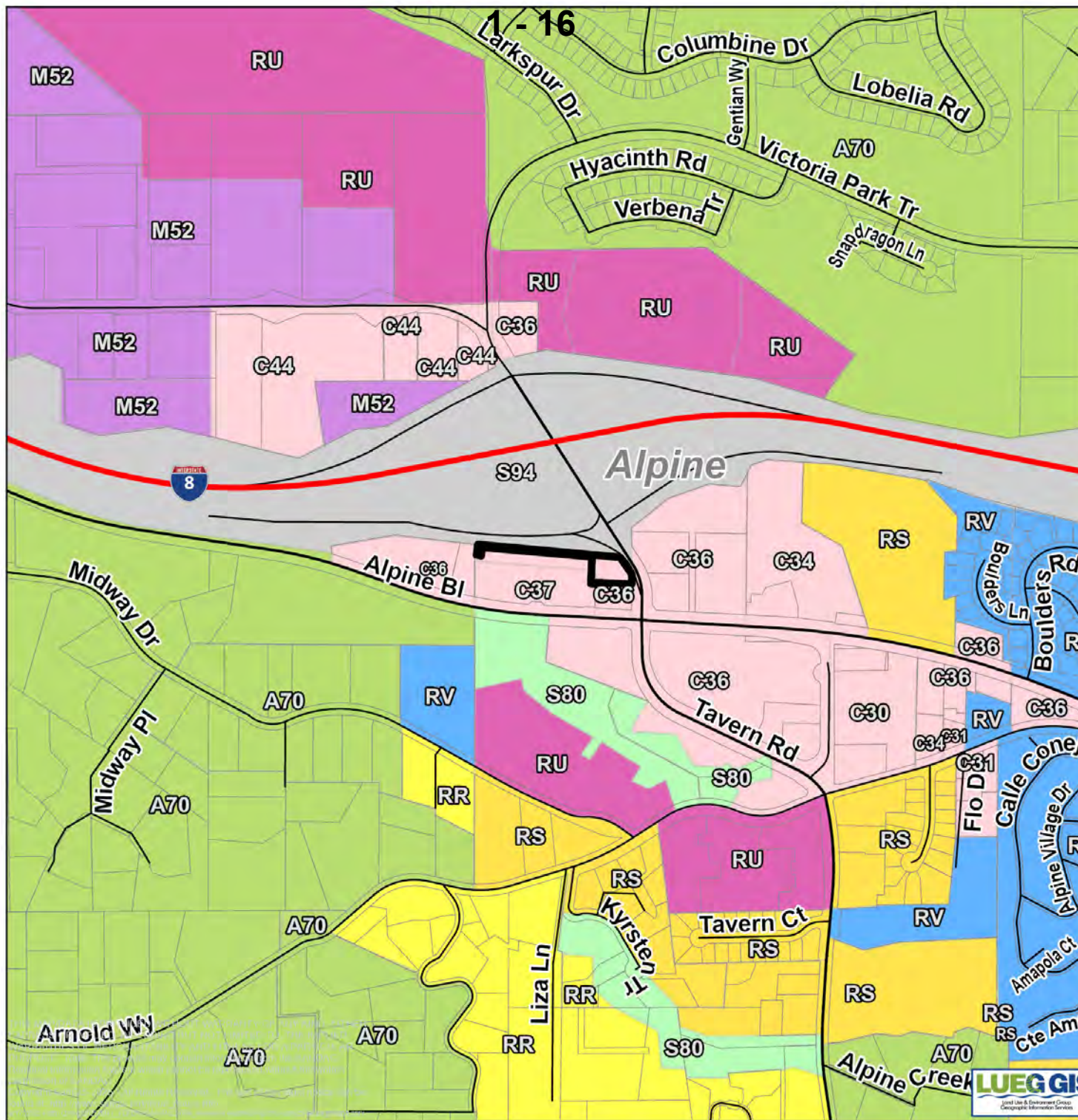
- Roads
- Site
- Parcels
- CPA

- General Plan
- (3) Village Residential (VR-15)
  - (4) Village Residential (VR-10.9)
  - (5) Village Residential (VR-7.3)
  - (6) Village Residential (VR-4.3)
  - (7) Village Residential (VR-2.9)
  - (8) Village Residential (VR-2)

- (9) Semi-Rural Residential (SR-1)
- (23) Office Professional
- (25) General Commercial
- (28) Limited Impact Industrial
- (32) Public/Semi-Public Facilities
- (40) Village Residential (VR-2)







Alpine Carwash MUP  
PDS2022-MUP-22-008

## Zoning

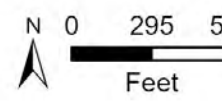
Alpine  
Community Plan Area

- Roads
- Site
- Parcels
- CPA

### Zoning

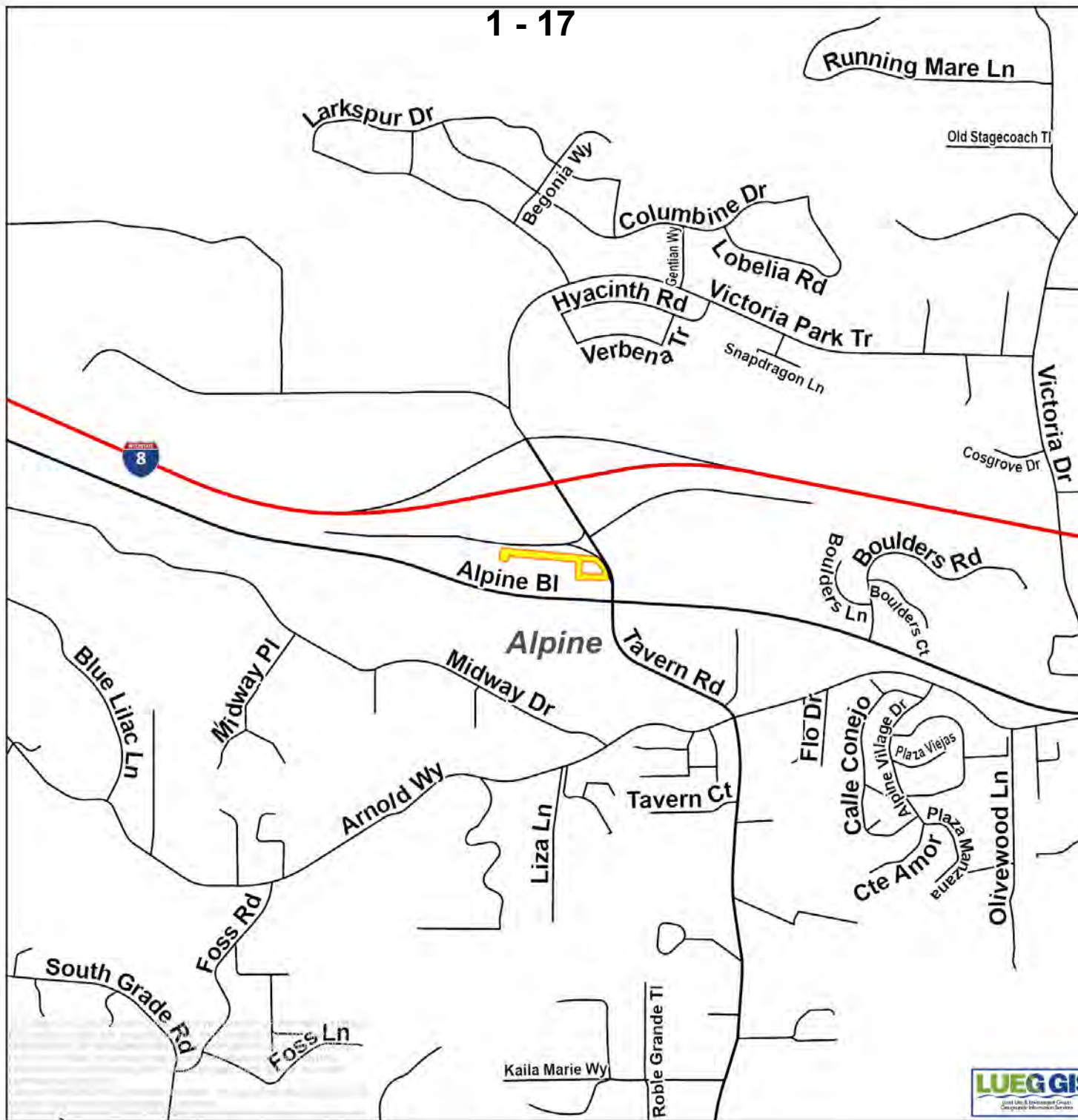
- (A) Agriculture
- (C##) Commercial and Office
- (M##) Industrial
- (S80) Open Space

- (RS) Residential - Single
- (RU) Residential - Urban
- (RV) Residential - Variable
- (RR) Rural Residential
- (S##) Transportation and Utility



**LUEGG**  
Land Use & Environment Group  
Geographic Information Services








Alpine Carwash MUP  
PDS2022-MUP-22-008

### Site Location

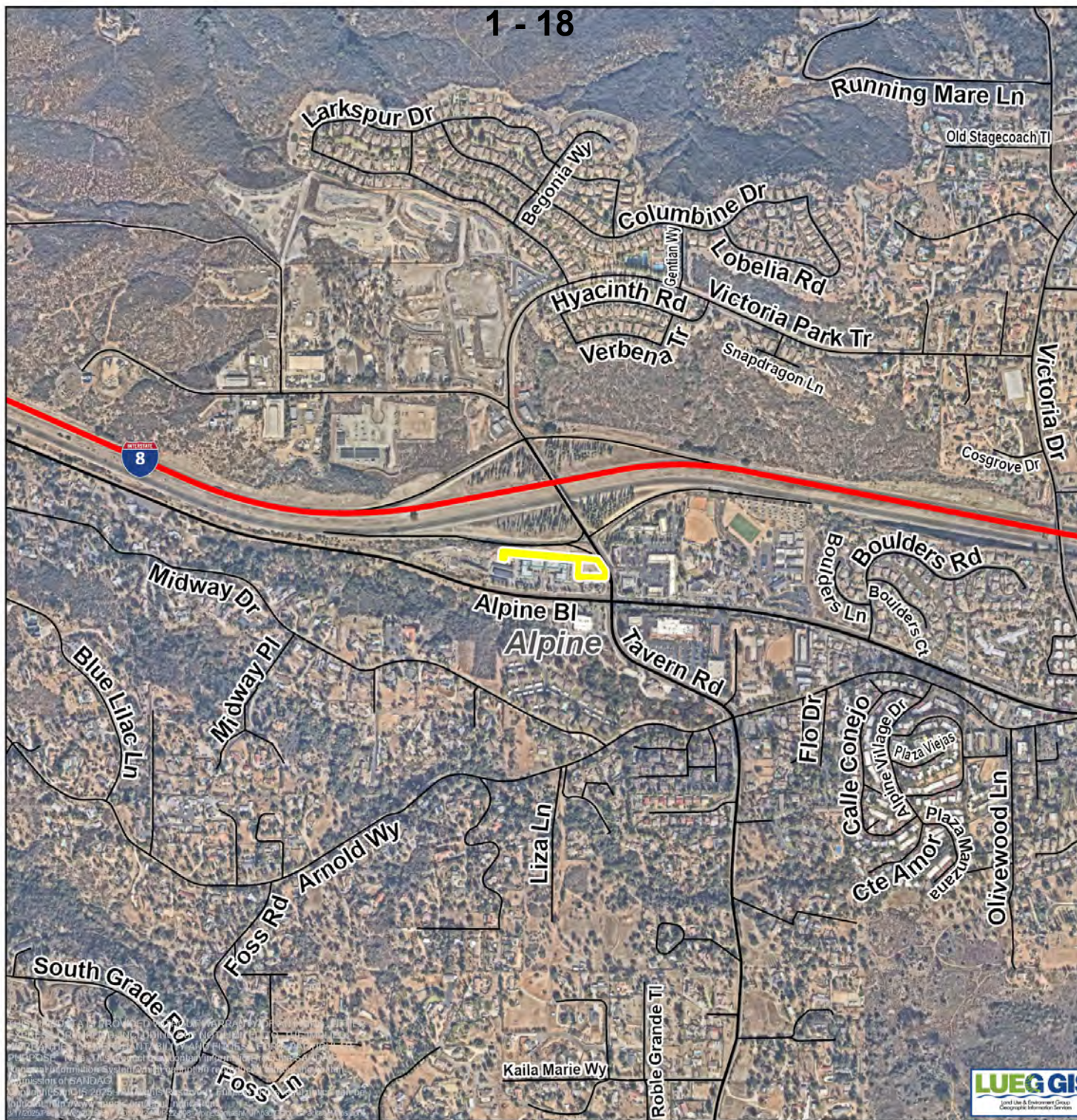
Alpine  
Community Plan Area

-  Roads
-  Site
-  CPA



0 500 1,000  
Feet







Alpine Carwash MUP  
PDS2022-MUP-22-008

**Aerial**

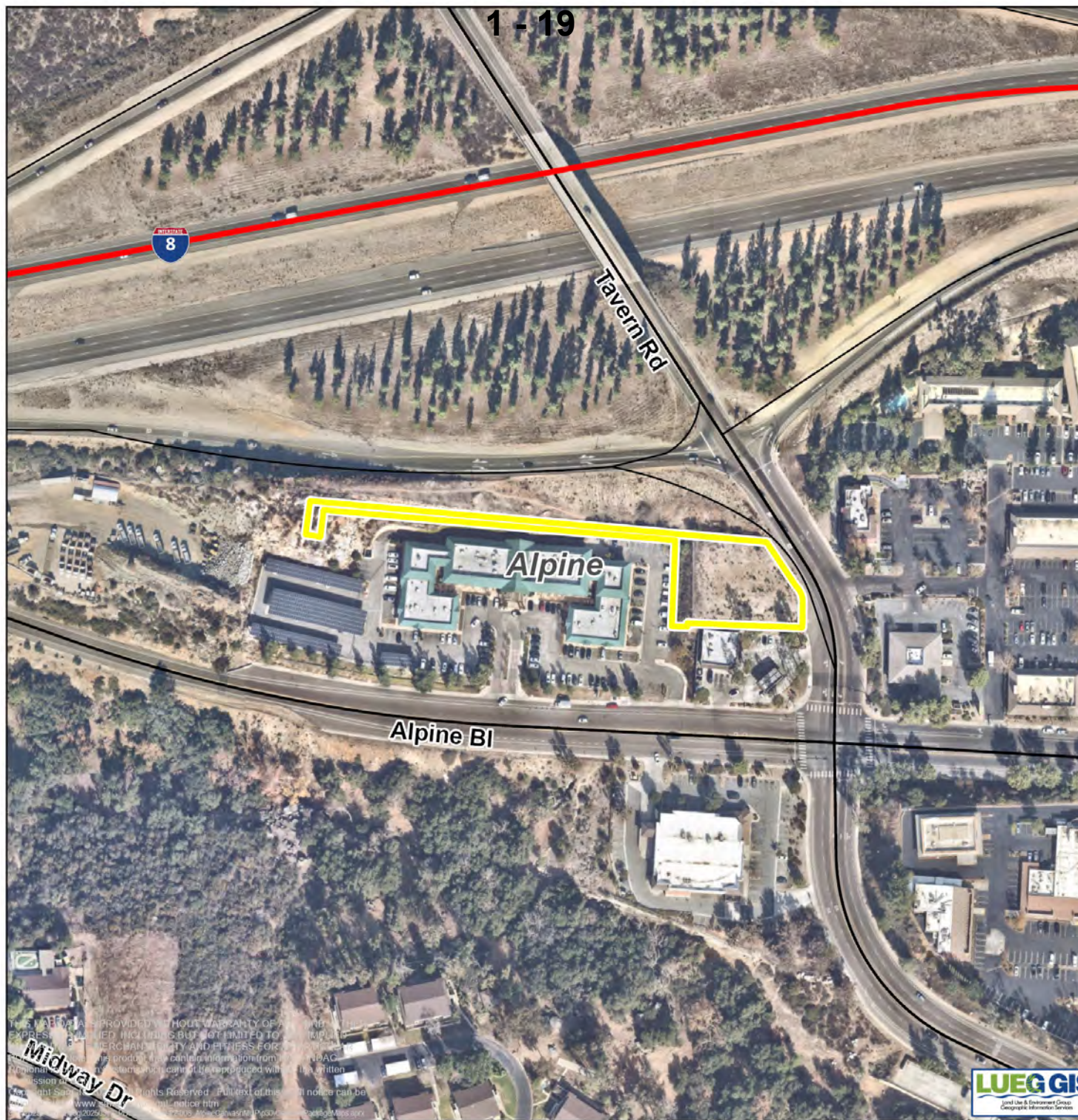
Alpine  
Community Plan Area

-  Roads
-  Site
-  CPA



0 500 1,000  
Feet





Alpine Carwash MUP  
PDS2022-MUP-22-008  
**Aerial**  
Alpine  
Community Plan Area

