



The County of San Diego

Planning Commission Hearing Report

Date:	September 18, 2020	Case/File No.:	Liberty Charter High School; PDS2015-MUP-15-027; PDS2015-ER-15-14-010
Place:	No In-Person Attendance Allowed - Teleconference Only - County Conference Center 5520 Overland Avenue San Diego, CA 92123	Project:	Major Use Permit for a charter high school
Time:	9:00 a.m.	Location:	Chase Avenue and Jamacha Road, Valle De Oro
Agenda Item:	#1	General Plan:	Semi-Rural (SR-0.5)
Appeal Status:	Appealable to Board of Supervisors	Zoning:	Rural Residential (RR)
Applicant/Owner:	Literacy First Charter Schools, Inc.	Community:	Valle De Oro Community Plan Area
Environmental:	Mitigated Negative Declaration	APN:	498-330-39

A. OVERVIEW

The purpose of this staff report is to provide the Planning Commission with the information necessary to consider the proposed Major Use Permit (PDS2015-MUP-15-027), the conditions of approval, and environmental findings prepared in accordance with the California Environmental Quality Act (CEQA).

The Liberty Charter High School project (Project) seeks a Major Use Permit to construct and operate a charter high school in the Valle De Oro Community Plan area. The proposed Project includes a 48,000-square-foot, two-story building that will house 22 classrooms, administrative offices, and a gymnasium. At full capacity, the school will serve 450 students and have 33 faculty and staff. The 7.7-acre property is zoned Rural Residential and is currently vacant.

During processing of the application, the County of San Diego received comments from stakeholders, including the Valle De Oro Community Planning Group, relating to parking, traffic, noise, and community character. The sections contained in this report describe the following: development proposal, analysis and discussion, community planning group and public input, and the Planning & Development Services (PDS) recommendation. PDS analyzed the project for consistency with the General Plan, Zoning Ordinance, and other applicable regulations, policies and ordinances and reviewed the project's potential impacts on the environment in accordance with CEQA. PDS found the Project to be consistent with all relevant regulations with inclusion of conditions in the Form of Decision (Attachment B).

The Planning Commission is asked to either approve the project as submitted, approve the project with modifications, or deny the project. Based on staff's analysis, staff thinks that the required findings can be made and recommends approval of the Project.

B. STAFF RECOMMENDATIONS

Staff recommends the Planning Commission take the following actions:

- a. Adopt the Environmental Findings, which includes a Mitigated Negative Declaration as required by CEQA guidelines (Attachment A).
- b. Approve the Major Use Permit PDS2015-MUP-15-027, confirm the findings, and include the requirements and conditions as set forth in the Form of Decision (Attachment B).

C. PROJECT BACKGROUND

The Project, submitted to the County in 2015, is a charter high school for grades nine through 12, operated by Literacy First Charter Schools (LFCS). LFCS operates under a charter issued by the San Diego County Board of Education, which requires that they operate within the boundaries of the Grossmont Union High School District. The charter was authorized by the San Diego County Board of Education in 2001. The existing Liberty Charter High School is located approximately eight miles west of the proposed site, at 8425 Palm Street in Lemon Grove, and has an enrollment of approximately 350 students. LFCS currently leases the former Palm Middle School campus from the Lemon Grove School District, and the existing Liberty Charter high school, once constructed, will be relocated to the Project location.

D. REGIONAL SETTING AND PROJECT LOCATION

The Project site is in the community of Valle De Oro within unincorporated San Diego County, just south of Chase Avenue where it meets Jamacha Road (Figure 1). Interstate 8 is 2.4 miles north, and State Route 125 is 4.3 miles west of the site. The property is bordered to the north by Chase Avenue, to the east by a commercial shopping center, to the south by a private access road and single-family residences, and to the west by single-family residences. Valhalla High School is approximately one mile southeast of the project site, which is currently vacant (Figure 2). On-site terrain consists of a gradual slope running northeast to southwest up towards the existing single-family residences to the west. Access to the site is proposed to be directly from Chase Avenue.

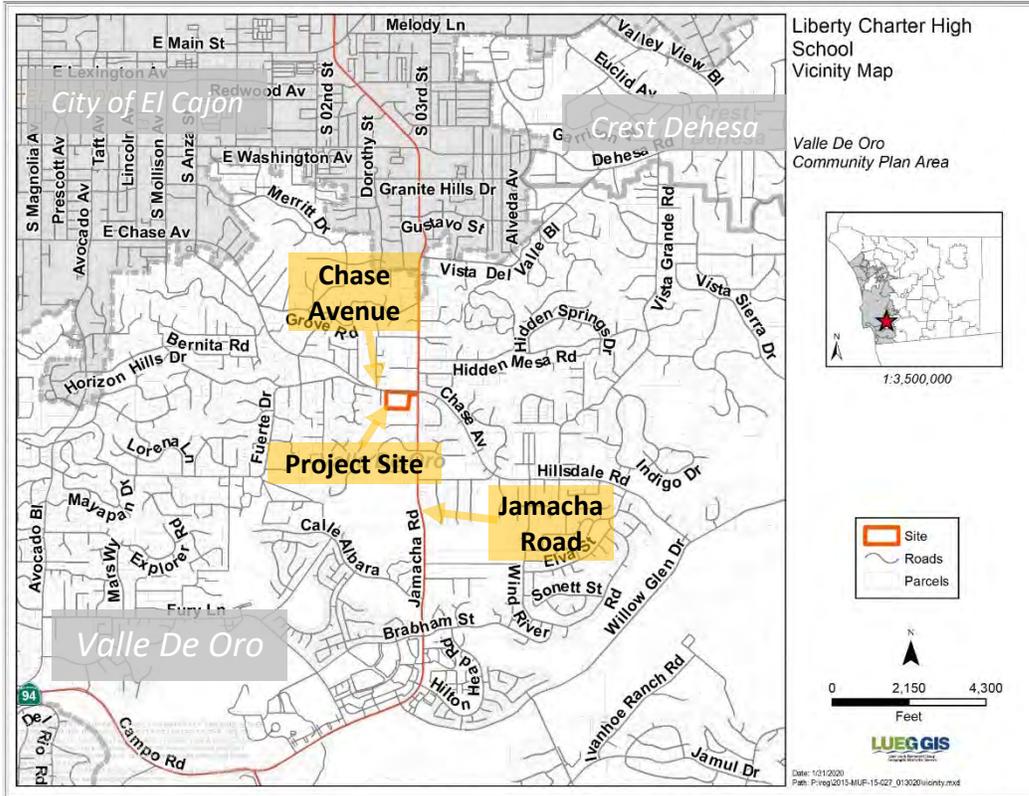


Figure 1: Vicinity Map



Figure 2: Existing Site

The General Plan Regional Category for the site is Semi-Rural Residential, or SR-0.5, which allows one dwelling unit per 0.5 acres. Surrounding land uses include single-family residential and commercial uses. Please refer to Attachment C – Planning Documentation, for maps of surrounding land uses and zoning designations.

Table D-1: Surrounding Zoning and Land Uses

Location	General Plan	Zoning	Adjacent Streets	Description
North	Semi-Rural (SR-0.5)	RR	Chase Avenue	Residential, Commercial
East	Neighborhood Commercial	C36	Jamacha Road	Commercial
South	Semi-Rural (SR-0.5)	RR	N/A	Single-Family Residential
West	Semi-Rural (SR-0.5)	RR	N/A	Single-Family Residential

E. DEVELOPMENT PROPOSAL

1. Project Description

The Project is a charter high school consisting of a 48,000 square-foot, two-story building that will house 22 classrooms, the administrative office, and a gymnasium on a 7.7-acre site located near the intersection of Chase Avenue and Jamacha Road. A sports field is also included on the eastern half of the site (Figure 3). At full capacity, the proposed school will serve a maximum of 450 students and have 33 faculty and staff.



Figure 3: Photo simulation of proposed high school campus, looking northeast from above.

Architecture and Design

The architectural design of the Project incorporates materials, colors and textures that are similar to surrounding structures, specifically the adjacent two-story shopping center buildings on Jamacha Road, as seen in Figure 5. The Project includes red tile roof accents at the entrances and building corners, as well as arched window frames (Figures 4 and 5). The exterior finish consists of concrete in two different colors with decorative tile and wood accents. The variation in color and finishes serve to break up the mass of the building and visually divide the structure into separate components, reducing visual prominence of the building.



Figure 4: Photo simulation of school entrance and parking lot looking east from onsite.



Figure 5: Photo simulation of Project, looking southwest from intersection of Chase Avenue and Jamacha Road. Existing shopping center is shown in foreground.

The parking lot, which is partially screened from street view by perimeter landscaping, separates the building from the public road. The proposed landscaping and sports field on the eastern portion of the site help to reduce the bulk of the building by providing screening and buffering from Chase Avenue and Jamacha Road, as seen in Figure 5. Additionally, the Project is surrounded by developed parcels in an area that transitions from a single-family residential neighborhood to commercial development fronting a major road. By relating to the design of the adjacent commercial development and placing the structure in the central portion of the site and using the sports field as a buffer, the Project as designed will not introduce a negative visual impact to the community.