- Roads
- Site
- CPA

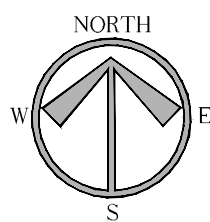
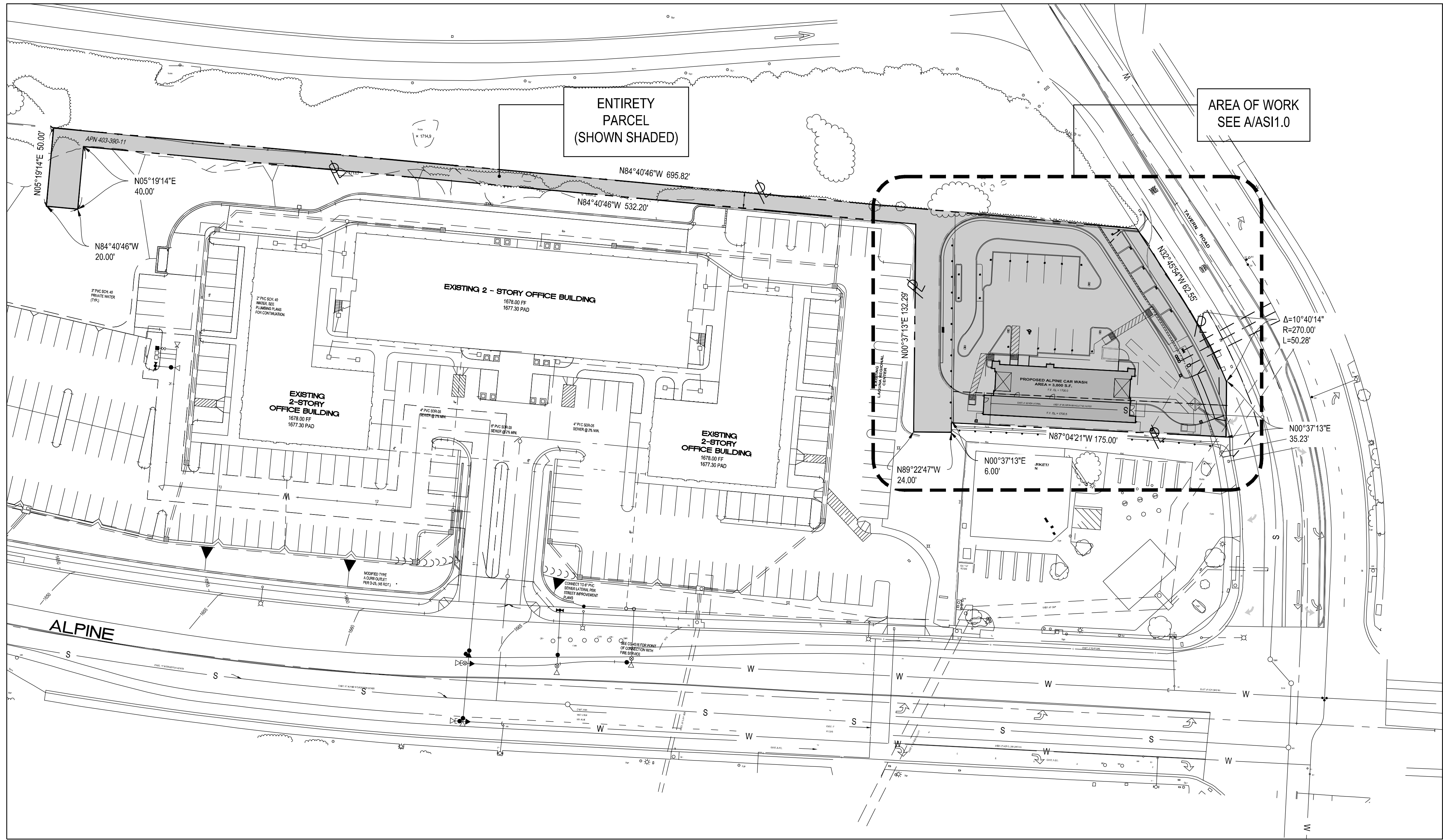




A PROPOSED COMMERCIAL DEVELOPMENT FOR:

# ALPINE CAR WASH

1250 TAVERN ROAD,  
ALPINE, CA 91901



OVERALL SITE PLAN

1" = 40' - 0"

0 20' 40' 60' 80' 100'

SCALE: 1" = 40'

A

### SITE ANALYSIS

#### SITE ANALYSIS

GROSS SITE AREA	30,056 S.F. (0.69 AC)
ROAD EASEMENT DEDICATION AREA	2,659 S.F.
NET SITE AREA	27,397 S.F. (0.63 AC)
PROPOSED BUILDING AREA	3,000 S.F.
PROPOSED LANDSCAPE AREA	± 6,309 S.F.
LOT COVERAGE	10.0%
FAR	10.0%

#### AREA ANALYSIS

PLANNING AREA	ALPINE
LAND USE ZONE	C36 GENERAL COMMERCIAL
THE GENERAL PLAN REGIONAL CATEGORY IS VILLAGE	GENERAL COMMERCIAL
AREA SEPARATION WALLS	NONE
CONSTRUCTION TYPE	V-B
NUMBER OF STORIES AND HEIGHT	2 STORY, 27' MAX HGT.

#### PARKING ANALYSIS

AUTOMOTIVE AND EQUIPMENT: CLEANING	
3 PARKING SPACES PER KSF GFA	
PARKING REQUIRED: 3 KSF GFA (9,000) =	9 STALLS
PARKING PROVIDED	9 STALLS
REQD AND PROVIDED VAN ACCESSIBLE PARKING	1 STALL

#### APN NUMBER

403-390-27-00

#### LEGAL DESCRIPTION

A PORTION OF SOUTHWEST QUARTER OF THE NORTH QUARTER OF SECTION 28, TOWNSHIP 15 SOUTH, RANGE 2 EAST, SAN BERNARDINO MERIDIAN, IN THE COUNTY OF SAN DIEGO, STATE OF CALIFORNIA, ACCORDING TO OFFICIAL PLAT THEREOF.

#### PROPERTY OWNER

GROWNEY USA LP  
MAIL ADDRESS: 10055 GRANDVIEW DR. LA MESA, CA 91941.  
PARCEL ADDRESS: 1250 TAVERN RD. ALPINE, CA 91901

### SCOPE OF WORK

THE PROJECT SITE IS LOCATED AT 1250 TAVERN ROAD IN THE ALPINE COMMUNITY PLANNING AREA WITHIN UNINCORPORATED SAN DIEGO COUNTY (APN 403-390-27). THE GENERAL PLAN REGIONAL CATEGORY IS VILLAGE. ZONING FOR THE SITE IS C36 GENERAL COMMERCIAL. THIS PROJECT REQUIRES A MAJOR USE PERMIT FOR THE COMMERCIAL DEVELOPMENT OF A 3,000 SQUARE FOOT EXPRESS CAR WASH ON A 30,056 SQUARE FOOT SITE PURSUANT TO ZONING ORDINANCE SECTION 1430. THE PROJECT WILL INCLUDE COVERED VACUUM PARKING STALLS AND ASSOCIATED SITE IMPROVEMENTS. THE CARWASH WILL OPERATE MONDAY - SUNDAY FROM 7AM-7PM WITH AN EXTRA HOUR TO OPEN AND CLOSE. THE CARWASH CAPACITY WILL BE 80 CARS PER HOUR. ACCESS TO THE SITE IS FROM TAVERN ROAD, A COUNTY MAINTAINED ROAD. THE PROJECT IS SERVED BY THE PADRE DAM MUNICIPAL WATER DISTRICT AND SEWER BY THE SAN DIEGO COUNTY SANITATION DISTRICT.

### PLANNING AND DEVELOPMENT SERVICES NOTE(S)

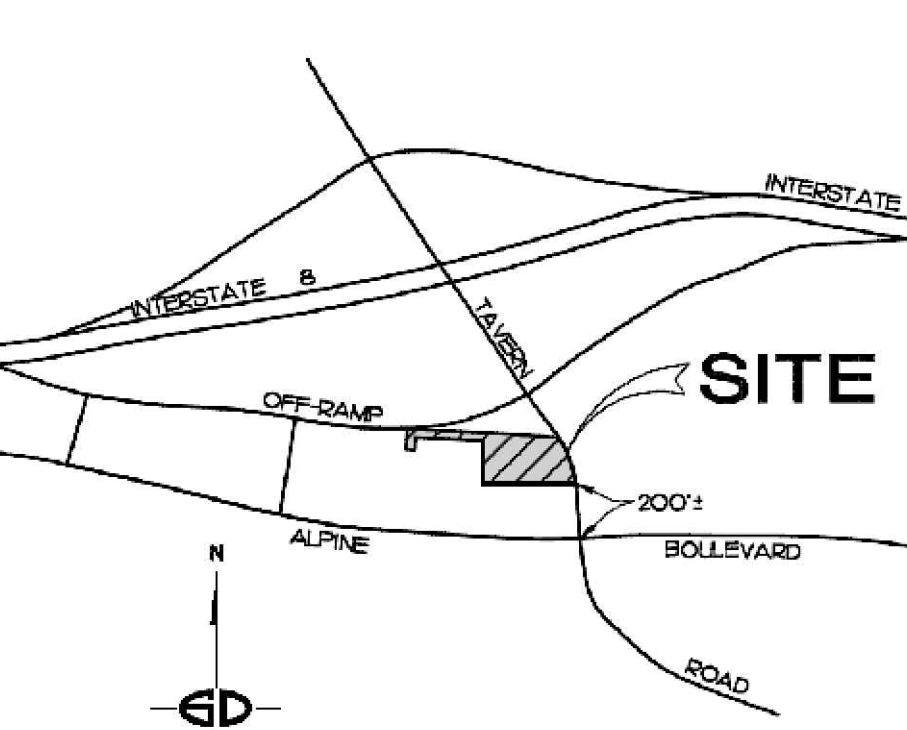
- FIRE ALARM MONITORING: A WIRELESS MESH OR GSM NETWORK SHALL BE INSTALLED WITH AN APPROVED FACP FIRE ALARM CONTROL PANEL AS PER NFPA 72.
- LIGHTING ORDINANCE COMPLIANCE: IN ORDER TO COMPLY WITH THE COUNTY LIGHTING ORDINANCE 59.101 ET SEQ. AND ZONING ORDINANCE SECTIONS 6322, 6324, AND 6328, THE ON-SITE LIGHTING SHALL COMPLY WITH THE APPROVED PLOT PLAN(S). SPECIFIC PERMIT CONDITIONS AND APPROVED BUILDING PLANS ASSOCIATED WITH THIS PERMIT. ALL LIGHT FIXTURES SHALL BE DESIGNED AND ADJUSTED TO REFLECT LIGHT DOWNWARD, AWAY FROM ANY ROAD OR STREET, AND AWAY FROM ADJOINING PREMISES, AND SHALL OTHERWISE CONFORM TO THE COUNTY LIGHTING ORDINANCE 59.101 ET SEQ. AND ZONING ORDINANCE SECTIONS 6322 AND 6324. THE PROPERTY OWNER AND PERMITTED SHALL CONFORM TO THE APPROVED PLOT PLAN(S). SPECIFIC PERMIT CONDITIONS, AND APPROVED BUILDING PLANS ASSOCIATED WITH THIS PERMIT AS THEY PERTAIN TO LIGHTING. NO ADDITIONAL LIGHTING IS PERMITTED IF THE PERMITTEE OR PROPERTY OWNER CHOOSES TO CHANGE THE SITE DESIGN IN ANY WAY, THEY MUST OBTAIN APPROVAL FROM THE COUNTY FOR A MINOR DEVIATION OR A MODIFICATION PURSUANT TO THE COUNTY OF SAN DIEGO ZONING ORDINANCE.
- THE ILLUMINATION OF ADJACENT PREMISES SHALL NOT EXCEED A VALUE OF 0.2 FOOT CANDLES MEASURED IN THE HORIZONTAL OR VERTICAL PLANE AT A POINT THREE FEET ABOVE GRADE LEVEL AND FIVE FEET INSIDE ADJACENT PROPERTY.
- ALL EXTERIOR LIGHTING FIXTURES SHALL BE CAP-OFF AT MAXIMUM OF 4050 LUMENS.

### PROJECT SUSTAINABLE DESIGN FEATURES

THE PROJECT HAS BEEN DESIGNED TO INCLUDE SUSTAINABLE DESIGN FEATURES FOR CONFORMANCE WITH THE GENERAL PLAN AND THE ALPINE COMMUNITY GUIDELINES. THE SUSTAINABLE FEATURES INCLUDE:

- ENERGY: THE PROJECT INCLUDES PROVISIONS FOR A FUTURE ROOFTOP PHOTOVOLTAIC PANELS WITH BATTERY STORAGE. THIS SYSTEM IS INTENDED TO ULTIMATELY PROVIDE 100% OF THE ELECTRIC REQUIREMENTS OF THE PROJECT.
- WATER: THE LANDSCAPE AND IRRIGATION SYSTEM HAS BEEN DESIGNED WITH DROUGHT TOLERANT, NATIVE PLANTING MATERIALS TO MINIMIZE WATER CONSUMPTION AND GREEN WASTE.
- WASTEWATER: THE CARWASH INCLUDES A WASTE WATER RECLAMATION AND RECYCLING SYSTEM.
- SOLID WASTE: THE OPERATION OF THIS CARWASH WILL PRODUCE MINIMAL WASTE. A TRASH ENCLOSURE WITH 50% OF THE BINS RESERVED FOR RECYCLING IS PROVIDED.
- THE METAL ROOF PANELS ARE A KYNAR 500 FINISH THAT COMPLY WITH ENERGY STAR COOL ROOF CRITERIA.
- BOTH THE ROOF AND EXTERIOR METAL WALL PANELS ARE 100% RECYCLABLE AND ARE MANUFACTURED USING UP TO 30% RECYCLED METAL.
- THE STRUCTURAL STEEL SUPPLIED IS ALSO 100% RECYCLABLE AND USES UP TO 50% RECYCLED STEEL IN ITS MANUFACTURING PROCESS.
- POLYCARBONATED PANELS ARE 100% RECYCLABLE AND USE UP TO 40% RECYCLED MATERIAL DURING THE MANUFACTURING PROCESS.

### VICINITY MAP



### ARCHITECT



MEMBER AMERICAN INSTITUTE OF ARCHITECTS

3578 30th Street  
San Diego, CA 92104  
V. 619.236.0595  
F. 619.236.0557  
www.mpa-architects.com

### CLIENT

Growney USA, LP  
10055 Grandview Dr.  
La Mesa, CA 91941  
P: (619) 254.9648  
Contact: Linus Klein  
E: linus.klein@growney.de

### PROJECT

A PROPOSED COMMERCIAL DEVELOPMENT FOR:

ALPINE CAR WASH

1250 TAVERN ROAD  
ALPINE, CA 91901

### REVISIONS

NO.	DESCRIPTION	DATE
1	PDS COMMENTS	10-21-22
2	PDS COMMENTS	01-25-23
3	PDS COMMENTS	05-23-24

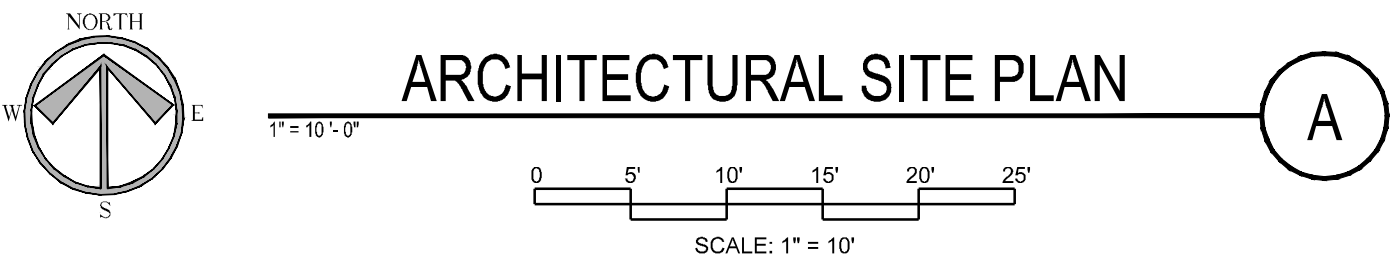
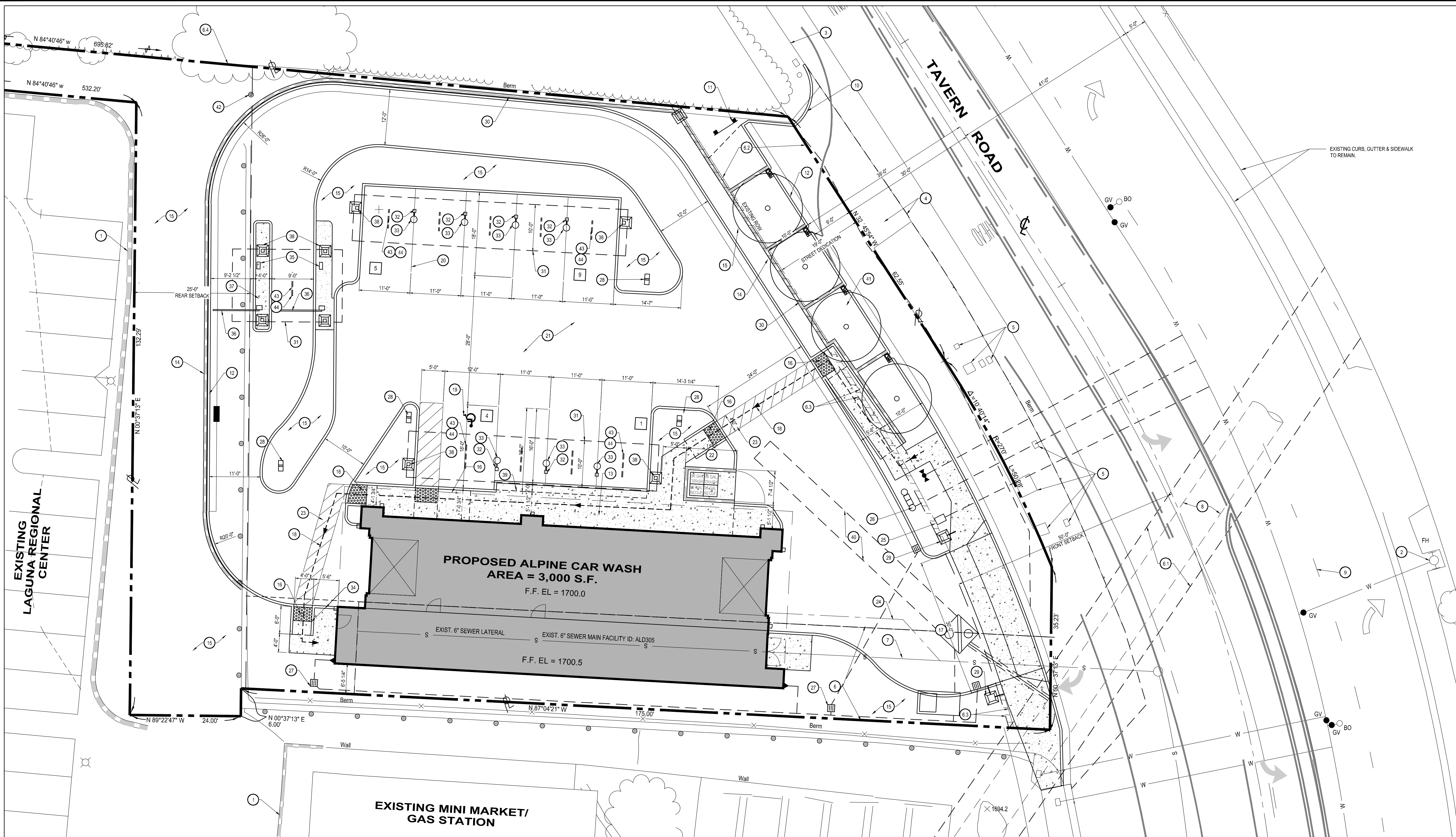


### SHEET TITLE

OVERALL SITE PLAN

DATE:	11-08-23	SCALE:	AS NOTED
DRAWN BY:	CB	CAD/AR:	23002.DWG
CHECKED BY:	JR	SHEET NUMBER:	
PROJECT NO:	23002		AS0.0





# SHEET KEYNOTES					
1.	EXISTING RETAINING WALL, TO REMAIN (NOT A PART).	11.	NEW PED BARRICADE, REFER TO CIVIL PLANS.	27.	NEW STORM DRAIN, REFER TO CIVIL PLANS.
2.	EXISTING FIRE HYDRANT.	12.	NEW CONC. CURBS AND GUTTER, REFER TO CIVIL PLANS.	28.	NEW LED SITE LIGHT POLE WITH PEDESTAL AT 11'-4" A.F.F.. FOR DETAILS REFER TO SHEETS L2 & L3 OF LANDSCAPE PLANS.
3.	EXISTING BERM A.C. TO REMAIN.	13.	NEW CONCRETE SIDEWALK, REFER TO CIVIL PLANS.	29.	NEW 7' HIGH PEDESTAL WITH DIRECTIONAL SIGNAGE, REFER TO LANDSCAPE PLANS.
4.	EXISTING PORTION A.C. BERM TO BE REMOVED AND A.C. PAVEMENT TO BE REPLACED PER CIVIL PLANS.	14.	NEW RETAINING WALL, REFER TO CIVIL PLANS.	30.	NEW 36" HIGH WOOD FENCE ON TOP OF RETAINING WALL.
5.	EXISTING ELECTRICAL VAULTS AND TRANSFORMER TO BE RELOCATED.	15.	NEW LANDSCAPE AREAS, REFER TO LANDSCAPE PLANS.	31.	PERIMETER OF NEW WOOD TRELLIS (DASH LINE).
6.	EXISTING 20' SEWER EASEMENT TO ALPINE SANITATION DISTRICT RECORDED SEPTEMBER 25, 1969 AS INSTRUMENT NO.176546 OF OFFICIAL RECORDS.	16.	NEW CURB-CUT RAMP, REFER TO CIVIL PLANS.	32.	NEW TRELLIS COLUMN.
6.1	EXISTING 15' PUBLIC DRAINAGE EASEMENT, PER DOC 87-568352, RECORDER OCTOBER 8, 1987.	17.	NEW CONC. DRIVE WAY PER CITY STANDARDS, REFER TO CIVIL PLANS.	33.	NEW TRASH BIN AT VACUUM STATIONS.
6.2	EASEMENT TO COUNTY OF SAN DIEGO FOR HIGHWAY PURPOSES RECORDER MAY 6, 2015 AS INSTRUMENT NO. 2015-0223047 OF OFFICIAL RECORDS.	18.	NEW PEDESTRIAN ACCESSIBLE PATH OF TRAVEL FROM PUBLIC WAY (DASH LINE) WITH 5% MAX. SLOPE AND 2% MAX. CROSS SLOPE (DASH LINE).	34.	NEW 36" HIGH MIN. METAL RAILING.
6.3	RELINQUISHMENT OF ACCESS RIGHTS TO TAVERN ROAD, EXCEPT 36' ACCESS OPENING RECORDED MAY, 2015 AS INSTRUMENT NO. 2015-0223048 OF OFFICIAL RECORDS.	19.	NEW VAN ACCESSIBLE PARKING STALL.	35.	NEW CAR WASH PAY POINT STATION.
6.4	ABUTTER'S RIGHTS OF INGRESS AND EGRESS TO OR FROM STATE FREEWAY RELINQUISHED PER DOCUMENT RECORDED MARCH 4, 1966 AS INSTRUMENT NO. 37482 OF OFFICIAL RECORDS.	20.	NEW PARKING STALLS, TYP.	36.	NEW PIVOT ACCESS GATE.
7.	EXISTING SEWER MAIN TO BE REMOVED, REFER TO CIVIL PLANS.	21.	NEW PAVED PARKING LOT, REFER TO CIVIL PLANS.	37.	NEW HARDSCAPE AREA PER CIVIL PLANS.
8.	EXISTING 96" CMP STORM DRAIN.	22.	NEW TRASH ENCLOSURE WITH HARD COVER PER CITY STANDARDS, AND (4) 96 GAL. BINS (50% MUST BE DEDICATED FOR RECYCLE).	38.	NEW COLUMN TRELLIS WITH PEDESTAL.
9.	EXISTING TRAFFIC STRIPING, (TYP).	23.	NEW ACCESSIBLE STRIPPED PATH OF TRAVEL, 48" MIN WIDE WITH 5% MAX. SLOPE AND 2% MAX. CROSS SLOPE.	39.	2'-0" PARKING OVERHANG (DASH LINE).
10.	NEW A.C. BERM, REFER TO CIVIL PLANS.	24.	NEW SEWER LINE, REFER TO CIVIL PLANS.	40.	NEW STORM DRAIN, REFER TO CIVIL PLANS.
		25.	NEW WATER METER, REFER TO CIVIL PLANS.	41.	NEW TREE WELLS, REFER TO CIVIL PLANS.
		26.	NEW HYDRANT, REFER TO CIVIL PLANS.	42.	NEW FLAG POLE BY OTHERS.
				43.	NEW 4' LED STRIP LIGHT AT TRELLIS, TYP.
				44.	PROVIDE SOLID TRIM AT EACH END OF TRELLIS FIXTURE TO MINIMIZE ANY LIGHT TRESPASSING ONTO ADJACENT PROPERTIES AS REQUIRED BY COUNTY'S LIGHT POLLUTION CODE.

ARCHITECT

MPA ARCHITECTS INC.

MEMBER AMERICAN INSTITUTE OF ARCHITECTS

3578 30th Street  
San Diego, CA 92104  
V. 619.236.0595  
F. 619.236.0557  
www.mpa-architects.com

CLIENT

Growney USA, LP  
10055 Grandview Dr.  
La Mesa, CA 91941  
P: (619) 254.9648  
Contact: Linus Klein  
E: linus.klein@growney.de