Access and Public Improvements

Access to the site will be provided by a one-way circulation driveway connecting to Chase Avenue at two locations. Based on the Traffic Impact Analysis prepared, the Project was determined to have a direct traffic impact to Chase Avenue between the westernmost driveway and Jamacha Road. To mitigate this impact, the Project must widen Chase Avenue and provide a westbound left-turn lane into the site, as well as include bicycle and pedestrian improvements along the frontage (Figure 6). The road widening is consistent with the Valle De Oro Mobility Element classification of Chase Avenue as a 4-lane road with bike lanes (4.1B Major Road).

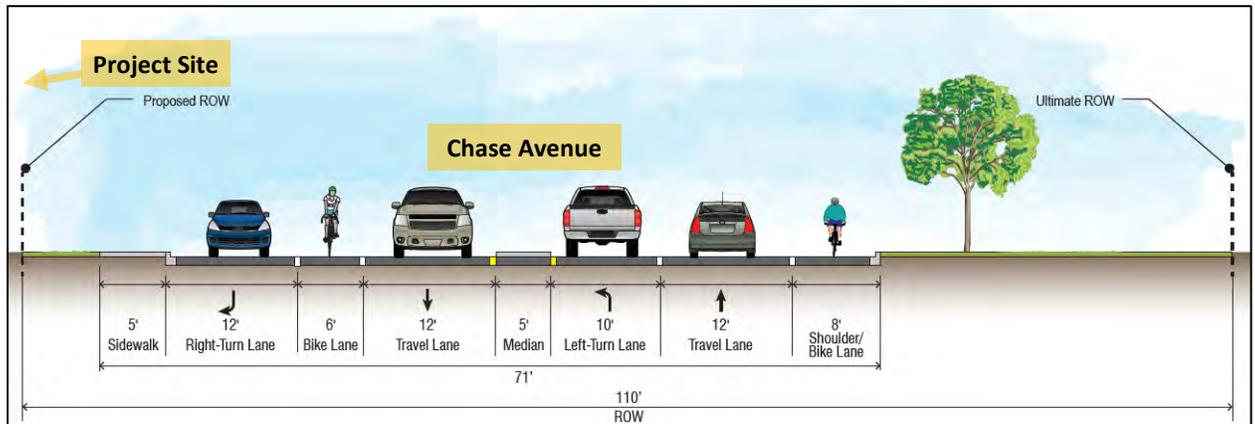


Figure 6: Proposed Chase Avenue road improvements.

A third driveway is proposed along the frontage of the sports field; it will be gated and used only for emergency personnel and their vehicles using a KnoxBox key-lock system for entry.

Additional improvements include upgrading the Jamacha Road/Chase Avenue intersection crosswalks and stoplights to meet accessibility requirements. This will involve restriping all four crosswalks with ladder-style continental striping and installing new ADA-compliant traffic and crosswalk signals.

Parking and Internal Circulation

The County Zoning Ordinance requires senior high schools to provide one parking space per employee, 0.25 spaces per student, and 15 spaces for visitors. With a maximum of 33 employees and 450 students, the total number of parking spots required is 160.5. The Project includes a total of 161 parking spaces, including four accessible parking spaces and two spaces with electric vehicle charging stations installed. A minimum of 45 bicycle spaces will also be provided.

The parking lot is designed to have a one-way circulation pattern to help vehicle movement in and out of the site onto Chase Avenue. The parking lot includes approximately 700 feet of drop-off/pick-

up area, accommodating a lineup of approximately 28 vehicles on-site at any given time. An analysis of the operation of the current Liberty Charter High School campus in Lemon Grove was completed and determined that the proposed size and design of the parking lot will accommodate internal traffic and parking for a maximum of 450 students and 33 employees.

Parking will be prohibited on both the north and south sides of Chase Avenue; the Project must install "no parking" signage in accordance with County standards. A small portion of Chase Avenue in between the entrance and exit driveways will be available for street parking. The amount of parking allowed in that location will be determined upon completion of additional sight distance analysis at the exit driveway during final engineering. The applicant is required through a condition in the MUP to work with the owner of the adjacent commercial shopping center on Jamacha Road to install signage prohibiting school parking, as well as student drop-off and pick-up before and after school.

School Operation

At full capacity, the Project will serve 450 students and have 33 full-time faculty and staff. The school year will occur from August through June, and school hours will be from 8:30 a.m. to 3:45 p.m. Monday through Friday, as regulated by the MUP. However, Liberty High School administrators provided a summary of the daily schedule, which indicated that teachers and administrators arrive on campus around 8:00 a.m. and assist in the drop-off of students. When school ends at 3:45 p.m., approximately 125 students stay on campus for various team practices or games either on the sports field or in the gymnasium. All outdoor activities are required to end by 8:00 p.m., and indoor activities are required to end by 10:00 p.m. The field lighting will be required through a condition in the MUP to be turned off by 8:00 p.m. to avoid impacting adjacent residential properties, therefore practices and games on the field will end by approximately 7:30 p.m. School administrators have indicated that students who do not participate in sports are typically picked up or leave campus within 15 minutes of school ending. School administrators are required through a condition in the MUP to supervise drop-off and pick-up activities in the designated areas. Staff members and teachers generally leave campus by 4:30 p.m.

During school hours, approximately 30 visitors come to the campus each day. This includes the postal service, UPS and FedEx deliveries, lunch delivery, and parents picking up students early for appointments.

The school will have 6-foot fencing surrounding the field and outdoor student activity areas to prevent non-students from accessing the property during and after school hours and prevent students from unauthorized departure. Students are required to wear school uniforms, which help distinguish students from faculty and adjacent business owners or customers. Weekend activities on campus will be limited and may include flag football games on some Saturday mornings in the fall, and drama production in the spring.

Municipal Services

The Project has demonstrated that all necessary services and facilities are available as required by the General Plan and Board of Supervisors Policy I-84 (Project Facility Availability Forms for Public Water, School, and Fire Services – Attachment F). The applicant is responsible for funding all the necessary services and facilities to serve the Project.

- The site will have potable water and sewer service supplied by the Otay Water District

- Fire protection will be provided by the San Miguel Consolidated Fire Protection District
- The Project site is within the Cajon Valley Union School District and Grossmont Union High School District

F. ANALYSIS AND DISCUSSION

The Project has been reviewed for conformance with all relevant ordinances and guidelines, including the San Diego County General Plan, the Valle De Oro Community Plan, the County Zoning Ordinance, and CEQA Guidelines. During processing of the Project, staff received comments from stakeholders relating primarily to traffic, parking, compatibility with existing community character, noise, light and glare. Staff's responses to these comments and a discussion of the Project's consistency with applicable codes, policies, and ordinances is described on the following pages.

1. Key Requirements for Requested Actions

- a. Is the Project consistent with the vision, goals, and policies of the General Plan?
- b. Does the Project comply with the policies set forth under the Valle De Oro Community Plan?
- c. Is the Project consistent with the County's Zoning Ordinance?
- d. Is the Project consistent with other applicable County regulations?
- e. Does the Project comply with CEQA?

2. Analysis

Traffic and Transportation

Comments were received indicating that the Project will have significant traffic impacts to Chase Avenue and Jamacha Road. A Focused Traffic Impact Study dated November 2018 was prepared for the Project using the San Diego Association of Governments (SANDAG) trip generation rates. It found that the Project would generate a total of 585 new daily trips, including 117 morning peak-hour trips, 99 afternoon (school traffic) peak-hour trips, and 59 afternoon (commuter traffic) peak-hour trips. There will be a direct traffic impact to Chase Avenue between the westernmost Project driveway and Jamacha Road. To mitigate this impact the Project will widen and improve Chase Avenue to accommodate a westbound left-turn lane onto the site. Bicycle lanes and sidewalks will also be installed along the Project frontage. Trips generated by the Project will not exceed the capacity of surrounding roads and intersections.

The surrounding road network supports traffic from nearby regions including Rancho San Diego, Crest/Dehesa, and El Cajon (Figure 7). Specifically, Jamacha Road (also known as State Route 54) is used by many residents in the Rancho San Diego and Jamul areas to connect to Interstate 8, 2.4 miles north of the project site. Chase Avenue is used to enter the City of El Cajon to the west, and to access Valhalla High School campus one mile southeast of the Project (Figure 8). Comments received stated that the additional traffic, along with the existing traffic from Valhalla High School, will increase wait time at the Jamacha Road/Chase Avenue intersection and negatively impact the community. The Traffic Impact Study analyzed nearby intersections and found that the Project would not have a direct impact at any of the intersections.



Figure 7: Surrounding road network

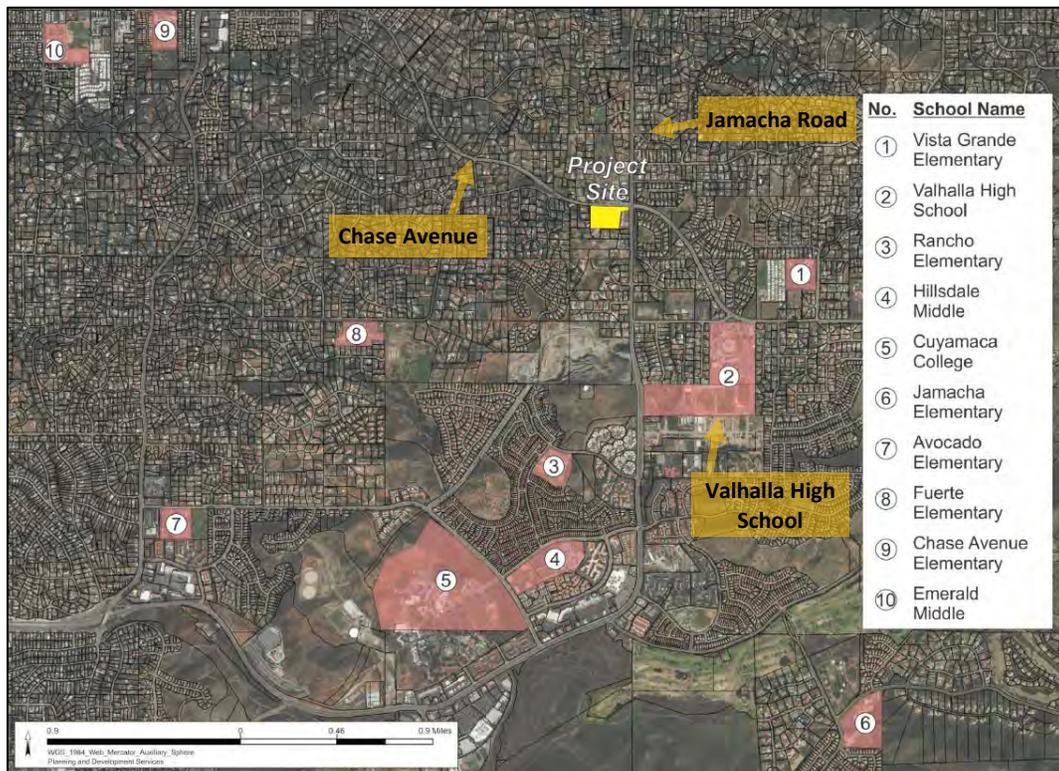


Figure 8: Nearby schools

To avoid the possibility of student drop-off traffic from both Liberty Charter and Valhalla High School occurring at the same time, Liberty Charter High School will be required to start at 8:30 a.m. to avoid starting at the same time as Valhalla, which starts at 7:25am. The Project is allowed to change its start time through a minor deviation process if Valhalla revises its starting schedule in the future to avoid conflicting schedules.

The Project also must improve the Jamacha Road/Chase Avenue intersection sidewalks and curb ramps, as well as upgrade all pedestrian and vehicle signals to meet current accessibility requirements. Currently, only the easternmost crosswalk is marked with a traditional parallel line crosswalk, and the other three crosswalks have no striping. Enhancements to the intersection will greatly improve the pedestrian safety and connectivity from the proposed school to existing public transit stops along Jamacha Road, approximately 600 feet from the Project entrance (Figure 9). Metropolitan Transit Service (MTS) provides bus transit every 30 minutes along Route 816, which runs from the El Cajon Transit Center to the Cuyamaca College campus along Jamacha Road. With construction of the Project, the southbound bus stop on Jamacha Road near Chase Avenue will be upgraded to meet accessibility requirements.



Figure 9: Nearby public transit stops identified by blue circles.

Parking and Circulation

Concerns were also raised concerns about the adequacy of parking on site. Neighbors are concerned that parking associated with the school will occur on nearby residential streets and in the

adjacent shopping center parking lot. The County's Zoning Ordinance requires a total of 160.5 parking spaces to be provided based on the maximum capacity of the Project. A total of 161 parking spaces are provided, meeting the County's requirements.

The applicant's traffic engineer conducted a parking analysis at the current Liberty Charter High School campus in Lemon Grove to confirm the parking needs for the school. Parking utilization was evaluated by collecting parking occupancy counts every 30 minutes between 7:45 a.m. and 4:45 p.m. in November 2014. The study found that during a typical school day, less than 80% of the total available regular parking spaces were occupied. Assuming a 90% maximum parking occupancy rate, the current site would require approximately 49 spaces, which corresponds to 0.153 spaces per student. Based on this rate, the proposed school site to accommodate 450 students would require 69 regular parking spaces. The Project includes 92 more parking spaces than was needed based on the parking analysis of the current campus.

The Project is required to install "no parking" and "no drop-off" signage on Chase Avenue along the project frontage and across the street to prevent parents from dropping students off across the street, which would require kids to cross the street into the school. To prevent school users from parking in the adjacent shopping center parking lot, the applicant will be required to work with the shopping center property owner to install signage prohibiting school parking in the lot. To satisfy this condition, the applicant must provide evidence of coordination with the shopping center property owner and good faith effort to find a solution.

Community Character and Visual Impacts

The Project is located between commercial development fronting on a major thoroughfare and single-family homes located on the surrounding hillsides. The Project includes measures to eliminate potential compatibility issues between the proposed school and adjacent land uses. Measures include placement of the school building in the central portion of the site and using the sports field as a buffer to the adjacent commercial uses and public roads. The campus will be surrounded by a six-foot tall fence, which will be placed around the buildings, sports field, and outdoor areas to prevent non-students from accessing the property and prevent students from unauthorized departure. Landscaping, including street trees and shrubs, will be installed in accordance with the County's Landscape Ordinance along the perimeter of the entire site (Figures 10 and 11), including Chase Avenue and the western and southern portions abutting single-family homes. Perimeter trees will be installed as 24-inch box trees and include coast live oak, western redbud, and Brisbane box, which grow to approximately 20-40 feet in height and width within about five to 10 years. Evergreen shrubs growing six-feet high and higher will be planted from 15-gallon containers to supplement screening with trees. A bioretention basin for storm water management with landscaping will be installed in the space between the sports field and the adjacent shopping center as identified in Figure 11.



Figure 10: Photo simulation of Project looking south from across Chase Avenue. Street trees in foreground shown as semi-transparent for better visual of school.



Figure 11: Photo simulation of Project looking northeast from above, showing landscaping along perimeter. Perimeter landscaping will include evergreen shrubs in between trees, not shown here.

The nearest neighboring residence is approximately 110 feet south of the proposed school building and will be separated by an existing private road, the school parking lot, and perimeter landscaping within the 40-foot rear yard setback (Figure 11). Residents to the west are approximately 160-200 feet away from the proposed school building and are located on a slope looking down on the school (Figures 11 and 12). These residents will have views of the Project, however a buffer of the parking lot as well as the 20- to 40-foot tall trees and 6-foot shrubs will help soften the view of the buildings. The Project is required to screen any rooftop equipment from view using architecturally integrated screening walls for visual and noise purposes.

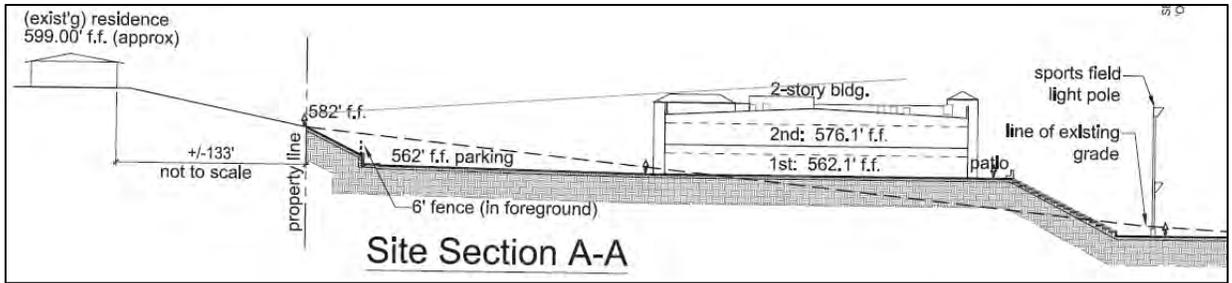


Figure 12: Approximate elevation of project site, existing residence to the left

Major Use Permit Findings

The discussion below covers scale, bulk and coverage, availability of services, effects upon neighborhood character, and suitability of the site for the type of proposed use. Staff has analyzed the Project in relation to each of these.

The proposed location, size, design, and operating characteristics of the Project will be compatible with adjacent commercial and residential uses. The existing residential development surrounding the site typically consist of single-family, detached homes on lots ranging from 0.5 acres to 2 acres. Most homes are single-story, with some two-story homes and are on average 16 to 25 feet in height. The existing commercial shopping center to the east of the Project is comprised of three two-story buildings at approximately 28 to 30 feet in height totaling 30,096 square feet on a 1.75-acre site. The Project is designed to be similar in scale to the adjacent commercial center; it uses similar colors and architectural features and is comparable in height and site coverage as demonstrated in Figure 5. The proposed school building is 32 feet tall, with architectural accent towers reaching a maximum of 38 feet in height. Proposed site coverage is approximately 12%, while coverage on nearby commercial sites ranges from 24% to 39%. Coverage on residential properties nearby ranges from 3% to 14%. While the proposed Liberty Charter school building is larger than adjacent single-family homes, the Project is compatible in terms of bulk and scale because of the large setbacks, landscape screening, use of varying architectural elements and materials, and differing building facades. The coverage of the Project as designed is consistent with surrounding land uses.