PROJECT

A PROPOSED COMMERCIAL DEVELOPMENT FOR:  
**ALPINE CAR WASH**  
1250 TAVERN ROAD  
ALPINE, CA 91901

REVISIONS		
NO.	DESCRIPTION	DATE
1	PDS COMMENTS	10/21/22
2	PDS COMMENTS	01/25/23
3	PDS COMMENTS	05/23/24

REGISTERED ARCHITECT  
LINUS KLEIN  
Exp. 3-31-2024  
STATE OF CALIFORNIA

SHEET TITLE  
**ARCHITECTURAL SITE PLAN**

DATE: 11-08-23  
DRAWN BY: CB  
CHECKED BY: JR  
PROJECT NO: 23002

SCALE: AS NOTED  
DATE: 23002 DWG  
SHEET NUMBER:  
PROJECT NO: 23002

AS1.0















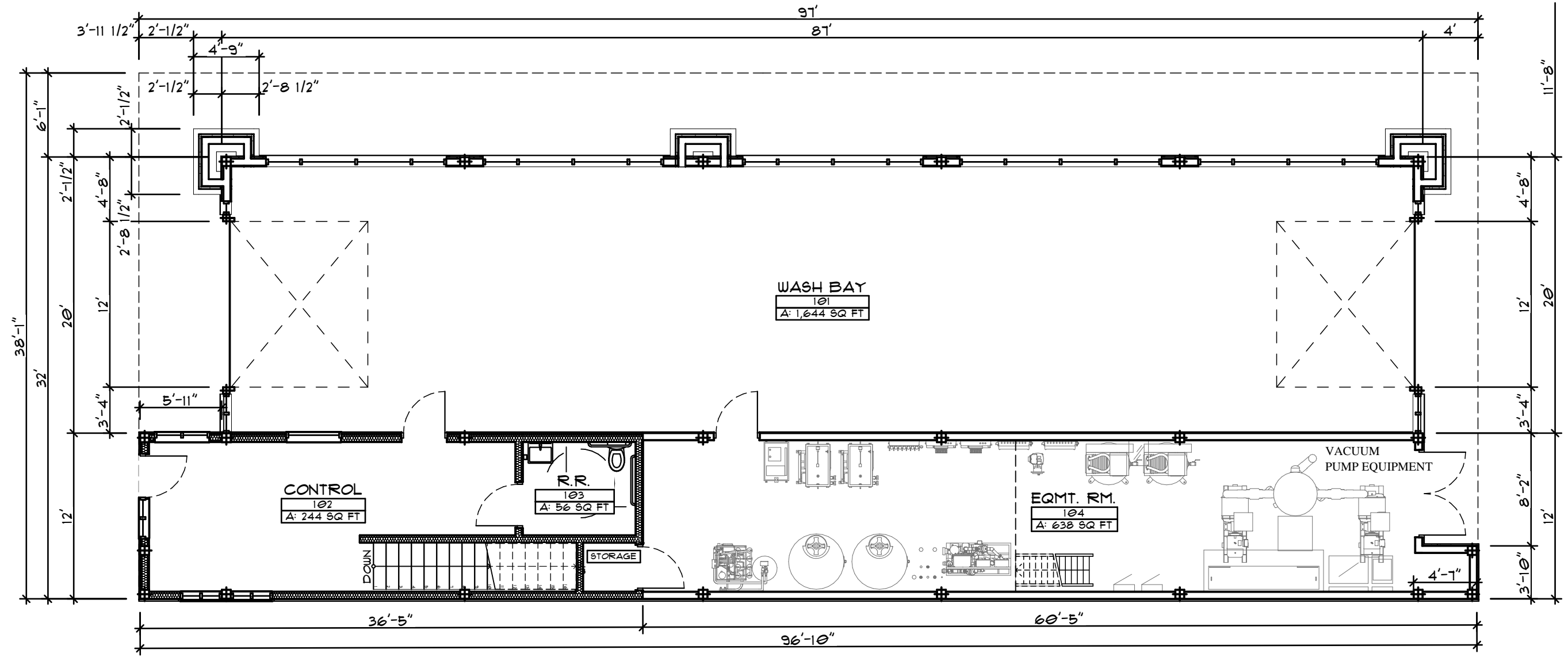




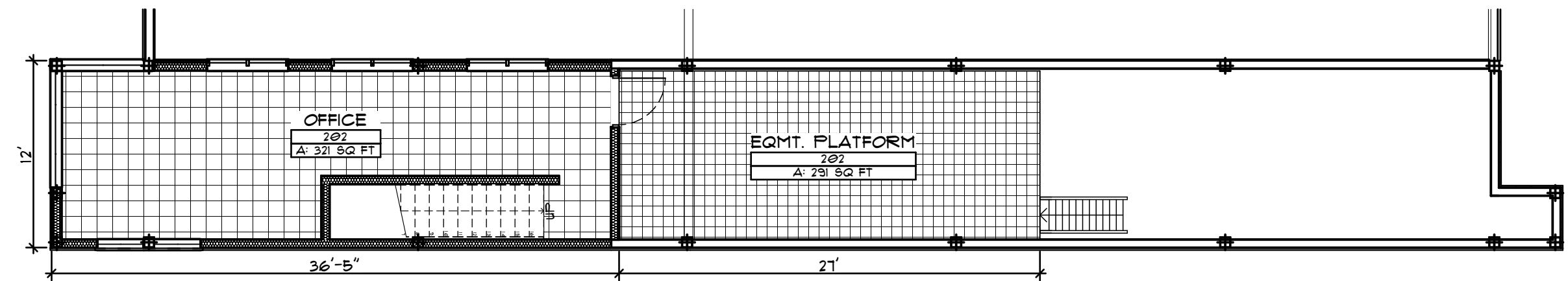




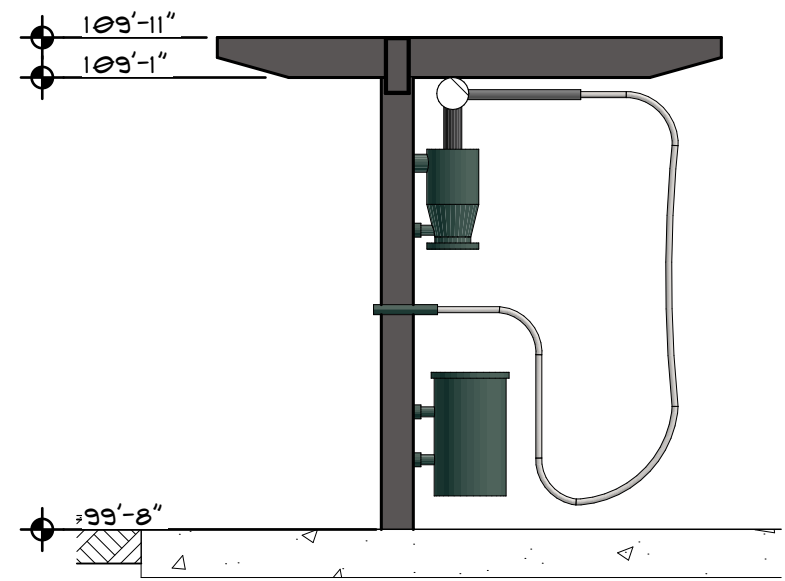




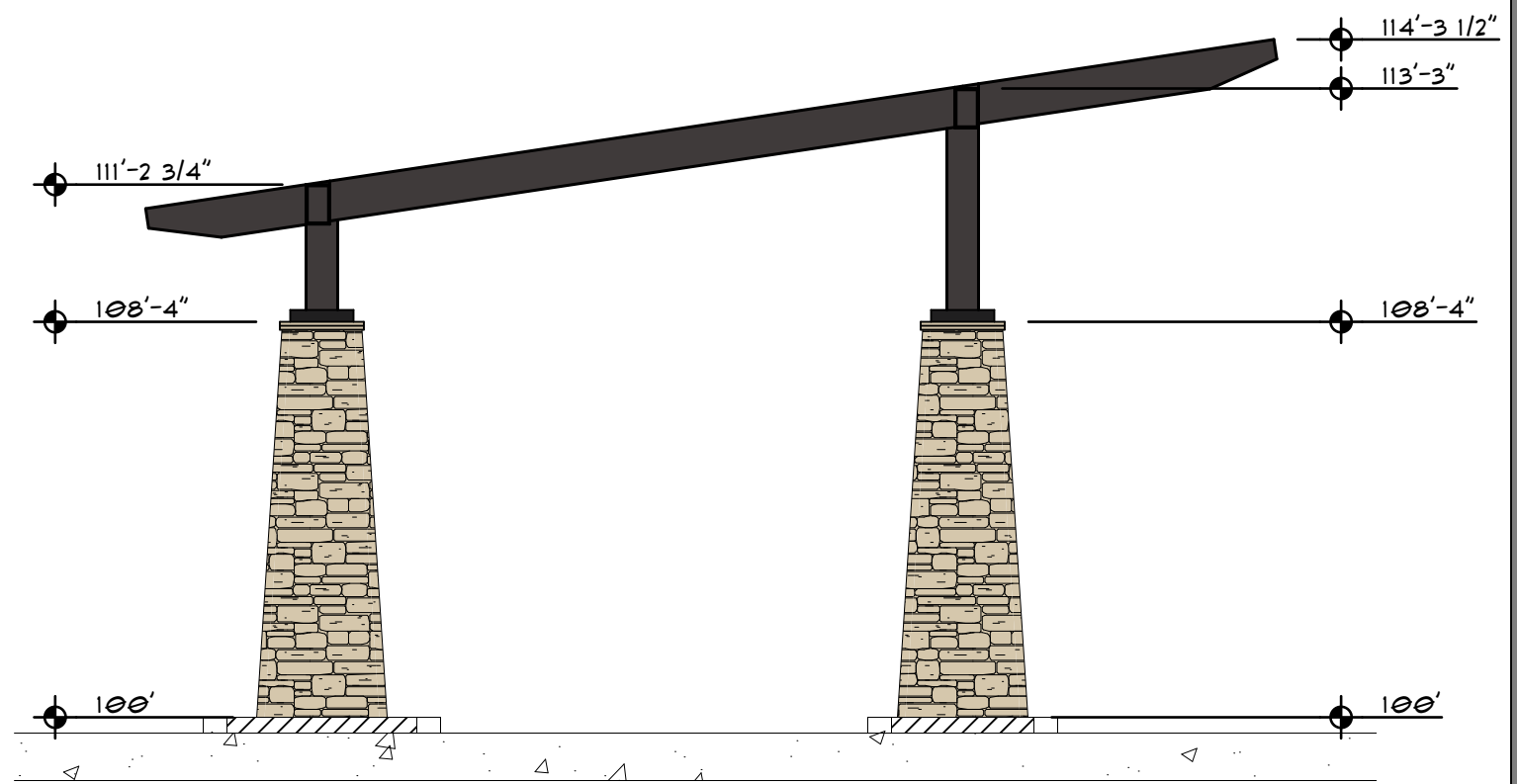
1 1ST FLOOR  
SCALE: 1/8" = 1'-0"



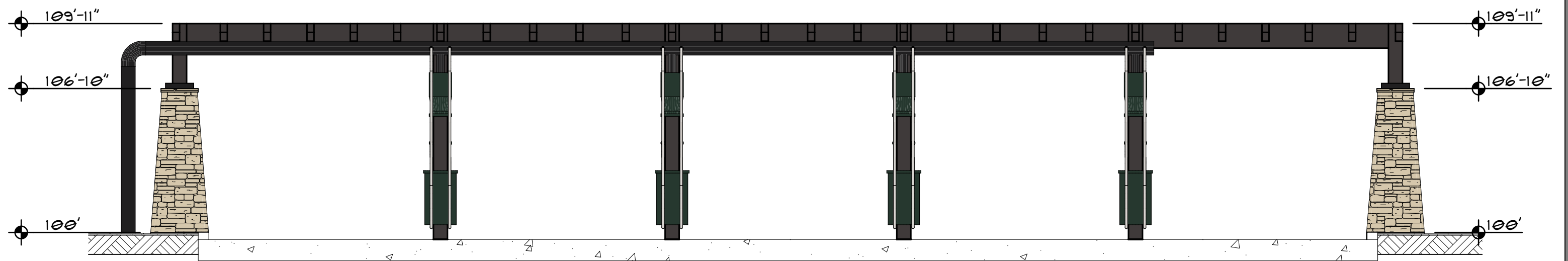
2 2ND FLOOR  
SCALE: 1/8" = 1'-0"



1 VAC CANOPY  
NOT TO SCALE



2 PAY CANOPY  
NOT TO SCALE



3 VAC CANOPY ELEVATION  
NOT TO SCALE

**Attachment B – Form of Decision  
Approving PDS2022-MUP-22-008**



**VINCE NICOLETTI**  
DIRECTOR

**PLANNING & DEVELOPMENT SERVICES**  
5510 OVERLAND DRIVE, SUITE 210, SAN DIEGO, CALIFORNIA 92123  
(858) 505-6445 General • (858) 694-2705 Codes Compliance  
(858) 565-5920 Building Services

**COMMISSIONERS**  
Ronald Ashman (Chair)  
Michael Edwards (Vice Chair)  
Molly Weber  
Yolanda Calvo  
David Pallinger  
Colton Sudberry

July 9, 2025

**PERMITTEE:** GROWNEY USA LP (ATTN: LINUS KLEIN)  
**MAJOR USE PERMIT:** PDS2022-MUP-22-008  
**E.R. NUMBER:** PDS2022-ER-22-03-001  
**PROPERTY:** 1250 TAVERN ROAD WITHIN THE ALPINE COMMUNITY PLANNING AREA  
**APN:** 403-390-27-00

### **DECISION OF THE PLANNING COMMISSION**

This Major Use Permit for MUP-22-008 consists of twenty (20) sheets including a site plan, and elevations. This permit authorizes the construction and operation of a 3,000-squarefoot express carwash facility and associated equipment.

**MAJOR USE PERMIT EXPIRATION:** This Major Use Permit shall expire on **July 9, 2027** at 4:00 p.m. (or such longer period as may be approved pursuant to Section 7376 of The Zoning Ordinance of the County of San Diego prior to said expiration date) unless construction or use in reliance on this Minor Use Permit has commenced prior to said expiration date.

**WAIVER(S) AND EXCEPTION(S):** This Major Use Permit Modification is hereby approved pursuant to the provisions of the San Diego County Zoning Ordinance, the County Public Road Standards and Private Road Standards, and all other required ordinances of the County of San Diego. The sole exceptions to the aforementioned are:

1. Allow a reduction in the required minimum separation distance between intersecting centerlines of a Non-Mobility Element Road (including driveways) entering a Mobility Element Road, in accordance with Section 6.1.C.2 for the proposed driveway along Tavern Road pursuant to the Design Exception Request that was approved on November 8, 2012 for MUP-12-019.

**SPECIFIC CONDITIONS:** Compliance with the following Specific Conditions (Mitigation Measures when applicable) shall be established before the property can be used in reliance upon this Site Plan. Where specifically indicated, actions are required prior to approval of any



grading, improvement, building plan and issuance of grading, construction, building, or other permits as specified:

**ANY PERMIT:** (Prior to the issuance of any permit and prior to occupancy or use of the premises in reliance of this permit).

1. **GEN#1–COST RECOVERY**

**INTENT:** In order to comply with Section 362 of Article XX of the San Diego County Administrative Code, Schedule B.5, existing deficit accounts associated with processing this permit shall be paid. **DESCRIPTION OF REQUIREMENT:** The applicant shall pay off all existing deficit accounts associated with processing this permit. **DOCUMENTATION:** The applicant shall provide evidence to Planning & Development Services, Zoning Counter, which shows that all fees and trust account deficits have been paid. No permit can be issued if there are deficit trust accounts. **TIMING:** Prior to the approval of any plan and prior to the issuance of any permit and prior to use in reliance of this permit, all fees and trust account deficits shall be paid. **MONITORING:** The PDS Zoning Counter shall verify that all fees and trust account deficits have been paid.

2. **GEN#2–RECORDATION OF DECISION**

**INTENT:** In order to comply with Section 7019 of the Zoning Ordinance, the Permit Decision shall be recorded to provide constructive notice to all purchasers, transferees, or other successors to the interests of the owners named, of the rights and obligations created by this permit. **DESCRIPTION OF REQUIREMENT:** The applicant shall sign, notarize with an all-purpose acknowledgement' and return the original recordation form to PDS. **DOCUMENTATION:** Signed and notarized original recordation form. **TIMING:** Prior to the approval of any plan and prior to the issuance of any permit and prior to use in reliance of this permit, a signed and notarized copy of the Decision shall be recorded by PDS at the County Recorder's Office. **MONITORING:** The PDS Zoning Counter shall verify that the Decision was recorded and that a copy of the recorded document is on file at PDS.

3. **LNDSKP#1–LANDSCAPE DOCUMENTATION PACKAGE**

**INTENT:** In order to provide adequate Landscaping that addresses screening, and to comply with the Alpine Community Design Objectives, a landscape plan shall be prepared. **DESCRIPTION OF REQUIREMENT:** The Landscape Plans shall be prepared pursuant to the [COSD Water Efficient Landscape Design Manual](#) and the [COSD Water Conservation in Landscaping Ordinance](#), the [COSD Off-Street Parking Design Manual](#), the COSD Grading Ordinance, the Alpine Design Guidelines, and the requirements of the S Designator. All Plans shall be prepared by a California licensed Landscape Architect, Architect, or Civil Engineer, and include the following information:

- a. Indication of the proposed width of any adjacent public right-of-way, and the locations of any required improvements and any proposed plant materials to be installed or planted therein. The applicant shall also obtain a permit approving the variety, location, and spacing of all trees proposed to be planted within said right(s)-of-way. A copy of this permit and a letter stating that all landscaping within the said right(s) -of-way shall be maintained by the landowner(s) shall be submitted to PDS.



- b. A complete planting plan including the names, sizes, and locations of all plant materials, including trees, shrubs, and groundcover. Wherever appropriate, native or naturalizing plant materials shall be used which can thrive on natural moisture. These plants shall be irrigated only to establish the plantings.
- c. A complete watering system including the location, size, and type of all backflow prevention devices, pressure, and non-pressure water lines, valves, and sprinkler heads in those areas requiring a permanent, and/or temporary irrigation system.
- d. The watering system configuration shall indicate how water flow, including irrigation runoff, low head drainage, overspray or other similar conditions will not impact adjacent property, non-irrigated areas, structures, walkways, roadways or other paved areas, including trails and pathways by causing water to flow across, or onto these areas.
- e. Spot elevations of the hardscape, building and proposed fine grading of the installed landscape.
- f. The location and detail of all walls, fences, and walkways shall be shown on the plans, including height from grade and type of material. A lighting plan and light standard details shall be included in the plans (if applicable) and shall be in compliance with the [County's Light Pollution Code](#).
- g. No landscaping material or irrigation or other infrastructure shall be located within a proposed trail easement or designated pathway.
- h. Parking areas shall be landscaped and designed pursuant to the [Off-street Parking Design Manual](#) and the County Zoning Ordinance Section 6793.b
- i. Additionally, the following item shall be addressed as part of the Landscape Documentation Package:
  - Indicate on the plans, any existing planting installed per the approved landscape plans (PDS2016-LP-16-071, approved on December 29, 2016, associated with PDS2012-3300-12-019), currently on file for the property. The owner shall provide a letter withdrawing those plans from record.

**DOCUMENTATION:** The applicant shall prepare the Landscape Plans using the [Landscape Documentation Package Checklist](#) (PDS Form #404), and pay all applicable review fees. **TIMING:** Prior to approval of any plan, issuance of any permit, and prior to use of the premises in reliance of this permit, the Landscape Documentation Package shall be prepared and approved. **MONITORING:** The [PDS, LA] and [DPR, TC, PP] shall review the Landscape Documentation Package for compliance with this condition.