The proposed use is consistent with the General Plan and Zoning designations, and all necessary public facilities and services are available to the site based on service availability forms provided by the applicable utility providers and districts. Therefore, the Project will be compatible with the surrounding community.

Noise

Concerns were raised regarding noise from the bells and students attending the proposed school. A preliminary noise study was prepared by LDN Consulting, Inc. to evaluate noise impacts associated with the development of the Project. The analysis found that the on-site and off-site noise levels comply with the County's community noise equivalent level (CNEL) and that the Project will not cause any significant impacts to existing or future land uses. CNEL is a measurement of average sound level over a 24-hour period, applying different weights to evening and nighttime hours to compensate for the increased noise-sensitivity of people at those times. The Project includes requirements relating to the operational characteristics, number of allowed events and activities, and hours of operation to ensure it does not exceed the allowable noise levels set by the County's Noise Ordinance and General Plan Noise Element.

Light and Glare

The community raised concerns regarding the visual impact of the proposed lighting and glare associated with the proposed school and sports field lighting. A Photometric Study was prepared by Hamann Companies and looks at the details of the Project's proposed lighting, including parking lot, building, and sports field lighting. All proposed lighting for the Project is designed to comply with both the San Diego Light Pollution Code (§59.108-59.110) and the County Zoning Ordinance, which prohibits outdoor lighting from spilling onto adjacent properties in excess of 0.2 foot-candles (unit of light intensity). Lighting proposed in the parking lot consists of four 22-foot high aluminum light poles with 180-watt low pressure sodium lamps in the center median of the parking lot. Building lighting includes 21 wall mounted aluminum fixtures with 180-watt low pressure sodium lamps. Sports field lighting will consist of four 60-foot tall poles with six fixtures per pole. All proposed fixtures will be fully shielded downward, preventing light from trespassing onto adjacent properties. The photometric study concluded that the Project will comply with the established guidelines and that it will not have a significant effect related to dark skies or light pollution. The Project is required to turn off sports field lighting by 8:00 p.m. each night when in use, which is anticipated to be twice per week in the fall and winter months.

Greenhouse Gas Emissions

A Global Climate Change study prepared for the Project analyzed greenhouse gas (GHG) emissions associated with the Project using a project-specific service-population metric (2.94 MT CO₂e/service population) and compliance with the California Air Pollution Control Officers Association (CAPCOA) 900 metric ton threshold. Compliance with these thresholds demonstrates the Project's greenhouse gas emissions will not have a significant impact on the environment or conflict with any applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases because it shows the Project's emissions will not exceed the levels necessary to accomplish reductions within the County to meet applicable targets. The Project was estimated to be below both thresholds with incorporation of sustainable design standards and reduction measures. The Project is required to incorporate efficient outdoor water usage, a 90-kilowatt solar photovoltaic system, energy efficient outdoor lighting, electric vehicle charging stations, carpool parking spaces, building efficiency standards, recycling areas, and bicycle parking.

Additionally, most Liberty Charter students live in the East County area of San Diego. The proposed school campus will replace the current campus located in Lemon Grove. The relocated high school will be closer to elementary and middle schools operated by Literacy First Charter Schools which are likely to feed into the high school. Typically, a significant portion of GHG emissions is related to vehicle trips generated by a project, and the length of those trips. Based on a zip code analysis included in the Global Climate Change study, a reduction in trip length would occur with the relocation of the high school closer to the student population, which would equate to a reduction in greenhouse gas emissions from vehicles.

The Global Climate Change study also evaluated and found the Project consistent with the County's Climate Action Plan (CAP); however, the Project does not rely on the CAP for analysis of the impacts its greenhouse gas emissions. Approval of the CAP was overturned in June 2020. The Project incorporates reduction measures included in the CAP. Specifically, the proposed project will implement a landscape plan demonstrating a 40% reduction in outdoor water use and will install two electric vehicle charging stations in designated parking spaces.

3. General Plan Consistency

The site is subject to the General Plan Regional Category Semi-Rural and Land Use Designation SR-1. The Project is consistent with the following relevant General Plan goals, policies, and actions as described in Table F-1.

Table F-1: General Plan Conformance

General Plan Policy	Explanation of Project Conformance
<p>Policy LU-5.5 – Projects that Impede Non-Motorized Travel. Ensure that development projects and road improvements do not impeded bicycle and pedestrian access. Where impacts to existing planned routes would occur, ensure that impacts are mitigated, and acceptable alternative routes are implemented.</p>	<p>The Project will improve and enhance bicycle and pedestrian access by the installation of sidewalks and bicycle lanes along the Project frontage. Improvements will provide a continuous connection to adjacent sidewalks and transit stops along Jamacha Road.</p>
<p>Policy LU-6.5 – Sustainable Stormwater Management. Ensure that development minimizes the use of impervious surfaces and incorporates other Low Impact Development (LID) techniques as well as a combination of site design, source control, and stormwater best management practices, where applicable and consistent with the County's LID Handbook.</p>	<p>The Project has incorporated required stormwater management features in accordance with the County's LID Handbook, including structural BMPs and two biofiltration basins on the east end of the parcel to treat stormwater.</p>
<p>Policy LU-6.9 – Development Conformance with Topography. Require development to conform to the natural topography to limit grading; incorporate and not significantly alter the dominant physical characteristics of a site; and to utilize natural drainage and topography in conveying stormwater to the maximum extent practicable.</p>	<p>The Project involves approximately 23,500 cubic yards of cut and fill, balanced on-site. The existing site gradually slopes down from the west end to the east end near the commercial shopping center. The proposed building on the western portion of the site is close to the existing grade, and the proposed sports field is down slope from the building, following the natural topography of the site. On-site drainage flows to the east into bioretention basins adjacent to the sports field.</p>

General Plan Policy	Explanation of Project Conformance
<p>Policy LU-17.2 – Compatibility of Schools with Adjoining Uses. Encourage school districts to minimize conflicts between schools and adjacent land uses through appropriate siting and adequate mitigation, addressing such issues as student drop-off/pick-up locations, parking access, and security.</p>	<p>The Project is in an area of transition between residential and commercial development fronting on a major thoroughfare. The proposed building is sited in the central portion of the site and uses the sports field as a buffer between the school building and the commercial uses. The school will be a closed campus fully enclosed by fencing and will require students to wear school uniforms for additional security. The Project includes adequate parking as well as adequate circulation on-site for drop-off/pick-up activities. Access to the site is from Chase Avenue, a Mobility Element Road. The Project is compatible with adjacent land uses.</p>
<p>Policy COS-13.1 – Restrict Light and Glare. Restrict outdoor light and glare from development projects in Semi-Rural and Rural Lands and designated rural communities to retain the quality of night skies by minimizing light pollution.</p>	<p>All proposed lighting is designed to comply with both the San Diego County Light Pollution Code (§59.108-59.110) and the County Zoning Ordinance. Lighting for the sports field will only be used periodically when school games or events are scheduled and are required to be shut off by 8:00pm. No public use of the facilities is proposed. The building materials for the proposed school have been selected to minimize the potential impacts due to glare from highly reflective materials. All site lighting is required to be shielded down to avoid polluting adjacent properties.</p>
<p>Policy COS-19.1 – Sustainable Development Practices. Require land development, building design, landscaping, and operational practices that minimize water consumption.</p>	<p>The Project will incorporate efficient irrigation systems and use native plant species and non-invasive drought tolerant/low water use plants in landscaping. The sports field will function as a biofiltration feature of the stormwater management system along with biofiltration basins on the easternmost portion of the site. The proposed building will include installation of solar panels.</p>
<p>Policy M-4.4 – Accommodate Emergency Vehicles. Design and construct public and private roads to allow for necessary access for appropriately sized fire apparatus and emergency vehicles while accommodating outgoing vehicles from evacuating residents.</p>	<p>All access drives within the Project are designed to allow for ingress and egress of fire apparatus and emergency vehicles. Additionally, a separate access driveway is proposed near the sports field for emergency personnel only.</p>

General Plan Policy	Explanation of Project Conformance
<p>Policy M-10.1 – Parking Capacity. Require new development to:</p> <ul style="list-style-type: none"> ■ Provide sufficient parking capacity for motor vehicles consistent with the project’s location, use, and intensity ■ Provide parking facilities for motorcycles and bicycles ■ Provide staging areas for regional and community trails 	<p>Parking is provided in accordance with Section 6764 of the Zoning Ordinance, which requires 160 parking spaces. The Project provides 161 parking spaces, including three accessible spaces 45 bicycle parking spaces. There are no designated trails or staging areas in the vicinity of the site.</p>

4. Valle De Oro Community Plan Consistency

The Project is consistent with the following relevant Valle De Oro Community Plan goals, policies, and actions as described in Table F-2.

Table F-2: Community Plan Conformance

Valle De Oro Community Plan Policy	Explanation of Project Conformance
<p>Community Character Goal. Retain the unique balance of urban, semi-rural, agricultural, and open space land uses within the community, with open space and low-density buffers that separate the community from adjacent cities and unincorporated communities, while new development within the community conserves natural resources and topography.</p>	<p>The Project is located on a site that is surrounded by development. Land to the north, west and south is fully developed with single-family residential land uses. To the east is a commercial plaza.</p>
<p>Community Character Policy 2. Verify the existence of adequate public facilities prior to development approvals.</p>	<p>A Service Availability Letter was received stating that water and sewer service are available from the Otay Water District. A Service Availability Letter was received stating that fire service is available from the San Miguel Consolidated Fire Protection District.</p>
<p>Community Design Policy 5. Require the provision of adequate, appropriate, off-street parking for all types of vehicles in all new developments.</p>	<p>The Project includes required parking for students, staff, and visitors of the charter school, in accordance with County requirements.</p>
<p>Mobility Policy 11. Eliminate safety hazards caused by direct access of traffic onto major arterial or collector streets when reviewing projects along such streets.</p>	<p>The Project has three access points on Chase Avenue, one of which is an emergency exit.</p>

5. Zoning Ordinance Consistency

The Project complies with all applicable zoning requirements of the Rural Residential (RR) zone with the incorporations of conditions of approval (Table F-3).

Table F-3: Zoning Ordinance Development Regulations

CURRENT ZONING REGULATIONS		CONSISTENT?
Use Regulation:	RR	Yes, upon approval of a Major Use Permit
Animal Regulation:	J	N/A
Density:	-	N/A
Lot Size:	0.5 AC	N/A
Building Type:	C	Yes
Height:	G	Yes, upon approval of Major Use Permit
Lot Coverage:	-	N/A
Setback:	G	Yes
Open Space:	-	N/A
Special Area Regulations:	-	N/A

Development Standard	Proposed/Provided	Complies?
Section 4600 of the Zoning Ordinance sets the maximum height requirements. This parcel has a designated height of "G" which requires structures to be no more than 35 feet in height.	The design and height of the Project will be in conformance with the height requirement upon approval of the Major Use Permit. The main building has a height of 32 feet, with the architectural accent towers at a maximum of 38 feet.	Yes <input checked="" type="checkbox"/> No <input type="checkbox"/> Upon approval of Major Use Permit
Section 4800 of the Zoning Ordinance requires that the project meet the "D" setback requirements of a 60-foot front yard setback, 15-foot interior side yard setback, 35-foot exterior side yard setback, and a 25-foot rear yard setback.	The proposed charter school will be located out of all required setbacks including front, rear, and side yard setbacks.	Yes <input checked="" type="checkbox"/> No <input type="checkbox"/>

6. California Environmental Quality Act (CEQA) Compliance

The project has been reviewed for compliance with the CEQA. A Mitigated Negative Declaration under CEQA was prepared and was available for Public Review from December 12, 2019, through January 17, 2020 (36 days), and is on file with Planning & Development Services as PDS2015-ER-15-010. The Mitigated Negative Declaration found that the Project, with the incorporation of mitigation measures to lessen the impact on biological resources, traffic, and air quality, would not cause any significant effects on the environment. To mitigate for impacts to 5.23 acres of non-native grassland, the applicant is required to purchase 2.62 acres of Tier III habitat credits from an approved mitigation bank. The Traffic Impact Study found a direct traffic impact to Chase Avenue, which will be mitigated by widening Chase Avenue to provide a westbound left-turn lane onto the site. The project is also conditioned to pay the Transportation Impact Fee to mitigate cumulative impacts. To mitigate for air quality impacts during construction, the Project must utilize Tier 4 diesel equipment with diesel particulate filters. These mitigation measures have been included in the conditions of

approval for the project. The Initial Study and Mitigated Negative Declaration can be found in Attachment D of this report.

7. **Applicable County Regulations**

Table F-5: Applicable Regulations

County Regulation Policy	Explanation of Project Conformance
a. Resource Protection Ordinance (RPO)	The site contains no wetlands, floodplain, flood fringe, or steep slopes. The Project will mitigate for impacts on biological resources. The site does not contain archaeological or historical sites. Therefore, the Project is consistent with the RPO.
b. County Consolidated Fire Code	The Fire Protection Plan has been reviewed and approved by the San Miguel Consolidated Fire Protection District and the County Fire Authority. The Project will comply with the County Consolidated Fire Code.
c. Noise Ordinance	The Project's Noise Study determined it complies with the Noise Ordinance. Conditions of approval will ensure compliance with the General Plan Noise Element.
d. Light Pollution Code	The Project will implement outdoor lighting and glare controls, such as the use of lights that are shielded down, to ensure compliance with the Light Pollution Code.
e. Watershed Protection Ordinance (WPO)	The Project's Storm Water Quality Management Plan and Hydromodification Management Study have been reviewed and were found to be complete and in compliance with the WPO.
f. Multiple Species Conservation Program (MSCP)	The Project is within the Metro-Lakeside-Jamul segment of the County's Multiple Species Conservation Program (MSCP). Impacts to non-native grassland will require mitigation in accordance with the Biological Mitigation Ordinance. The impact area does not qualify as a Biological Resource Core Area based on analysis detailed in the MSCP Conformance Statement dated July 22, 2019.

G. **COMMUNITY PLANNING GROUP**

On January 12, 2020, the Valle De Oro Community Planning Group (Group) objected to the Mitigated Negative Declaration and requested a full Environmental Impact Report (EIR) to be prepared based on concerns regarding traffic, community character, safety, aesthetics, and proximity to neighboring businesses and schools. This motion passed by a vote of 11-00-3-1 (11-Yes, 0-No, 0-Abstain, 3-Vacant, 1-Absent).

On February 4, 2020, the Group recommended denial of the Major Use Permit by a vote of 10-0-0-3-2 (10-Yes, 0-No, 0-Abstain, 3-Vacant, 2-Absent). The Group raised concerns regarding the location of the proposed charter school, safety, and traffic.

H. PUBLIC INPUT

A total of 115 comment letters were received during the public review period of the Mitigated Negative Declaration from December 12, 2019 to January 17, 2020 (36 days). Most of the comments were in support of the project, with 27 comments in opposition. Comments were received from California Department of Transportation, Grossmont High School District, and the Valle De Oro Community Planning Group, as well as from individuals, citing concerns related to traffic, safety and community character as described earlier in this report. At the time of application submittal and in accordance with Board Policy I-49, public notices were sent to property owners within a minimum radius of 300 feet of the project site until at least 20 different property owners were noticed. A total of 27 property owners received notices of the application submittal in 2015. Public notices were also sent to neighboring property owners and other interested parties at the start of the public review period of the Mitigated Negative Declaration, and prior to the Planning Commission hearing.

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AUTHORIZED REPRESENTATIVE:



MARK WARDLAW, DIRECTOR

ATTACHMENTS:

- Attachment A – Environmental Findings
- Attachment B – Major Use Permit Decision PDS2015-MUP-15-027
- Attachment C – Planning Documentation
- Attachment D – Environmental Documentation
- Attachment E – Public Documentation
- Attachment F – Service Availability Forms
- Attachment G – Ownership Disclosure

**Attachment A –
ENVIRONMENTAL FINDINGS**

LIBERTY CHARTER HIGH SCHOOL MAJOR USE PERMIT

PDS2015-MUP-15-027; PDS2015-ER-15-14-010

ENVIRONMENTAL FINDINGS

September 18, 2020

CALIFORNIA ENVIRONMENTAL QUALITY ACT (CEQA) FINDINGS

- 1) Find that the Planning Commission has reviewed and considered the information contained in the Mitigated Negative Declaration on file with Planning & Development Services as Environmental Review Number PDS2015-ER-15-14-010 before making its decision on the proposed project.
- 2) Adopt the Mitigation and Monitoring Program as incorporated into the project conditions of approval pursuant to CEQA Guidelines section 15074(d).
- 3) Find that the proposed project is consistent with the Resource Protection Ordinance (RPO) (County Code, Section 86.601 et seq.).
- 4) Find that plans and documentation have been prepared for the proposed project that demonstrate that the project complies with the Watershed Protection, Stormwater Management, and Discharge Control Ordinance (County Code, Section 67.801 et seq.).
- 5) Find that the project is consistent with the Multiple Species Conservation Plan (MSCP), the County Subarea Plan and the Biological Mitigation Ordinance (County Code, section 86.501 et seq.) as explained in the MSCP Conformance Statement dated July 22, 2019 on file with Planning & Development Services as Environmental Review Number PDS2015-ER-15-14-010.