#### 4. **ROADS#1–SIGHT DISTANCE**

**INTENT:** In order to provide an unobstructed view for safety while exiting the property and accessing a public road from the site, and to comply with the Design Standards of Section 6.1.(E) of the County of San Diego Public Road Standards, an unobstructed sight distance shall be verified. **DESCRIPTION OF REQUIREMENT:**

- a. A registered civil engineer or a licensed land surveyor provides a certified signed statement that: "There is \_\_\_\_\_feet of unobstructed intersectional sight distance in both direction(s) along *Tavern Road* from the proposed driveway in accordance with the methodology described in Table 5 of the March 2012 County of San Diego Public Road Standards. These sight distances exceed the required intersectional Sight Distance requirements of \_\_\_\_\_ as described in Table 5 based on a speed of \_\_\_\_\_, which I have verified to be the higher of the prevailing speed or the minimum design speed of the road classification. I have exercised responsible charge for the certification as defined in Section 6703 of the Professional Engineers Act of the California Business and Professions Code."
- b. If the lines of sight fall within the existing public road right-of-way, the engineer or surveyor shall further certify: "Said lines of sight fall within the existing right-of-way and a clear space easement is not required."

**DOCUMENTATION:** The applicant shall complete the certifications and submit them to the [PDS, LDR] for review. **TIMING:** Prior to the approval of any plan, issuance of any permit, and prior to occupancy or use of the premises in reliance of this permit, the sight distance shall be verified. **MONITORING:** The [PDS, LDR] shall verify the sight distance certifications.

## 5. **ROADS#2–CALTRANS' ENCROACHMENT PERMIT**

**INTENT:** In order to ensure that improvements for Tavern Road comply with the County of San Diego Public Road Standards, and the CALTRANS Facility Standards and Requirements, an encroachment permit from CALTRANS shall be obtained and implemented for any work within the CALTRANS right-of-way. **DESCRIPTION OF REQUIREMENT:** A permit shall be obtained from CALTRANS ~~for the~~ if there are improvements to be made within the public right-of-way. A copy of the permit and evidence from the issuing agency that all requirements of the permit have been met shall be submitted to the [PDS, LDR]. **DOCUMENTATION:** The applicant shall obtain the encroachment permit and provide a copy of the permit, proof of payment, and evidence that all the requirements of the permit have been met, to the [PDS, LDR]. The developer shall obtain an encroachment permit from CALTRANS ~~authorizing access onto Interstate 8~~ if there is any work done within the CALTRANS right-of-way. This will allow CALTRANS to set conditions such as sight distance and road improvements. The applicant should contact the CALTRANS Permit Office at (619) 688-6843 for additional details. **TIMING:** Prior to the approval of any plan, issuance of any County permit, and prior to occupancy or use of the premises in reliance of this permit, the encroachment permit shall be obtained if there is any work done within the CALTRANS right-of-way. **MONITORING:** The [PDS, LDR] shall review the permit for compliance with this condition and the applicable improvement plans and implement any conditions of the permit in the County improvement plans.

## 6. **ROADS#3–PUBLIC ROADS IMPROVEMENTS**

**INTENT:** In order to promote orderly development and to comply with the County of San Diego Board Policy I-18 and the County Community Trails Master Plan, York Drive shall be improved. **DESCRIPTION OF REQUIREMENT:**

- a. Improve or agree to improve and provide security for **Tavern Road** from centerline, curb and gutter etc. to a one-half graded width of **forty-nine feet (49')** with Portland cement concrete curb, gutter, and sidewalk, asphalt concrete pavement over approved base, asphalt concrete dike, asphalt concrete driveway apron, street light(s), asphalt concrete acceleration/deceleration lane, and asphalt concrete dike taper to existing pavement. Face of curb/dike shall be **thirty-nine feet (39')** from centerline.
- b. Asphalt concrete surfacing material shall be hand-raked and compacted to form smooth tapered connections along all edges including those edges adjacent to soil. The edges of asphalt concrete shall be hand-raked at 45 degrees or flatter to provide a smooth transition next to existing soil, including those areas scheduled for shoulder backing.
- c. Provide transition, tapers, traffic striping to match existing pavement. The proposed access shall be restricted with right in and right out only. All of the above shall be to the satisfaction of the Director of PDS.

All plans and improvements shall be completed pursuant to the County of San Diego Public Road Standards, the PDS Land Development Improvement Plan Checking Manual and the Community Trails Master Plan. The improvements shall be completed within 24 months from the approval of the improvement plans, execution of the agreements, and acceptance of the securities. **DOCUMENTATION:** The applicant shall complete the following:

- a. Process and obtain approval of Improvement Plans to improve Tavern Road.
- b. Provide Secured Agreements. The required security shall be in accordance with Section 7613 of the Zoning Ordinance.
- c. Pay all applicable inspection fees with [DPW, PDC].
- d. If the applicant is a representative, then one of the following is required: a corporate certificate indicating those corporation officers authorized to sign for the corporation, or a partnership agreement recorded in this County indicating who is authorized to sign for the partnership.
- e. Obtain approval for the design and construction of all driveways, turnarounds, and private easement road improvements to the satisfaction of the Alpine Fire Protection District and [PDS, LDR].
- f. Obtain a Construction Permit for any work within the County road right-of-way. PDS Construction/Road right-of-way Permits Services Section should be contacted at [rowpermitcounter@sdcounty.ca.gov](mailto:rowpermitcounter@sdcounty.ca.gov) or (858)-694-2055 to coordinate departmental requirements. Also, before trimming, removing or planting trees or shrubs in the County Road right-of-way, the applicant must first obtain a permit to remove, plant or trim shrubs or trees from the Permit Services Section.

**TIMING:** Prior to the approval of any plan, and prior to use of the premises in reliance of this permit the plans shall be approved and securities must be provided. **MONITORING:**

The [PDS, LDR] and [DPR, TC] shall review the plans for consistency with the condition and County Standards and Community Trails Master Plan. Upon approval of the plans [PDS, LDR] shall request the required securities and improvement agreements. The securities and improvement agreements shall be approved by the Director of PDS before any work can commence.

7. **ROADS#4-TAVERN ROAD RE-STRIPING PLAN**

**INTENT:** In order to prevent left turn movements in and out from the project site; Tavern Road shall be re-striped from the project's driveway southerly to Alpine Boulevard.

**DESCRIPTION OF REQUIREMENT:** A detailed striping plan and signal modification plan for southbound travel way lanes shall be prepared to provide a striped median, 1 left turn lane, 2 through lanes, and 1 right turn lane. **DOCUMENTATION:** The applicant shall have the detailed striping plan and signal modification plan prepared by a licensed Traffic Engineer and submit the plans to the County [PDS, LDR] for review. **TIMING:** Prior to the approval of any plan, and prior to use of the premises in reliance of this permit the plans shall be approved and securities must be provided. **MONITORING:** The [PDS, LDR] shall review the plans for consistency with the condition.

8. **UTILITIES#1-PAVEMENT CUT POLICY**

**INTENT:** In order to prohibit trench cuts for undergrounding of utilities in all new, reconstructed, or resurfaced paved County-maintained roads for a period of three years following project pavement treatment, and to comply with County Policy RO-7 adjacent property owners shall be notified and solicited for their participation in the extension of utilities. **DESCRIPTION OF REQUIREMENT:** All adjacent property owners shall be notified who may be affected by this policy and are considering development of applicable properties, this includes requesting their participation in the extension of utilities to comply with this policy. No trench cuts for undergrounding of utilities in all new, reconstructed, or resurfaced paved County-maintained roads for a period of three years following project surface. **DOCUMENTATION:** The applicant shall sign a statement that they are aware of the County of San Diego Pavement Cut Policy and submit it to the [PDS LDR] for review. **TIMING:** Prior to approval of any grading or improvement plan and prior to issuance of any grading or construction permit, and prior to use of the property in reliance of this permit, the Acknowledgement of Department of Public Works Pavement Cut Policy shall be submitted for approval. **MONITORING:** [PDS, LDR] shall review the acknowledgement letter to determine compliance with the condition.

9. **ROADS#5-TRAFFIC CONTROL PLAN**

**INTENT:** In order to mitigate below levels of significance for temporary traffic impacts, a traffic control plan shall be prepared and implemented. **DESCRIPTION OF REQUIREMENT:** Have Registered Civil Engineer or licensed Traffic Control Contractor prepare a Traffic Control Plan (TCP) to the satisfaction of the Director of Department of Public Works (DPW). **DOCUMENTATION:** The applicant shall have the TCP prepared by a Registered Civil Engineer or a licensed Traffic Control Contractor and submit it to [PDS, LDR] for review by [DPW, Traffic]. **TIMING:** Prior to the issuance of any grading and/or improvement plans and issuance of any Grading, Construction, or Excavation Permits and prior to use of the premises in reliance of this permit, a TCP shall be prepared and approved. **MONITORING:** The [PDS, LDR] shall review the TCP for compliance with this condition.

10. **STRMWTR#1–STORMWATER MAINTENANCE DOCUMENTATION**

**INTENT:** In order to promote orderly development and to comply with the County Watershed Protection Ordinance (WPO) No.10410 (County Code Section 67.801 et. seq.), the maintenance agreements shall be completed. **DESCRIPTION OF REQUIREMENT:**

- a. Process a Maintenance Notification Agreement to assure maintenance of the Category 1 Structural BMPs to the satisfaction of the Director of DPW and/or PDS. The Maintenance Notification Agreement shall be signed, notarized, and recorded by the applicant.

OR

- b. Process a Stormwater Facilities Maintenance Agreement (SWMA) to assure maintenance of the Category 2 Structural BMPs and provide security to back up the maintenance pursuant to the County Maintenance Plan Guidelines to the satisfaction of the Director of DPW and/or PDS. The SWMA shall be signed and notarized by the applicant and recorded by the County.

**DOCUMENTATION:** The applicant shall process the agreement forms with [*PDS, LDR*] and pay any deposit and applicable review fees. **TIMING:** Prior to approval of any grading or improvement plan or construction permit, prior to use of the property in reliance of this permit; execution of the recorded agreements and securities shall be completed. **MONITORING:** The [*PDS, LDR*] shall review the agreements/mechanisms for consistency with the condition and County Standards.

11. **STRMWTR#2–EROSION CONTROL**

**INTENT:** In order to Comply with all applicable stormwater regulations the activities proposed under this application are subject to enforcement under permits from the State Construction General Permit, Order No. 2022-0057-DWQ, or subsequent order and the County Watershed Protection Ordinance (WPO) No.10410, County Code Section 67.801 et. seq., and all other applicable ordinances and standards for this priority project.

**DESCRIPTION OF REQUIREMENT:** The applicant shall maintain the appropriate on-site and offsite Best Management Practices pursuant to the approved Stormwater Quality Management Plan (SWQMP) and Erosion Control Plan including, but not limited to the erosion control measures, irrigation systems, slope protection, drainage systems, desilting basins, energy dissipators, and silt control measure. **DOCUMENTATION:** The applicant shall process an Erosion Control Plan and provide the letter of agreement and any additional security and/or cash deposit to the [*PDS, LDR*]. **TIMING:** Prior to approval of any grading or improvement plan or construction permit, and prior to use of the property in reliance of this permit, the Erosion Control Plan shall be approved and the agreement and securities shall be executed. **MONITORING:** The [*PDS, LDR*] shall ensure that the Erosion Control Plan adequately satisfies the requirements of the conditions to potentially perform the required erosion control and stormwater control measures proposed on all construction and grading plans. [*DPW, PDCI*] shall use the securities pursuant to the agreement to implement and enforce the required stormwater and erosion control measures pursuant to this condition during all construction phases as long as there are open and valid permits for the site.

**BUILDING PERMIT:** *(Prior to approval of any building plan and the issuance of any building permit).*

12. **ROADS#6–ANNEX TO LIGHTING DISTRICT**

**INTENT:** In order to promote orderly development and to comply with the Street Lighting Requirements of the Centerline Ordinance (County Code Section 51.301 et seq.), County of San Diego Board Policy I-18 and the County of San Diego Public Road Standards, the property shall transfer into the Lighting District. **DESCRIPTION OF REQUIREMENT:** Allow the transfer of the property subject of this permit into Zone A of the San Diego County Street Lighting District without notice or hearing, and pay the cost to process such transfer. **DOCUMENTATION:** The applicant shall pay the Zone A Lighting District Annexation Fee at the [PDS, LDR]. The applicant shall provide the receipt to [PDS, PCC]. **TIMING:** Prior to building permit issuance, or use in the premises in reliance of this permit, the fee shall be paid. **MONITORING:** The [PDS, LDR] shall calculate the fee pursuant to this condition and provide a receipt of payment for the applicant.

**OCCUPANCY:** *(Prior to any occupancy, final grading release, or use of the premises in reliance of this permit).*

13. **GEN#3–INSPECTION FEE**

**INTENT:** In order to comply with Zoning Ordinance Section 7362.e the inspection fee shall be paid. **DESCRIPTION OF REQUIREMENT:** Pay the inspection fee at the [PDS, ZC] to cover the cost of inspection(s) of the property to monitor ongoing conditions associated with this permit. In addition, submit a letter indicating who should be contacted to schedule the inspection. **DOCUMENTATION:** The applicant shall provide a receipt showing that the inspection fee has been paid along with updated contact information [PDS, PCC]. **TIMING:** Prior to any approval of any building plan, issuance of any building permit, and/or final sign off associated with the structures referenced above, compliance with this condition is required. **MONITORING:** The [PDS, ZC] shall process an invoice and collect the fee. PDS will schedule an inspection within one year from the date that occupancy or use of the site was established.

14. **PLN#2–SITE PLAN IMPLEMENTATION**

**INTENT:** In order to comply with the approved project design indicated on the approved plot plan, the project shall be constructed as indicated on the approved building and construction plans. **DESCRIPTION OF REQUIREMENT:** The site shall conform to the approved plot plan and the building plans. This includes, but is not limited to: installing all required design features, and all temporary construction facilities have been removed from the site. **DOCUMENTATION:** The applicant shall ensure that the site conforms to the approved plot plan and building plans. **TIMING:** Prior to any occupancy, final grading release, or use of the premises in reliance of this permit, the site shall conform to the approved plans. **MONITORING:** The [PDS, BI] and [DPR TC, PP] shall inspect the site for compliance with the approved Building Plans.

15. **LNDSCP#2–CERTIFICATION OF INSTALLATION**

**INTENT:** In order to provide adequate Landscaping that addresses screening, and to comply with the [COSD Water Efficient Landscape Design Manual](#), the [COSD Water Conservation in Landscaping Ordinance](#), the [COSD Off-Street Parking Design Manual](#),

the COSD Grading ordinance, the Alpine Design Guidelines, and the requirements of the S Designator, all landscaping shall be installed. **DESCRIPTION OF REQUIREMENT:** All of the landscaping shall be installed pursuant to the approved Landscape Documentation Package. This does not supersede any erosion control plantings that may be applied pursuant to [Section 87.417 and 87.418 of the County Grading Ordinance](#). These areas may be overlapping, but any requirements of a grading plan shall be complied with separately. The installation of the landscaping can be phased pursuant to construction of specific buildings or phases to the satisfaction of the [PDS, LA, PCC] [DPR, TC, PP]. **DOCUMENTATION:** The applicant shall submit to the [PDS LA, PCC], a Landscape Certificate of Completion from the project California licensed Landscape Architect, Architect, or Civil Engineer, that all landscaping has been installed as shown on the approved Landscape Documentation Package. The applicant shall prepare the Landscape Certificate of Completion using the Landscape Certificate of Completion Checklist, PDS Form #406. **TIMING:** Prior to any occupancy, final grading release, or use of the premises in reliance of this permit, the landscaping shall be installed. **MONITORING:** The [PDS, LA] shall verify the landscape installation upon notification of occupancy or use of the property, and notify the [PDS, PCC] [DPR, TC, PP] of compliance with the approved Landscape Documentation Package.