**Attachment B –
MAJOR USE PERMIT DECISION
PDS2015-MUP-15-027**



County of San Diego
PLANNING & DEVELOPMENT SERVICES

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Director
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Assistant Director

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COMMISSIONERS
Douglas Barnhart (Chairman)
Bryan Woods (Vice Chairman)
Michael Beck
Yolanda Calvo
Michael Edwards
David Pallinger
Michael Seiler

PERMITEE: LITERACY FIRST CHARTER SCHOOLS, INC.
MAJOR USE PERMIT: PDS2015-MUP-15-027
E.R. NUMBER: PDS2015-ER-15-14-010
PROPERTY: CHASE AVENUE AND JAMACHA ROAD, VALLE DE ORO
APN(S): 498-330-39

DECISION OF THE PLANNING COMMISSION

This Major Use Permit for Liberty Charter High School (MUP-15-027) consists of twelve (12) sheets including site plan, floor plans, photo simulations, elevations, conceptual landscape plans, and site lighting plans dated February 8, 2018. This permit authorizes the development and operation of a charter high school pursuant to Section 2105 (requiring a MUP for Major Impact Services and Utilities) and 7350 of the Zoning Ordinance.

Grant an exception to Section 4610 of the Zoning Ordinance, pursuant to section 4620(g), to allow portions of the proposed school building, specifically the architectural accent towers at the building corners and entry, to be constructed at a maximum height of 38 feet, where 35 feet is the maximum height allowed.

The granting of this use permit also approves the Preliminary Grading and Improvement Plan dated February 8, 2018 consisting of one (1) sheet. In accordance with the [Section 87.207 of the County Grading Ordinance](#), Environmental Mitigation Measures or other conditions of approval required and identified on the plan, shall be completed or implemented on the final engineering plan before any final improvement or grading plan can be approved and any permit issued in reliance of the approved plan. Any Substantial deviation therefrom the Preliminary Grading and Improvement Plan may cause the need for further environmental review. Additionally, approval of the preliminary plan does not constitute approval of a final engineering plan. A final engineering plan shall be approved pursuant to [County of San Diego Grading Ordinance \(Sec 87.701 et. al.\)](#)

MAJOR USE PERMIT EXPIRATION: This Major Use Permit shall expire on **September 18, 2022** at 4:00 p.m. (or such longer period as may be approved pursuant to Section 7376 of The Zoning Ordinance of the County of San Diego prior to said expiration date) unless construction or use in reliance on this Major Use Permit has commenced prior to said expiration date.

.....

SPECIFIC CONDITIONS: Compliance with the following Specific Conditions (Mitigation Measures when applicable) shall be established before the property can be used in reliance upon this Major Use Permit. Where specifically indicated, actions are required prior to approval of any grading, improvement, building plan and issuance of grading, construction, building, or other permits as specified:

ANY PERMIT: *(Prior to the approval of any plan, issuance of any permit, and prior to occupancy or use of the premises in reliance of this permit).*

1. GEN#1 – COST RECOVERY: [PDS, DPW, DEH, DPR], [GP, CP, BP, UO]

INTENT: In order to comply with Section 362 of Article XX of the San Diego County Administrative Code, Schedule B.5, existing deficit accounts associated with processing this permit shall be paid. **DESCRIPTION OF REQUIREMENT:** The applicant shall pay off all existing deficit accounts associated with processing this permit. **DOCUMENTATION:** The applicant shall provide a receipt to Planning & Development Services, Zoning Counter, which shows that all discretionary deposit accounts have been paid. No permit can be issued if there are deficit deposit accounts. **TIMING:** Prior to the approval of any plan and prior to the issuance of any permit and prior to use in reliance of this permit, all fees and discretionary deposit accounts shall be paid. **MONITORING:** The PDS Zoning Counter shall review the receipts and verify that all PDS, DPW, DEH, and DPR deposit accounts have been paid.

2. GEN#2 – RECORDATION OF DECISION: [PDS], [GP, CP, BP, UO]

INTENT: In order to comply with Section 7019 of the Zoning Ordinance, the Permit Decision shall be recorded to provide constructive notice to all purchasers, transferees, or other successors to the interests of the owners named, of the rights and obligations created by this permit. **DESCRIPTION OF REQUIREMENT:** The applicant shall sign, notarize with an 'all purpose acknowledgement' and return the original Recordation Form to PDS. **DOCUMENTATION:** Signed and notarized original Recordation Form. **TIMING:** Prior to the approval of any plan and prior to the issuance of any permit and prior to use in reliance of this permit, a signed and notarized copy of the Decision shall be recorded by PDS at the County Recorder's Office. **MONITORING:** The PDS Zoning Counter shall verify that the Decision was recorded and that a copy of the recorded document is on file at PDS.

3. LNDSCP#1 – LANDSCAPE DOCUMENTATION PACKAGE

INTENT: In order to provide adequate Landscaping that provides screening, a landscape plan shall be prepared. **DESCRIPTION OF REQUIREMENT:** The Landscape Plans shall be prepared pursuant to the [COSD Water Efficient Landscape Design Manual](#) and the [COSD Water Conservation in Landscaping Ordinance](#), the [COSD Parking Design Manual](#), the COSD Climate Action Plan and the COSD Grading Ordinance. All Plans shall be prepared by a California licensed Landscape Architect, Architect, or Civil Engineer, and include the following information:

- a. Indication of the proposed width of any adjacent public right-of-way, and the locations of any required improvements and any proposed plant materials to be installed or planted therein. The applicant shall also obtain a permit approving the variety, location, and spacing of all trees proposed to be planted within said right(s) -of-way. A copy of this permit and a letter stating that all landscaping within

- the said right(s) -of-way shall be maintained by the landowner(s) shall be submitted to PDS.
- b. A complete planting plan including the names, sizes, and locations of all plant materials, including trees, shrubs, and groundcover. Wherever appropriate, native or naturalizing plant materials shall be used which can thrive on natural moisture. These plants shall be irrigated only to establish the plantings.
 - c. A complete watering system including the location, size, and type of all backflow prevention devices, pressure, and non-pressure water lines, valves, and sprinkler heads in those areas requiring a permanent, and/or temporary irrigation system.
 - d. The watering system configuration shall indicate how water flow, including irrigation runoff, low head drainage, overspray or other similar conditions will not impact adjacent property, non-irrigated areas, structures, walkways, roadways or other paved areas, including trails and pathways by causing water to flow across, or onto these areas.
 - e. Spot elevations of the hardscape, building and proposed fine grading of the installed landscape.
 - f. The location and detail of all walls, fences, and walkways shall be shown on the plans, including height from grade and type of material. A lighting plan and light standard details shall be included in the plans (if applicable) and shall be in compliance with the [County's Light Pollution Code](#).
 - g. No landscaping material or irrigation or other infrastructure shall be located within a proposed trail easement or designated pathway.
 - h. Parking areas shall be landscaped and designed pursuant to the [Parking Design Manual](#) and the County Zoning Ordinance Section 6793.b
 - i. Additionally, the following items shall be addressed as part of the Landscape Documentation Package:
 - j. All required street trees shall be planted outside of the public right-of-way on private property. If tree planting is proposed within the public right-of-way, a copy of an Encroachment Maintenance and Removal Agreement issued by the Department of Public Works shall be included with this submittal.
 - k. Screening shrubs, mature height reaching 6 feet minimum, 15 gallon minimum container size, low water use, such as *Arbutus unedo* (Strawberry Tree) and *Heteromeles Arbutifolia* (Toyon) shall be provided at perimeter planting areas on the south, the west and especially the east, where adjacent to viewing areas from Scenic Highway Jamacha Road.
 - l. Provide a six-foot-high solid fence or wall at the west and south property lines of the site. Provide construction details on the landscape plans reflecting the proposed condition.
 - m. All proposed planting shall be clearly delineated on the planting plan and noted in the plant legend with one unique symbol for each tree, shrub, vine, and one unique hatch for each ground cover plant etc.
 - n. Please be aware that the County's Climate Action Plan (CAP) was approved last February (2/14/18) and now requires an ETAF value of 0.42 be used within the MAWA formula instead of the current 0.55 for residential projects. Measure W-1.2 (Reduce Outdoor Water Use) of the CAP requires a 40% reduction in outdoor

water use as compared to the baseline year of the CAP (2014). The ETAF used during 2014 was 0.7. 40% of that is 0.42. The calculations on the Water Efficient Landscape Worksheet, submitted during Final Landscape Plans, must reflect the 0.42 value.

DOCUMENTATION: The applicant shall prepare the Landscape Plans using the [Landscape Documentation Package Checklist](#) (PDS Form #404), and pay all applicable review fees. **TIMING:** Prior to approval of any plan, issuance of any permit, and prior to use of the premises in reliance of this permit, the Landscape Documentation Package shall be prepared and approved. **MONITORING:** The [PDS, LA] and [DPR, TC, PP] shall review the Landscape Documentation Package for compliance with this condition.

4. CULT#1 ARCHAEOLOGICAL MONITORING [PDS, FEE X 2]

INTENT: In order to mitigate for potential impacts to undiscovered buried archaeological resources, an Archaeological Monitoring Program and potential Data Recovery Program shall be implemented pursuant to the County of San Diego Guidelines for Determining Significance for Cultural Resources and the California Environmental Quality Act (CEQA).

DESCRIPTION OF REQUIREMENT: A County Approved Principal Investigator (PI) known as the "Project Archaeologist," shall be contracted to perform archaeological monitoring and a potential data recovery program during all grading, clearing, grubbing, trenching, and construction activities. The archaeological monitoring program shall include the following:

- a. The Project Archaeologist shall perform the monitoring duties before, during and after construction pursuant to the most current version of the County of San Diego Guidelines for Determining Significance and Report Format and Requirements for Cultural Resources. The Project Archaeologist and Kumeyaay Native American monitor shall also evaluate fill soils to determine that they are clean of cultural resources. The contract or letter of acceptance provided to the County shall include an agreement that the archaeological monitoring will be completed, and a Memorandum of Understanding (MOU) between the Project Archaeologist and the County of San Diego shall be executed. The contract or letter acceptance shall include a cost estimate for the monitoring work and reporting.
- b. The Project Archeologist shall provide evidence that a Kumeyaay Native American has been contracted to perform Native American Monitoring for the project.
- c. The cost of the monitoring shall be added to the grading bonds or bonded separately.

DOCUMENTATION: The applicant shall provide a copy of the Archaeological Monitoring Contract or letter of acceptance, cost estimate, and MOU to the [PDS, PPD]. Additionally, the cost amount of the monitoring work shall be added to the grading bond cost estimate.

TIMING: Prior to approval of any grading and or improvement plans and issuance of any Grading or Construction Permits. **MONITORING:** The [PDS, PPD] shall review the contract or letter of acceptance, MOU and cost estimate or separate bonds for compliance with this condition. The cost estimate should be forwarded to [PDS, LDR], for inclusion in the grading bond cost estimate, and grading bonds and the grading monitoring requirement shall be made a condition of the issuance of the grading or construction permit.

5. BIO#1 - OFF-SITE MITIGATION – NON-NATIVE GRASSLAND

INTENT: In order to mitigate for the impacts to 5.26 acres of Non-native grassland habitat, which is a sensitive biological resource pursuant to CEQA, and the [Biological Mitigation Ordinance \(BMO\)](#), off-site mitigation (0.5:1 ratio) shall be acquired.

DESCRIPTION OF REQUIREMENT: The applicant shall purchase habitat credit or provide for the conservation of 2.62 acres of Tier III habitat or higher, located within the South County MSCP Subarea as indicated below.

- a. **Option 1:** If purchasing Mitigation Credit, the mitigation bank shall be approved by the California Department of Fish & Wildlife. The following evidence of purchase shall include the following information to be provided by the mitigation bank:
 1. A copy of the purchase contract referencing the project name and numbers for which the habitat credits were purchased.
 2. Submit a Bill of Sale or Credit Sales receipt.
 3. If not stated explicitly in the purchase contract, a separate letter must be provided identifying the entity responsible for the long-term management and monitoring of the preserved land.
 4. To ensure the land will be protected in perpetuity, evidence must be provided that a dedicated conservation easement or similar land constraint has been placed over the mitigation land.
 5. An accounting of the status of the mitigation bank. This shall include the total amount of credits available at the bank, the amount required by this project and the amount remaining after utilization by this project.
- b. **Option 2:** If habitat credit cannot be purchased in a mitigation bank, then the applicant shall provide for the conservation habitat of the same amount and type of land located within the South County MSCP Subarea in San Diego County as indicated below:
 1. Prior to purchasing the land for the proposed mitigation, the location should be pre-approved by [PDS].
 2. A Resource Management Plan (RMP) shall be prepared and approved pursuant to the County of San Diego Biological Report Format and Content Requirements to the satisfaction of the Director of PDS. If the offsite mitigation is proposed to be managed by DPR, the RMP shall also be prepared and approved to the satisfaction of the Director of DPR.
 3. An open space easement over the land shall be dedicated to the County of San Diego or like agency to the satisfaction of the Director of PDS. The land shall be protected in perpetuity.
 4. The purchase and dedication of the land and the selection of the Resource Manager and establishment of an endowment to ensure funding of annual ongoing basic stewardship costs shall be complete prior to the approval of the RMP.

5. In lieu of providing a private habitat manager, the applicant may contract with a federal, state or local government agency with the primary mission of resource management to take fee title and manage the mitigation land). Evidence of satisfaction must include a copy of the contract with the agency, and a written statement from the agency that (1) the land contains the specified acreage and the specified habitat, or like functioning habitat, and (2) the land will be managed by the agency for conservation of natural resources in perpetuity.

DOCUMENTATION: The applicant shall purchase the off-site mitigation credits and provide the evidence to the [PDS, PPD] for review and approval. If the offsite mitigation is proposed to be owned or managed by DPR, the applicant must provide evidence to the [PDS PPD] that [DPR, GPM] agrees to this proposal. It is recommended that the applicant submit the mitigation proposal to the [PDS, PPD], for pre-approval. If option #2 is selected, a RMP shall be prepared and an application for the RMP shall be submitted to the [PDS, ZONING]. **TIMING:** Prior to approval of any plan or issuance of any permit, and prior to use of the premises in reliance of this permit, the mitigation shall occur. **MONITORING:** The [PDS, PPD] shall review the mitigation purchase for compliance with this condition. Upon request from the applicant [PDS, PPD] can pre-approve the location and type of mitigation only. The credits shall be purchased before the requirement can be completed. If the applicant chooses option #2, then the [PDS, ZONING] shall accept an application for an RMP, and [PDS, PPD] [DPR, GPM] shall review the RMP submittal for compliance with this condition and the RMP Guidelines.

6. ROADS#1–ENCROACHMENT PERMIT

INTENT: In order to ensure that improvements for the intersection of Chase Avenue (SA 910.1) and Jamacha Road (SF 1399) comply with the County of San Diego Public Road Standards, and The Caltrans Facility Standards and Requirements an encroachment permit(s) shall be obtained and implemented. **DESCRIPTION OF REQUIREMENT:** A permit shall be obtained from CALTRANS for the improvements to be made within the Caltrans' right-of-way. The improvements will include the upgrade of the existing pedestrian curb ramps, replacement of the existing pedestrian signal heads to be countdown and the replacement of the existing pedestrian push buttons to meet ADA requirements. A copy of the permit and evidence from the issuing agency that all requirements of the permit have been met shall be submitted to the [PDS, LDR]. **DOCUMENTATION:** The applicant shall obtain the encroachment permit(s) and provide a copy of the permit, proof of payment, and evidence that all the requirements of the permit have been met, to the [PDS, LDR]. The applicant should contact the CALTRANS Permit Office at (619) 688-6843 for additional details. **TIMING:** Prior to occupancy or use of the premises in reliance of this permit, the encroachment permit shall be obtained. **MONITORING:** The [PDS, LDR] shall review the permit for compliance with this condition and the applicable improvement plans and implement any conditions of the permit in the County improvement plans.

7. ROADS#2–ROAD DEDICATION

INTENT: In order to promote orderly development and to comply with the [County of San Diego Board Policy I-18](#), the [County Public Road Standards](#), and the [Community Trails Master Plan](#), road right of way shall be dedicated to the County. **DESCRIPTION OF REQUIREMENT:** Grant by separate document to the County of San Diego an easement for road purposes that provides:

- a. A one-half right-of-way width of fifty-five feet (55') from centerline, plus slope rights and drainage easements for **Chase Avenue (SA 910.1)** along the frontage of the project in accordance with Mobility Element 4.1B Major Road Standards.
- b. A one-half right-of-way width of sixty-seven feet (67') from centerline, plus slope rights and drainage easements for **Jamacha Road (SF 1399)** along the frontage of the project in accordance with Mobility Element 6.2 Prime Arterial Standards.
- c. A thirty-foot (30') radius at the intersection of Chase Avenue and Jamacha Road.

The grant of right-of-way shall be free of any burdens or encumbrances, which would interfere with the purpose for which it is required and shall be accepted for public use. The affected utility company/district shall enter into a joint use agreement with the County of San Diego to the satisfaction of the Director of PDS. **DOCUMENTATION:** The applicant shall prepare the legal descriptions of the easements, and submit them for preparation with the [DGS, RP], and pay all applicable fees associated with preparation of the documents. Upon Recordation of the easements, the applicant shall provide copies of the easement documents to the [PDS, LDR] for review. **TIMING:** Prior to approval of any plan or issuance of any permit, and prior to use of the premises in reliance of this permit the easements shall be executed and recorded. **MONITORING:** The [DGS, RP] shall prepare, approve the easement documents for recordation, and forward the recorded copies to [PDS, LDR] for review and approval. The [PDS, LDR] shall review the easements to assure compliance with this condition.