#### 16. **ROADS#7–SIGHT DISTANCE**

**INTENT:** In order to provide an unobstructed view for safety while exiting the property and accessing a public road from the site, and to comply with the Design Standards of Section 6.1.E of the County of San Diego Public Road Standards, an unobstructed sight distance shall be verified. **DESCRIPTION OF REQUIREMENT:**

- a. A registered civil engineer or a licensed land surveyor provides a certified signed statement that: "There is \_\_\_\_\_feet of unobstructed intersectional sight distance in both direction(s) from the proposed driveway along **Tavern Road** in accordance with the methodology described in Table 5 of the March 2012 County of San Diego Public Road Standards. These sight distances exceed the required intersectional Sight Distance requirements of \_\_\_\_\_ as described in Table 5 based on a speed of \_\_\_\_\_, which I have verified to be the higher of the prevailing speed or the minimum design speed of the road classification. I have exercised responsible charge for the certification as defined in Section 6703 of the Professional Engineers Act of the California Business and Professions Code."
- b. If the lines of sight fall within the existing public road right-of-way, the engineer or surveyor shall further certify: "Said lines of sight fall within the existing right-of-way and a clear space easement is not required."

**DOCUMENTATION:** The applicant shall complete the certifications and submit them to the [PDS, LDR] for review. **TIMING:** Prior to occupancy of the first structure built in association with this permit, or use of the premises in reliance of this permit, and annually after that until the project is completely built, the sight distance shall be verified. **MONITORING:** The [PDS, LDR] shall verify the sight distance certifications for compliance with this condition.

#### 17. **ROADS#8–INSTALL STREETLIGHTS**



**INTENT:** In order to promote orderly development and to comply with the Street Lighting Requirements of the Centerline Ordinance (County Code Section 51.301 et seq.), County of San Diego Board Policy I-18, streetlights shall be installed and energized. **DESCRIPTION OF REQUIREMENT:** Streetlights to be provided to the satisfaction of County of San Diego Public Roads Standard. Install or arrange to install streetlights to County standards and the satisfaction of the Director of PDS, and deposit with PDS, a cash deposit sufficient to energize and operate the streetlights until the property has been transferred into Zone A. **DOCUMENTATION:** The applicant shall pay the Zone A Lighting District Annexation Fee at the [PDS, LDR], and arrange for the installation and energizing of the streetlights. **TIMING:** Prior to occupancy of the first structure built in association with this permit, final grading release, or use in the premises in reliance of this permit, the streetlights shall be installed and all fees paid. **MONITORING:** The [PDS, LDR] shall calculate the fee pursuant to this condition and provide a receipt of payment for the applicant. The [PDS, LDR] shall ensure that the streetlights have been installed and all fees have been paid.

18. **STRMWTR#3–VERIFICATION OF STRUCTURAL BMPs**

**INTENT:** In order to promote orderly development and to comply with the County Watershed Protection Ordinance (WPO) No.10410 (County Code Section 67.801 et. seq.), verification of Structural BMPs shall be completed. **DESCRIPTION OF REQUIREMENT:** Complete a Structural BMP Verification Form as shown in Attachment 10 of the PDP SWQMP. **DOCUMENTATION:** The applicant shall process the Structural BMP Verification Forms with [DPW, PDCI] or [PDS, BLDG]. **TIMING:** Prior to any occupancy, final grading release, or use of the premises in reliance of this permit; execution of the Structural BMP Verification Form shall be completed. **MONITORING:** The [PDS, LDR] and [DPW, WPP] shall review the Structural BMP Verification Forms for consistency with the condition and County Standards.

19. **STRMWTR#4–PROVISION OF STORMWATER DOCUMENTATION TO PROPERTY OWNER**

**INTENT:** In order to promote orderly development and to comply with the County Watershed Protection Ordinance (WPO) No.10410 (County Code Section 67.801 et. seq.), stormwater documentation shall be provided to property owner. **DESCRIPTION OF REQUIREMENT:** Demonstrate that copies of the following documents are provided to the property owner and initial occupants:

- a. A copy of the project's approved SWQMP (with attached Operation & Maintenance Plan).
- b. A copy of project's recorded Maintenance Notification Agreement and/or Stormwater Facilities Maintenance Agreement and/or Private Road Maintenance Agreement showing the Structural BMPs pertaining to the property.
- c. Sample copies of the following:
  1. A Letter for Privately Owned Stormwater Treatment Control Best Management Practices Operation and Maintenance Verification.
  2. One Operation and Maintenance Verification Form for each type of Private Treatment Control BMP.

**DOCUMENTATION:** The applicant shall submit a letter stating that the above documentation has been submitted to the property owner and initial occupants. **TIMING:** Prior to any occupancy, final grading release, or use of the premises in reliance of this permit; provision of stormwater documents shall be completed. **MONITORING:** The [PDS, LDR] and [DPW, WPP] shall review the letter provided by the applicant for consistency with the condition and County Standards.

**ONGOING:** *(The following conditions shall apply during the term of this permit).*

20. **PLN#3–SITE CONFORMANCE**

**INTENT:** In order to comply with Zoning Ordinance Section 7703, the site shall substantially comply with the approved plot plans and all deviations thereof, specific conditions and approved building plans. **DESCRIPTION OF REQUIREMENT:** The project shall conform to the approved building plans, and plot plan(s). Failure to conform to the approved plot plan(s); is an unlawful use of the land, and will result in enforcement action pursuant to Zoning Ordinance Section 7703. **DOCUMENTATION:** The property owner and permittee shall conform to the approved plot plan. If the permittee or property owner chooses to change the site design in any way, they must obtain approval from the County for a Minor Deviation or a Modification pursuant to the County of San Diego Zoning Ordinance. **TIMING:** Upon establishment of the use, this condition shall apply for the duration of the term of this permit. **MONITORING:** The [PDS, Code Compliance Division] is responsible for enforcement of this permit.

21. **ROADS#9–SIGHT DISTANCE**

**INTENT:** In order to provide an unobstructed view for safety while exiting the property and accessing a public road from the site, and to comply with the Design Standards of Section 6.1.(E) of the County of San Diego Public Road Standards, an unobstructed sight distance shall be maintained for the life of this permit. **DESCRIPTION OF REQUIREMENT:** There shall be a minimum unobstructed sight distance of \_\_\_\_ feet in both directions along **Tavern Road** from the project driveway openings for the life of this permit. **DOCUMENTATION:** A minimum unobstructed sight shall be maintained. The sight distance of adjacent driveways and street openings shall not be adversely affected by this project at any time. **TIMING:** Upon establishment of the use, this condition shall apply for the duration of the term of this permit. **MONITORING:** The [PDS, Code Compliance Division] is responsible for compliance of this permit.

22. **STRMWTR#5–SELF-VERIFICATION OPERATION AND MAINTENANCE LETTER**

**INTENT:** In order to promote orderly development and to comply with the County Watershed Protection Ordinance (WPO) No.10410, County Code Section 67.801 et. seq., an operation and maintenance verification form for each Structural BMPs shall be completed. **DESCRIPTION OF REQUIREMENT:** Every year the property owner shall receive from the County a BMP Verification Form to be completed for each privately owned Structural BMP. **DOCUMENTATION:** Every year the property owner shall file with the County the completed Structural BMP Verification Form stating the maintenance performed during the reporting period for each privately owned Structural BMP with [DPW, WPP]. **TIMING:** Upon establishment of the use, this condition shall apply for the duration of the term of this permit. **MONITORING:** The [DPW, WPP] is responsible for compliance of this permit.

**ORDINANCE COMPLIANCE NOTIFICATIONS:** The project is subject to, but not limited to the following County of San Diego, State of California, and U.S. Federal Government, Ordinances, Permits, and Requirements:

**STORMWATER ORDINANCE COMPLIANCE:** In order to Comply with all applicable stormwater regulations the activities proposed under this application are subject to enforcement under permits from the [San Diego Regional Water Quality Control Board \(RWQCB\)](#) and the County of San Diego Watershed Protection, Stormwater Management, and Discharge Control [Ordinance No. 10410](#) and all other applicable ordinances and standards for the life of this permit. The project site shall be in compliance with all applicable stormwater regulations referenced above and all other applicable ordinances and standards. This includes compliance with the approved Stormwater Quality Management Plan (SWQMP), all requirements for Low Impact Development (LID), Hydromodification, materials and wastes control, erosion control, and sediment control on the project site. Projects that involve areas 1 acre or greater require that the property owner keep additional and updated information onsite concerning stormwater runoff. The property owner and permittee shall comply with the requirements of the stormwater regulations referenced above.

**LOW IMPACT DEVELOPMENT NOTICE:** The San Diego Regional Water Quality Control Board (SDRWQCB) issued a new Municipal Stormwater Permit under the National Pollutant Discharge Elimination System (NPDES). The requirements of the Municipal Permit were implemented beginning in May 2013 and amended in November 2015. *Project design shall comply with the new Municipal Permit regulations.* The County has provided a Low Impact Development (LID) Handbook as a source for LID information to be utilized by County staff and outside consultants for implementing LID in our region. The LID Best Management Practices (BMP) Requirements of the Municipal Permit can be found at the following link:

[https://www.sandiegocounty.gov/content/dam/sdc/dpw/WATERSHED\\_PROTECTION\\_PROGRAM/susmpps/lid\\_handbook\\_2014sm.pdf](https://www.sandiegocounty.gov/content/dam/sdc/dpw/WATERSHED_PROTECTION_PROGRAM/susmpps/lid_handbook_2014sm.pdf)

**STORMWATER COMPLIANCE NOTICE:** Updated studies, including Hydro-modification Management Plans for Priority Development Projects, will be required prior to approval of grading and improvement plans for construction pursuant to County of San Diego Watershed Protection, Stormwater Management and Discharge Control Ordinance No. 10410 (N.S.), dated February 26, 2016 and BMP Design Manual. These requirements are subject to periodic adjustment as changes are made to the National Pollutant Discharge Elimination System (NPDES) Permit and Waste Discharge Requirements imposed by the San Diego Regional Water Quality Control Board (Regional Board) on discharges from municipal separate storm sewer systems (MS4). The new MS4 Permit was adopted by the Regional Board on May 8, 2013 and amended on November 18, 2015. The County has begun the process of amending ordinances and taking other action to implement the new MS4 Permit. Additional studies and other action may be needed to comply with the new and future MS4 Permits.

**DRAINAGE:** The project shall be in compliance with the County of San Diego [Flood Damage Prevention Ordinance](#) No. 10091, adopted December 8, 2010.

**GRADING PERMIT REQUIRED:** A grading permit is required prior to commencement of grading when quantities of excavation or fill results in the movement of material exceeding 200

cubic yards or eight feet (8') in vertical height of cut/fill, pursuant to [Section 87.201 of Grading Ordinance](#).

**CONSTRUCTION PERMIT REQUIRED:** A Construction Permit and/or Encroachment Permit are required for any and all work within the County road right-of-way. Contact DPW Construction/Road right-of-way Permits Services Section, (858) 694-3275, to coordinate departmental requirements. In addition, before trimming, removing or planting trees or shrubs in the County Road right-of-way, the applicant must first obtain a permit to remove plant or trim shrubs or trees from the Permit Services Section.

## MAJOR USE PERMIT FINDINGS FOR PDS2022-MUP-22-008

Pursuant to Section 7358 of The Zoning Ordinance, the following findings in support of the granting of the Major Use Permit Modification are made:

(a) *The location, size, design, and operating characteristics of the proposed use will be compatible with adjacent uses, residents, buildings, or structures with consideration given to*

1. *Harmony in scale, bulk, coverage, and density*

The proposed project meets the required findings for a Major Use Permit (MUP) regarding scale, bulk, and coverage by blending well with the surrounding uses. The 0.6-acre site aligns with adjacent commercial parcels, which range from 0.5 to three acres, ensuring compatibility in size. The 3,000-square-foot structure represents a modest ten percent lot coverage, complemented by driveways, parking, and landscaping that further integrate it into the area.

Though the development footprint is smaller than the previously approved MUP for a Car Wash and Lube Facility (PDS2012-3300-12-019), the project maintains consistency with existing commercial patterns. The facility will operate from 7 a.m. to 7 p.m., with an additional hour for opening and closing, and is expected to process approximately 20 to 30 cars per hour. These operational elements align with typical commercial activity, ensuring minimal disruption to neighboring properties.

By maintaining appropriate scale and lot coverage while ensuring functional compatibility, the project supports responsible land use and complements the existing development framework.

The project does not have a residential component subject to density regulations.

2. *The availability of public facilities, services, and utilities*

Service availability letters from the Alpine Fire Protection District, Padre Dam Municipal Water District, and San Diego County Sanitation District confirm that all necessary public services and utilities are in place. Electrical and telephone

services are available on-site. As a result, the project has access to all required utilities to support its development.

3. *The harmful effect, if any, upon desirable neighborhood character:*

The proposed project is designed to complement the existing community through thoughtful architectural integration and adherence to the Alpine Design Guidelines. The inclusion of an articulated facade, a cohesive color palette, and a mix of stone and wood-paneled siding ensures visual consistency with surrounding commercial structures, maintaining the area's established character.

Landscaping is proposed to further enhance compatibility, softening the project's presence and reinforcing its alignment with local development patterns. The design prevents stark contrasts, supporting a cohesive streetscape and preserving community identity. Additionally, the project contributes to responsible land use and local economic growth. It provides a needed service while minimizing infrastructure strain, ensuring smooth traffic flow, and mitigating noise impacts per County regulations. The development footprint is appropriately scaled to avoid congestion and excessive bulk, aligning with the character of the surrounding commercial district.

4. *The generation of traffic and the capacity and physical character of surrounding streets:*

The proposed project aligns with the capacity and function of Tavern Road, a designated mobility element road designed to accommodate commercial traffic. To enhance safety and efficiency, the project includes re-striping to restrict left turns, minimizing congestion and improving traffic flow. The surrounding streets provide adequate connectivity, ensuring smooth circulation without adverse impacts. Acoustical analysis confirms compliance with the County's Noise Ordinance, with noise impacts deemed less than significant. Overall, the project integrates well within the existing transportation network while prioritizing safety and efficiency.

5. *The suitability of the site for the type and intensity of use or development, which is proposed:*

Historically, the site has been used for commercial purposes, with its previous function as a gas station and lube/repair facility until its removal in 2003. The proposed development continues this commercial tradition while modernizing the space in a way that aligns with current community standards. The surrounding area features a range of similarly sized parcels supporting various commercial operations, including a gas station, pharmacy, convenience store, fast food restaurants, a grocery store, and a hardware store. This diversity of uses underscores the project's compatibility with the existing commercial landscape.

By maintaining a community consistent design aesthetic, implementing traffic management measures, and ensuring minimal environmental impact, the project contributes to the evolving character of the community while preserving its established commercial identity

6. *Any other relevant impact of the proposed use:*

None identified.

(b) *The impacts, as described in Findings (a) above, and the location of the proposed use will be consistent with the San Diego County General Plan:*

The project site is subject to the Land Use Element, Regional Category Village and General Plan Use Designation General Commercial (C-1). The Village category identifies areas where a higher intensity and wide range of land uses are established or planned. Typically, Village areas function as the center of community planning areas and contain the highest population and development densities. The General Commercial designation provides for areas where a wide range of retail activities and services are permitted. The project is located in the General Commercial (C36) Use Regulations zone which allows commercial sales and services generally conducted within an enclosed building, and car washes with a Major Use Permit pursuant to Zoning Ordinance Section 5760. The proposed use would blend with the surrounding area and include adequate access, appropriate off-street parking, internal circulation, setbacks, and landscaping. Therefore, the project is consistent with the San Diego County General Plan and the Alpine Community Plan

(c) *That the requirements of the California Environmental Quality Act have been complied with:*

The project has been reviewed for compliance with the California Environmental Quality Act (CEQA) and qualifies for a Categorical Exemption under CEQA Section 15303. Section 15303 includes the construction and location of limited numbers of new, small facilities or structures. Up to four commercial buildings not exceeding 10,000 square feet in floor area are allowed under this exemption on sites zoned for such use if not involving the use of significant amounts of hazardous substances where all necessary public services and facilities are available and the surrounding area is not environmentally sensitive. The project includes one building which has a floor area of 3,500 square feet (excludes second floor area). Both the car wash and lube and oil facilities will not have significant amounts of hazardous substances on site. All public facilities are available and the surrounding area is not environmentally sensitive..

**ORDINANCE COMPLIANCE NOTIFICATIONS:** The project is subject to, but not limited to, the following County of San Diego, State of California, and U.S. Federal Government, Ordinances, Permits, and Requirements:

**LIGHTING ORDINANCE COMPLIANCE:** In order to comply with the County Lighting Ordinance 59.101 et seq. and Zoning Ordinance Sections 6322, 6324, and 6326, the onsite lighting shall comply with the approved plot plan(s), specific permit conditions and approved building plans associated with this permit. All light fixtures shall be designed and adjusted to reflect light downward, away from any road or street, and away from adjoining premises, and shall otherwise conform to the County Lighting Ordinance 59.101 et seq. and Zoning Ordinance Sections 6322, and 6324. The property owner and permittee shall conform to the approved plot plan(s), specific permit conditions, and approved building plans associated with this permit as they pertain to



lighting. No additional lighting is permitted. If the permittee or property owner chooses to change the site design in any way, they must obtain approval from the County for a Minor Deviation or a Modification pursuant to the County of San Diego Zoning Ordinance.

**NOISE ORDINANCE COMPLIANCE:** In order to comply with the [County Noise Ordinance 36.401](#) et seq. and the Noise Standards pursuant to the General Plan Noise Element (Table N-1 & N-2), the property and all of its uses shall comply with the approved plot plans, specific permit conditions and approved building plans associated with this permit. No noise generating equipment and project related noise sources shall produce noise levels in violation of the County Noise Ordinance. The property owner and permittee shall conform to the approved plot plan(s), specific permit conditions, and approved building plans associated with this permit as they pertain to noise generating devices or activities. If the permittee or property owner chooses to change the site design in any way, they must obtain approval from the County for a Minor Deviation or a Modification pursuant to the County of San Diego Zoning Ordinance.

**COMPLIANCE INSPECTION:** In order to comply with Zoning Ordinance Section 7362.e the County shall inspect the Use Permit property for compliance with the terms of this Use Permit. The County Permit Compliance Officer will perform a site inspection and review the on-going conditions associated with this permit. The inspection shall be scheduled no later than the six months subsequent to establishing the intended use of the permit. If the County determines the applicant is not complying with the Major Use Permit terms and conditions the applicant shall allow the County to conduct follow up inspections more frequently than once every twelve months until the County determines the applicant is in compliance. The Property Owner/Permittee shall allow the County to inspect the property for which the Major Use Permit has been granted, at least once every twelve months, to determine if the Property Owner/Permittee is complying with all terms and conditions of the Use Permit. This requirement shall apply during the term of this permit.

**STORMWATER ORDINANCE COMPLIANCE:** In order to Comply with all applicable stormwater regulations the activities proposed under this application are subject to enforcement under permits from the [San Diego Regional Water Quality Control Board \(RWQCB\)](#) and the County of San Diego Watershed Protection, Stormwater Management, and Discharge Control [Ordinance No. 10410](#) and all other applicable ordinances and standards for the life of this permit. The project site shall be in compliance with all applicable stormwater regulations referenced above and all other applicable ordinances and standards. This includes compliance with the approved Stormwater Management Plan, all requirements for Low Impact Development (LID), Hydromodification, materials and wastes control, erosion control, and sediment control on the project site. Projects that involve areas 1 acre or greater require that during construction the property owner keeps the Storm Water Pollution Prevention Plan (SWPPP) onsite and update it as needed. The property owner and permittee shall comply with the requirements of the stormwater regulations referenced above.

**LOW IMPACT DEVELOPMENT NOTICE:** The San Diego Regional Water Quality Control Board (SDRWQCB) issued a new Municipal Stormwater Permit under the National Pollutant Discharge Elimination System (NPDES). The requirements of the Municipal Permit were implemented beginning in May 2013. *Project design shall be in compliance with the new Municipal Permit regulations.* The Low Impact Development (LID) Best Management Practices (BMP) Requirements of the Municipal Permit can be found at the following link:

[http://www.sandiegocounty.gov/content/dam/sdc/dpw/WATERSHED\\_PROTECTION\\_PROGRAM/susmppdf/lid\\_handbook\\_2014sm.pdf](http://www.sandiegocounty.gov/content/dam/sdc/dpw/WATERSHED_PROTECTION_PROGRAM/susmppdf/lid_handbook_2014sm.pdf)

The County has provided a LID Handbook as a source for LID information and is to be utilized by County staff and outside consultants for implementing LID in our region. See link below.  
<http://www.sdcounty.ca.gov/dplu/docs/LID-Handbook.pdf>

**STORMWATER COMPLIANCE NOTICE:** Updated studies, including Hydro-modification Management Plans for Priority Development Projects, will be required prior to approval of grading and improvement plans for construction pursuant to [County of San Diego Watershed Protection, Stormwater Management and Discharge Control Ordinance No. 10410](#) (N.S.), dated February 26, 2016 and BMP Design Manual. These requirements are subject to the MS4 Permit issued by the Regional Water Quality Control Board, Order No. R9-2013-0001 and any subsequent order. Additional studies and other action may be needed to comply with future MS4 Permits.

**GRADING PERMIT REQUIRED:** A grading permit is required prior to commencement of grading when quantities of excavation or fill results in the movement of material exceeding 200 cubic yards or eight feet (8') in vertical height of cut/fill, pursuant to [Section 87.201 of Grading Ordinance](#).

**CONSTRUCTION PERMIT REQUIRED:** A Construction Permit and/or Encroachment Permit are required for any and all work within the County road right-of-way. Contact PDS Construction/Road right-of-way Permits Services Section, (858) 694-3275, to coordinate County requirements. In addition, before trimming, removing or planting trees or shrubs in the County Road right-of-way, the applicant must first obtain a permit to remove plant or trim shrubs or trees from the Permit Services Section.

**ENCROACHMENT PERMIT REQUIRED:** An Encroachment Permit is required for any and all proposed/existing facilities within the County right-of-way. At the time of construction of future road improvements, the proposed facilities shall be relocated at no cost to the County, to the satisfaction of the Director of Public Works.

**EXCAVATION PERMIT REQUIRED:** An excavation permit is required for undergrounding and/or relocation of utilities within the County right-of-way.

**TRANSPORTATION IMPACT FEE:** The project is subject to County of San Diego Transportation Impact Fee (TIF) pursuant to [County TIF Ordinance number 77.201 – 77.223](#). The Transportation Impact Fee (TIF) shall be paid. The fee is required for the entire project, or it can be paid at building permit issuance for each phase of the project. The fee is calculated pursuant to the ordinance at the time of building permit issuance. The applicant shall pay the TIF at the [PDS, LD Counter] and provide a copy of the receipt to the [PDS, BD] at time of permit issuance.

<b>EXPLANATION OF COUNTY DEPARTMENT AND DIVISION ACRONYMS</b>			
<b>Planning &amp; Development Services (PDS)</b>			
Project Planning Division	PPD	Land Development Project Review Teams	LDR
Permit Compliance Coordinator	PCC	Project Manager	PM
Building Plan Process Review	BPPR	Plan Checker	PC
Building Division	BD	Map Checker	MC
Building Inspector	BI	Landscape Architect	LA
Zoning Counter	ZO		
<b>Department of Public Works (DPW)</b>			
Private Development Construction Inspection	PDCI	Environmental Services Unit Division	ESU
<b>Department of Environmental Health and Quality (DEHQ)</b>			
Land and Water Quality Division	LWQ	Local Enforcement Agency	LEA
Vector Control	VCT	Hazmat Division	HMD
<b>Department of Parks and Recreation (DPR)</b>			
Trails Coordinator	TC	Group Program Manager	GPM
Parks Planner	PP		
<b>Department of General Service (DGS)</b>			
Real Property Division	RP		

**APPEAL PROCEDURE:** Within ten calendar days after the date of this Decision of the Planning Commission, the decision may be appealed to the Board of Supervisors in accordance with [Section 7366 of the County Zoning Ordinance](#). An appeal shall be filed with the Director of Planning & Development Services or by mail with the Secretary of the Planning Commission within TEN CALENDAR DAYS of the date of this notice AND MUST BE ACCOMPANIED BY THE DEPOSIT OR FEE AS PRESCRIBED IN THE DEPARTMENT'S FEE SCHEDULE, PDS FORM #369, pursuant to Section 362 of the San Diego County Administrative Code. If the tenth day falls on a weekend or County holiday, an appeal will be accepted until 4:00 p.m. on the following day the County is open for business. Filing of an appeal will stay the decision of the Director until a hearing on your application is held and action is taken by the Planning Commission. Furthermore, the 90-day period in which the applicant may file a protest of the fees, dedications or exactions begins on the date of approval of this Decision.

COUNTY OF SAN DIEGO PLANNING COMMISSION  
VINCE NICOLETTI, DIRECTOR

BY:

Mark Slovick, Deputy Director  
Project Planning & Land Development Division  
Planning & Development Services

email cc:

Dag Bunnemeyer, Group Program Manager, PDS  
Aidan Pulley, Project Manager, PDS  
Jennilyn Gonzales, Land Development, PDS  
Taylor Ryan, Land Development, PDS  
Terry Strom, Applicant Team

## **Attachment C – Environmental Documentation**

# 1 - 51 NOTICE OF EXEMPTION

TO: Recorder/County Clerk  
1600 Pacific Highway, M.S. A33  
San Diego, CA 92101

FROM: County of San Diego  
Planning & Development Services, M.S. O650  
Attn: Project Planning Division Section Secretary

SUBJECT: **FILING OF NOTICE OF EXEMPTION IN COMPLIANCE WITH PUBLIC RESOURCES CODE SECTION 21108 OR 21152**

Project Name: PDS2022-MUP-22-008 – Alpine Car Wash

Project Location: 1250 Tavern Road, Alpine unincorporated San Diego County (APN: 403-390-27-00)

Project Applicant: Linus Klein: 619-254-9648  
10055 Grandview Drive, La Mesa CA 91941

Project Description: The applicant requests a Major Use Permit to build and operate a new 3,000 square foot express car wash facility in the Alpine Community Planning Area. This project falls under the General Commercial (C-1) General Plan Land Use Designation and is zoned General Commercial (C36), which permits a car wash facility with the approval of a Major Use Permit according to Sections 1430.a, 2365.c, and 7358 of the Zoning Ordinance. Access to the site would be provided by Tavern Road, a County maintained road. The project would be served by the Padre Dam Municipal Water District and sewer by the San Diego County Sanitation District.

Agency Approving Project: County of San Diego

County Contact Person: Aidan Pulley Telephone: 619-972-8234

Date Form Completed: July 9, 2025

This is to advise that the County of San Diego Planning & Development Services has approved the above described project on July 9, 2025 and found the project to be exempt from the CEQA under the following criteria:

- Exempt status and applicable section of the CEQA ("C") and/or State CEQA Guidelines ("G"): (check only one)
  - ☐ Declared Emergency [C 21080(b)(3); G 15269(a)]
  - ☐ Emergency Project [C 21080(b)(4); G 15269(b)(c)]
  - ☐ Statutory Exemption. C Section:
  - ☒ Categorical Exemption. G Section: 15303
  - ☐ G 15061(b)(3) - It can be seen with certainty that there is no possibility that the activity in question may have a significant effect on the environment and the activity is not subject to the CEQA.
  - ☐ G 15182 – Residential Projects Pursuant to a Specific Plan
  - ☐ G 15183 – Projects Consistent with a Community Plan, General Plan, or Zoning
  - ☐ Activity is exempt from the CEQA because it is not a project as defined in Section 15378.
- Mitigation measures ☐ were ☒ were not made a condition of the approval of the project.
- A Mitigation reporting or monitoring plan ☐ was ☒ was not adopted for this project.

Statement of reasons why project is exempt: Pursuant to Section 15303, the project is exempt from CEQA because of the project consists of construction and location of limited numbers of new, small facilities or structures; installation of small new equipment and facilities in small structures; and the conversion of existing small structures from one use to another where only minor modifications are made in the exterior of the structure. It has been determined that the project is not in an environmentally sensitive location; will not have a cumulative effect on the environment; is not on a hazardous waste site; will not cause a substantial change in the significance of a historical resource; and will not result in damage to a scenic highway.

The following is to be filled in only upon formal project approval by the appropriate County of San Diego decision-making body.

Signature: Aidan Pulley Telephone: (619) 972-8234

Name (Print): Aidan Pulley Title: Land Use/Environmental Planner

This Notice of Exemption has been signed and filed by the County of San Diego.

This notice must be filed with the Recorder/County Clerk as soon as possible after project approval by the decision-making body. The Recorder/County Clerk must post this notice within 24 hours of receipt and for a period of not less than 30 days. At the termination of the posting period, the Recorder/County Clerk must return this notice to the Department address listed above along with evidence of the posting period. The originating Department must then retain the returned notice for a period of not less than twelve months. Reference: CEQA Guidelines Section 15062.



## REVIEW FOR APPLICABILITY OF/COMPLIANCE WITH ORDINANCES/POLICIES

### FOR PURPOSES OF CONSIDERATION OF

**Alpine Carwash Facility MUP,  
PDS2022-MUP-22-008; PDS2022-ER-22-03-001**

**July 9, 2025**

**I. HABITAT LOSS PERMIT ORDINANCE** – Does the proposed project conform to the Habitat Loss Permit/Coastal Sage Scrub Ordinance findings?

YES  
☐

NO  
☐

NOT APPLICABLE/EXEMPT  
☒

The proposed project and any off site improvements are located within the boundaries of the Multiple Species Conservation Program. Therefore, conformance to the Habitat Loss Permit/Coastal Sage Scrub Ordinance findings is not required.

**II. MSCP/BMO** - Does the proposed project conform to the Multiple Species Conservation Program (MSCP) and Biological Mitigation Ordinance (BMO)?

YES  
☐

NO  
☐

NOT APPLICABLE/EXEMPT  
☒

The project proposes the construction of a carwash. The project site is located within the Metro-Lakeside-Jamul segment of the County's Multiple Species Conservation Program.

Staff has determined the proposed project is exempt from the California Environmental Quality Act pursuant to CEQA Section 15303. Projects found to be exempt from CEQA are also exempt from the County's Biological Mitigation Ordinance (Section 86.503 (a)(1)). Therefore, the project is not subject to the requirements of the Biological Mitigation Ordinance. However, no project within the MSCP County Subarea Plan, regardless of exemption status, may conflict or otherwise hinder the MSCP preserve system.

The project is consistent with the MSCP Subarea Plan due to the following:

- The site does not support sensitive habitat or wildlife.
- Surrounding land uses include dense development.
- There are other conditions that would restrict wildlife use of the area for nesting, foraging or dispersal.
- The site is not within a core, linkage, Pre-Approved Mitigation Area, Preserve Area, or other highly sensitive area as designated by the MSCP.
- The site does not support any features that might encourage wildlife movement, such as a well-vegetated drainage, stream, or creek.

Based on the above facts, staff has determined that the proposed project will not hinder or conflict with the County Subarea Plan. No take authorization for incidental or deliberate impacts to state or federally listed species is granted with this determination. While no impacts to listed species are anticipated based on staff's review of the project, the applicant is responsible for ensuring that none occur and/or appropriate authorization has been obtained.

**III. GROUNDWATER ORDINANCE** - Does the project comply with the requirements of the San Diego County Groundwater Ordinance?

YES

☐

NO

☐

NOT APPLICABLE/EXEMPT

☒

The project will obtain its water supply from the Padre Dam Water District which obtains water from surface reservoirs and/or imported sources. The project will not use any groundwater for any purpose, including irrigation or domestic supply.

**IV. RESOURCE PROTECTION ORDINANCE** - Does the project comply with:

The wetland and wetland buffer regulations (Sections 86.604(a) and (b)) of the Resource Protection Ordinance?

YES

☐

NO

☐

NOT APPLICABLE/EXEMPT

☒

The Floodways and Floodplain Fringe section (Sections 86.604(c) and (d)) of the Resource Protection Ordinance?

YES

☐

NO

☐

NOT APPLICABLE/EXEMPT

☒

The [Steep Slope](#) section (Section 86.604(e))?

YES

☐

NO

☐

NOT APPLICABLE/EXEMPT

☒

The Sensitive Habitat Lands section (Section 86.604(f)) of the Resource Protection Ordinance?

YES

☐

NO

☐

NOT APPLICABLE/EXEMPT

☒

The Significant Prehistoric and Historic Sites section (Section 86.604(g)) of the Resource Protection Ordinance?

YES

☐

NO

☐

NOT APPLICABLE/EXEMPT

☒

***Wetland and Wetland Buffers:***

The site contains no wetland habitats as defined by the San Diego County Resource Protection Ordinance. The site does not have a substratum of predominately undrained hydric soils, the land does not support, even periodically, hydric plants, nor does the site have a substratum that is non-soil and is saturated with water or covered by water at some time during the growing season of each year, and the project does not propose any change from the existing site and use. Therefore, it has been found that the proposed project complies with the Resource Protection Ordinance.

***Floodways and Floodplain Fringe:***

The project is not located near any floodway or floodplain fringe area as defined in the resource protection ordinance, nor is it near a watercourse plotted on any official County

floodway or floodplain map. Therefore, it has been found that the proposed project complies with the Resource Protection Ordinance.

***Steep Slopes:***

The average slope for the property is less than 25 percent gradient. Slopes with a gradient of 25 percent or greater and 50 feet or higher in vertical height are required to be placed in open space easements by the San Diego County Resource Protection Ordinance (RPO). There are no steep slopes on the property. Therefore, it has been found that the proposed project complies with Sections 86.604(e) of the RPO.

***Sensitive Habitats:***

Sensitive habitat lands include unique vegetation communities and/or habitat that is either necessary to support a viable population of sensitive species, is critical to the proper functioning of a balanced natural ecosystem, or which serves as a functioning wildlife corridor. No sensitive habitat lands are identified on or directly adjacent to the proposed carwash facility. Therefore, it has been found that the proposed project complies with Section 86.604(f) of the Resource Protection Ordinance.

***Significant Prehistoric and Historic Sites:***

The property has been surveyed and negative findings for cultural and historical resources were prepared for the Project site. Testing and other investigation determined the archaeological site does not meet the definition of significant site in a cultural resources report titled, "Cultural Resources Survey Report for the Alpine Car Wash Project," dated August 2023. The site does not need to be preserved under the Resource Protection Ordinance. It has been determined that there are no impacts to historical resources because they do not occur within the project site. Therefore, it has been found that the proposed project complies with Section 86.604(g) of the RPO.

**V. STORMWATER ORDINANCE (WPO)** - Does the project comply with the County of San Diego Watershed Protection, Stormwater Management and Discharge Control Ordinance (WPO)?

YES  
☒

NO  
☐

NOT APPLICABLE  
☐

The project Storm Water Quality Management Plan has been reviewed and is found to be complete and in compliance with the WPO.

**VI. NOISE ORDINANCE** – Does the project comply with the County of San Diego Noise Element of the General Plan and the County of San Diego Noise Ordinance?

YES  
☒

NO  
☐

NOT APPLICABLE  
☐

The proposal would not expose people to nor generate potentially significant noise levels which exceed the allowable limits of the County of San Diego Noise Element of the General Plan, County of San Diego Noise Ordinance, and other applicable local, State, and Federal noise control regulations.

Potential noise sources from the Project include car wash equipment and temporary construction noise. The applicable noise limit for the project is a one-hour average of 60 dBA from 7:00 a.m. to 10:00 p.m. for commercial zones. The Project would not operate during nighttime hours. Operational noise, including high volume air blowers, would be consistent with the Noise Ordinance. Any Construction noise for the proposed Project would not exceed 75 dBA at the property line during an eight-hour period between 7 a.m. to 7 p.m. In addition, the operation of construction equipment between 7 pm and 7 am is restricted and no work shall be done on Sundays and Holidays. The anticipated construction fleet would not exceed 75 dBA at the nearest sensitive receptors and would not exceed the construction noise level limit. Therefore, project-generated noise would be less than significant.

## **Attachment D – Environmental Findings**

**Alpine Carwash Facility MUP  
MAJOR USE PERMIT  
PERMIT NO.: PDS2022-MUP-22-008  
ENVIRONMENTAL LOG: PDS2022-ER-22-03-001**

**ENVIRONMENTAL FINDINGS**

July 9, 2025

1. Find that the proposed project is exempt from the California Environmental Quality Act (CEQA) pursuant to State CEQA Guidelines Section 15303 for the reasons stated in the Notice of Exemption.
2. Find that the proposed project is consistent with the Resource Protection Ordinance (RPO) (County Code, Section 86.601 et seq.).
3. Find that plans and documentation have been prepared for the proposed project that demonstrate that the project complies with the Watershed Protection, Stormwater Management and Discharge Control Ordinance (County Code, section 67.801 et seq.).

## **Attachment E – Public Documentation**



# Alpine Community Planning Group

P.O. Box 1419, Alpine, CA 91903-1419

## FINAL MEETING MINUTES

Thursday, December 7, 2023 at 6:00pm

Alpine Community Center | 1830 Alpine Boulevard, Alpine, CA 91901

Archived Agendas & Minutes – <http://www.sandiegocounty.gov/content/sdc/pds/gpupdate/comm/alpine.html>

### Group Member Roster

Travis Lyon  
Chair

[travislyonacpg@gmail.com](mailto:travislyonacpg@gmail.com)

Richard Saldano  
Vice Chair

[rsaldano@contelproject.com](mailto:rsaldano@contelproject.com)

Erika Taylor Simmons  
Secretary

[taylor.erika@gmail.com](mailto:taylor.erika@gmail.com)

Amy Blake  
[aebake80@gmail.com](mailto:aebake80@gmail.com)

Mary Kay Borchard  
[mktbinalpine@gmail.com](mailto:mktbinalpine@gmail.com)

Kim Brown  
[dawson\\_kim215@yahoo.com](mailto:dawson_kim215@yahoo.com)

Darlene Cossio  
[darlenecossio@gmail.com](mailto:darlenecossio@gmail.com)

Mary Davis  
[marydavisacpg@gmail.com](mailto:marydavisacpg@gmail.com)

Roger Garay  
[rogergaray1940@gmail.com](mailto:rogergaray1940@gmail.com)

Mike Milligan  
[michaelmilligan314@yahoo.com](mailto:michaelmilligan314@yahoo.com)

Angela May  
[angiemay@alpinecommunitynetwork.com](mailto:angiemay@alpinecommunitynetwork.com)

Sarah Reimund  
[reimundscience@gmail.com](mailto:reimundscience@gmail.com)

Colby Ross  
[colby.ross13@yahoo.com](mailto:colby.ross13@yahoo.com)

John Paul Sullivan  
[john.p.sullivan1@gmail.com](mailto:john.p.sullivan1@gmail.com)

Kippy Thomas  
[kippy123@gmail.com](mailto:kippy123@gmail.com)

### A. Call to Order – 6:00PM

### B. Invocation / Pledge of Allegiance

### C. Roll Call of Members

Present: Lyon, Blake, Borchard, Brown, Davis, Garay, Milligan, May, Thomas, Simmons, Reimund, Ross, Sullivan

Excused: Saldano, Cossio

### D. Approval of Minutes / Correspondence / Announcements

#### 1. Approval of Minutes

##### i. September 28, 2023

(1) **Motion to approve September 28, 2023 minutes made by A. Blake 2<sup>nd</sup> by M. Milligan – 13 yes, 0 no, 0 abstain, 2 absent/vacant.**

2. **ACPG Statement:** The Alpine Community Planning Group was formed for the purpose of advising and assisting the Director of Planning, the Zoning Administrator, the Planning Commission and the Board of Supervisors in the preparation, amendment and implementation of community and sub-regional plans. The Alpine Community Planning Group is an advisory body only.

E. **Open Discussion:** Opportunity for members of the public to speak to the ACPG on any subject matter within the ACPG's jurisdiction that is not on the posted agenda.

### F. Prioritization of this Meeting's Agenda Items

### G. Organized / Special Presentations

1. The owner of the property located at 1250 Tavern Rd, Alpine, CA (APN 403-390-27) has applied for a Major Use Permit to develop a car wash (PDS2022-MUP-22-008). The project is a Major Use Permit to allow the use of 3,000 square feet of a property to construct an express car wash pursuant to Zoning Ordinance Section 1430. The carwash would operate Monday – Sunday from 7am-7pm (with an extra hour to open and close). The carwash capacity would be 80 cars per hour. The General Plan Regional category is Village. Zoning for the site is C36 General Commercial. Pursuant to the zoning ordinance, Automotive and Equipment Cleaning uses within an C36 zone require a major use permit. Access to the site would be provided by Tavern Road, a County maintained road. The project would be served by the Padre Dam Municipal Water District and sewer by the San Diego County Sanitation District. The owner will present plans for the development and will seek a recommendation from the planning group. The group may make a recommendation on this application.  
**Presentation, Discussion & Action**

**Motion to approve the project as presented made by S. Reimund, 2<sup>nd</sup> by A. Blake 13 yes, 0 no, 0 abstain, 2 absent/vacant.**

2. The San Diego County Department of Public Works (DPW) has requested the ACPG's participation in identifying a prioritized list of transportation improvement projects on the county-maintained road

network in Alpine. Project types include the following:

- i. *Road Reconstruction/Reconfiguration/New Roads*: Improvements include adding and/or modifying street lighting, lanes, turning lanes, roundabouts, or bike lanes to improve safety and/or increase roadway capacity.
- ii. *Bridges Repair/Reconstruction*: Improvements include adding and/or modifying street lighting, lanes, or bike lanes to improve safety and/or increase bridge capacity.
- iii. *Sidewalk/Pathways/Pedestrian Access Ramps*: Improvements include sidewalks, pathways, and ramps that promote safety and walking in the community.
- iv. *Drainage*: Improvements include corrections to storm drain problems on roadways or dip sections.
- v. *Traffic Signals*: Installation of traffic signals to improve safety and traffic operation.
- vi. *Intersections*: Improvements could include new signals and turn lanes to improve safety and traffic operation.

**Presentation, Discussion & Action.**

**No action taken on this item. Group discussed removing this standing item from the agenda until a workshop can be held by the ACPG in the new year to develop future recommendations to add to the priority list.**

## **H. Group Business:**

1. Subcommittee Chairs to submit list of subcommittee members for approval. **Discussion & Action**

**Motion to approve addition of John Paul Sullivan made by A. May. 2<sup>nd</sup> by T. Lyon. 13 yes, 0 no, 0 abstain, 2 absent/vacant.**

**Motion to approve addition of Mary Davis made by A. May. 2<sup>nd</sup> by M. Milligan. 13 yes, 0 no, 0 abstain, 2 absent/vacant.**

## **I. Consent Calendar**

## **J. Subcommittee Reports (including Alpine Design Review Board)**

## **K. Officer Reports**

## **L. Open Discussion 2 (if necessary)**

## **M. Request for Agenda Items for Upcoming Agendas**

## **N. Approval of Expenses / Expenditures**

## **O. Announcement of Meetings:**

1. Alpine Community Planning Group – January 25<sup>th</sup>, 2024
2. ACPG Subcommittees – TBD
3. Planning Commission – January 19<sup>th</sup>, 2024
4. Board of Supervisors – January 9<sup>th</sup>, 10<sup>th</sup>, 23<sup>rd</sup>, & 24<sup>th</sup> 2024

## **P. Adjournment of Meeting**

1. **Motion to Adjourn made by T. Lyon, 2<sup>nd</sup> by M. Milligan. Motion Passed – 13 yes, 0 no, 0 abstain, 2 vacant/absent - Meeting adjourned at 8:06pm.**

## Disclaimer Language

### Public Disclosure

We strive to protect personally identifiable information by collecting only information necessary to deliver our services. All information that may be collected becomes public record that may be subject to inspection and copying by the public, unless an exemption in law exists. In the event of a conflict between this Privacy Notice and any County ordinance or other law governing the County's disclosure of records, the County ordinance or other applicable law will control.

### Access and Correction of Personal Information

You can review any personal information collected about you. You may recommend changes to your personal information you believe is in error by submitting a written request that credibly shows the error. If you believe that your personal information is being used for a purpose other than what was intended when submitted, you may contact us. In all cases, we will take reasonable steps to verify your identity before granting access or making corrections.

# Alpine Design Review Board Minutes

Monday May 2, 2022  
1830 Alpine Blvd.  
Alpine, CA 91901

- I Call to Order – Roll Call  
The meeting of the Alpine Design Review Board was called to order by Chairperson Curt Dean at 7:02 p.m. Board members present: Curt Dean, Carol Morrison, Dan Wasson and Peggy Easterling. Absent: Kippy Thomas.
- II Approval of Minutes  
There was a MOTION: to approve the minutes of November 1, 2021, as emailed. Moved by Peggy Easterling. Seconded by: Carol Morrison. Ayes: 4-0-0. No meetings were held in December 2021 or January, February, March or April 2022 for lack of an agenda.
- III Public Comment  
None
- IV Review Alpine Auto Wash, 1250 Tavern Road. Review of new car wash building and related site improvements. Applicant Linus Klein. (Discussion and Vote). There was a MOTION: To approve the conceptual project as proposed noting there will be no neon sign, the applicant return with the size and materials for all signs, the height of all site pole lights and times non security lights will be turned off. Moved by Peggy Easterling. Seconded by Dan Wasson. Ayes: 4-0-0
- V Next meeting – June 6, 2022, at 7:00pm Alpine Community Center.

Adjournment – The meeting was adjourned at 7:45 pm.

Respectfully submitted,  
Peggy Easterling  
Secretary Alpine Design Review Board

## Attachment F – Ownership Disclosure



## County of San Diego, Planning &amp; Development Services

**APPLICANT'S DISCLOSURE OF OWNERSHIP  
INTERESTS ON APPLICATION FOR ZONING  
PERMITS/ APPROVALS  
ZONING DIVISION**
Record ID(s) PDS2022-MUP-22-008Assessor's Parcel Number(s) 403-390-27

Ordinance No. 4544 (N.S.) requires that the following information must be disclosed at the time of filing of this discretionary permit. The application shall be signed by all owners of the property subject to the application or the authorized agent(s) of the owner(s), pursuant to Section 7017 of the Zoning Ordinance. **NOTE:** Attach additional pages if necessary.

A. List the names of all persons having any *ownership interest* in the property involved.

Linus KleinGerald KleinKarsten HesseLazaro Cardenas

B. If any person identified pursuant to (A) above is a corporation or partnership, list the names of all individuals owning more than 10% of the shares in the corporation or owning any partnership interest in the partnership.

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

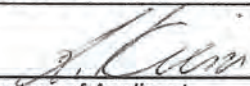
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

C. If any person identified pursuant to (A) above is a non-profit organization or a trust, list the names of any persons serving as director of the non-profit organization or as trustee or beneficiary or trustor of the trust.

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

**NOTE: Section 1127 of The Zoning Ordinance defines Person as:** "Any individual, firm, copartnership, joint venture, association, social club, fraternal organization, corporation, estate, trust, receiver syndicate, this and any other county, city and county, city, municipality, district or other political subdivision, or any other group or combination acting as a unit."

  
Signature of Applicant

Linus Klein  
Print Name

----- OFFICIAL USE ONLY -----

**SDC PDS RCVD 06-23-22  
MUP22-008**

5510 OVERLAND AVE, SUITE 110, SAN DIEGO, CA 92123

For any questions, please email us at: [PDSZoningPermitCounter@sdcounty.ca.gov](mailto:PDSZoningPermitCounter@sdcounty.ca.gov)<http://www.sdcounty.ca.gov/pds>