8. ROADS#3–ROAD IMPROVEMENTS

INTENT: In order to promote orderly development and to comply with the [County of San Diego Board Policy I-18](#), the [County Public Road Standards](#), and the [County Community Trails Master Plan](#), *Chase Avenue* shall be improved. **DESCRIPTION OF REQUIREMENT:**

- a. Improve or agree to improve and provide security for **Chase Avenue (SA 910.1)**, along the project frontage in accordance with Public Road Standards for a 4.1B Major Road with a Class IV Bikeway and a raised median with intermittent turn lanes, to a graded width of fifty-five feet (55') and to an improved width of forty-five feet (45') from centerline with asphalt concrete pavement over approved base with Portland cement concrete curb, gutter and sidewalk. The face of curb shall be located forty-five feet (45') from official centerline. A Parking Prohibition shall be proposed on both sides of **Chase Avenue (SA 910.1)** along the project frontage. Provide signage and striping for school zone. Provide transition, tapers, traffic striping to match existing pavement. All of the above shall be to the satisfaction of the Directors of Planning & Development Services (PDS) and the Department of Public Works (DPW).
- b. Improve or agree to improve and provide security for the intersection of **Chase Avenue (SA 910.1)** and **Jamacha Road (SF 1399)** in accordance with Public Road Standards, CALTRANS standards and requirements and Americans with Disabilities Act (ADA) standards to include ADA pedestrian ramps at all four corners, crosswalks, markings, signage, striping, countdown pedestrian traffic signal heads, and pedestrian push buttons with audible functionality. Provide transition, tapers, traffic striping to match existing pavement. All of the above shall be to the satisfaction of the Directors of PDS, DPW and CALTRANS.

- c. Asphalt concrete surfacing material shall be hand-raked and compacted to form smooth tapered connections along all edges including those edges adjacent to soil. The edges of asphalt concrete shall be hand-raked at 45 degrees or flatter, so as to provide a smooth transition next to existing soil, including those areas scheduled for shoulder backing.

All plans and improvements shall be completed pursuant to the [County of San Diego Public Road Standards](#), the PDS [Land Development Improvement Plan Checking Manual](#) and the Community Trails Master Plan. The improvements shall be completed within 24 months from the approval of the improvement plans, execution of the agreements, and acceptance of the securities. **DOCUMENTATION:** The applicant shall complete the following:

- d. Process and obtain approval of Improvement Plans to improve Chase Avenue (SA 910.1).
- c. Process and obtain approval of Improvement Plans to improve the intersection of **Chase Avenue (SA 910.1)** and **Jamacha Road (SF 1399)**.
- d. Provide Secured agreements. The required security shall be in accordance with [Section 7613 of the Zoning Ordinance](#).
- e. Pay all applicable inspection fees with [DPW, PDC].
- f. If the applicant is a representative, then one of the following is required: a corporate certificate indicating those corporation officers authorized to sign for the corporation, or a partnership agreement recorded in this County indicating who is authorized to sign for the partnership.
- g. Obtain approval for the design and construction of all driveways, turnarounds, pathways and private easement road improvements to the satisfaction of the San Miguel Fire Protection District and the [PDS, LDR].
- h. Obtain a Construction Permit for any work within the County road right-of-way. PDS Construction/Road right-of-way Permits Services Section should be contacted at (858) 694-3275 to coordinate departmental requirements. Also, before trimming, removing or planting trees or shrubs in the County Road right-of-way, the applicant must first obtain a permit to remove, plant or trim shrubs or trees from the Permit Services Section.

TIMING: Prior to approval of any occupancy permits, and prior to use of the premises in reliance of this permit, the plans shall be approved and securities must be provided.

MONITORING: The [PDS, LDR] and [DPR, TC] shall review the plans for consistency with the condition and County Standards and Community Trails Master Plan. Upon approval of the plans [PDS, LDR] shall request the required securities and improvement agreements. The securities and improvement agreements shall be approved by the Director of PDS before any work can commence.

9. ROADS#4–SIGHT DISTANCE

INTENT: In order to provide an unobstructed view for safety while exiting the property and accessing a public road from the site, and to comply with the Design Standards of Section 6.1.(E) of the [County of San Diego Public Road Standards](#), an unobstructed sight distance shall be verified. **DESCRIPTION OF REQUIREMENT:**

- a. A registered civil engineer or a licensed land surveyor provides a certified signed statement that: "There is _____ feet of unobstructed intersectional sight distance in both directions from each of the proposed driveways along **Chase Avenue (SA 910.1)** in accordance with the methodology described in Table 5 of the March 2012 County of San Diego Public Road Standards. These sight distances exceed the required intersectional Sight Distance requirements of _____ as described in Table 5 based on a speed of _____, which I have verified to be the higher of the prevailing speed or the minimum design speed of the road classification. I have exercised responsible charge for the certification as defined in Section 6703 of the Professional Engineers Act of the California Business and Professions Code."
- b. If the lines of sight fall within the existing public road right-of-way, the engineer or surveyor shall further certify: "Said lines of sight fall within the existing right-of-way and a clear space easement is not required."

DOCUMENTATION: The applicant shall complete the certifications and submit them to the [PDS, LDR] for review. **TIMING:** Prior to the approval of any plan, issuance of any permit, and prior to occupancy or use of the premises in reliance of this permit, the sight distance shall be verified. **MONITORING:** The [PDS, LDR] shall verify the sight distance certifications.

10. ROADS#5–RELINQUISH ACCESS

INTENT: In order to promote orderly development and to comply with the [Mobility Element of the General Plan](#) access shall be relinquished. **DESCRIPTION OF REQUIREMENT:**

- a. Relinquish access rights onto **Chase Avenue (SA 910.1)** and **Jamacha Road (SF 1399)** with the exception of the driveways as shown on the approved plot plan. The access relinquishment shall be free of any burdens or encumbrances, which would interfere with the purpose for which it is required. Only the three access points are permitted along **Chase Avenue (SA 910.1)** as indicated on the approved plot plan.
- b. The access relinquishment shall be free of any burdens or encumbrances, which would interfere with the purpose for which it is required.

DOCUMENTATION: The applicant shall prepare the legal descriptions of the easement(s), submit them for preparation with the [DGS, RP], and pay all applicable fees associated with preparation of the documents. **TIMING:** Prior to approval of any plan or issuance of any permit, and prior to use of the premises in reliance of this permit the access shall be relinquished. **MONITORING:** The [DGS, RP] shall prepare the relinquishment documents and forward a copy of the documents to [PDS, LDR] for preapproval. [DGS, RP] shall forward copies of the recorded documents to [PDS, LDR]. The [PDS, LDR] shall review the documents for compliance with this condition.

11. UTILITIES#1–PAVEMENT CUT POLICY

INTENT: In order to prohibit trench cuts for undergrounding of utilities in all new, reconstructed, or resurfaced paved County-maintained roads for a period of three years following project surfacing, and to comply with [County Policy RO-7](#) adjacent property owners shall be notified and solicited for their participation in the extension of utilities.

DESCRIPTION OF REQUIREMENT: All adjacent property owners shall be notified who may be affected by this policy and are considering development of applicable properties, this includes requesting their participation in the extension of utilities to comply with this policy. No trench cuts for undergrounding of utilities in all new, reconstructed, or resurfaced paved County-maintained roads for a period of three years following project surface. **DOCUMENTATION:** The applicant shall sign a statement that they are aware of the County of San Diego Pavement Cut Policy and submit it to the [PDS LDR] for review. **TIMING:** Prior to approval of any grading or improvement plan and prior to issuance of any grading or construction permit, and prior to use of the property in reliance of this permit, the letters must be submitted for approval. **MONITORING:** [PDS, LDR] shall review the signed letters to determine compliance with the condition.

GRADING PERMIT: (Prior to approval of any grading and or improvement plans and issuance of any Grading or Construction Permits).

12. PLN#1 – PLAN CONFORMANCE: [DPW, ESU] [GP, IP,] [DPR, TC, PP]

INTENT: In order to implement the required mitigation measures for the project, the required plot plans shall conform to the approved Conceptual Grading and Development Plan pursuant to Section 87.207 of the County Grading Ordinance. **DESCRIPTION OF REQUIREMENT:** The plot plans shall conform to the approved Conceptual Grading and Development Plan. All conditions, requirements, mitigation measures and information stated on the sheets of the plans shall be made conditions of the permit's issuance and shall be implemented pursuant to the adopted Mitigation Monitoring and Reporting Program (MMRP) of this Permit. No deviation of the requirements can be made without modification of this permit. **DOCUMENTATION:** The applicant shall submit the grading plans and improvement plans, which conform to the conceptual development plan for the project. **TIMING:** Prior to approval of any grading or improvement plan and prior to issuance of any grading or construction permit, the notes and items shall be placed on the plans as required. **MONITORING:** The [DPW, Environmental Services Unit Division, DPR, TC, or PDS, Building Division for Minor Grading] shall verify that the grading and or improvement plan requirements have been implemented on the final grading and or improvement plans as applicable. The environmental mitigation notes shall be made conditions of the issuance of said grading or construction permit.

13. STRMWTR#1–STORMWATER FACILITIES MAINTENANCE AGREEMENT

INTENT: In order to promote orderly development and to comply with the [County Flood Damage Prevention Ordinance 10091 \(Title 8, Division 11\)](#), and the [County Watershed Protection Ordinance \(WPO\) No.10385, County Code Section 67.801 et. seq.](#), the maintenance agreements shall be completed. **DESCRIPTION OF REQUIREMENT:**

- a. The private storm drain system shall be maintained by a maintenance mechanism such as a homeowners association or other private entity to the satisfaction of the Director of DPW and/or PDS.
- b. Establish a maintenance agreement / mechanism (to include easements) to assure maintenance of the Category 2 post-construction best management practices

(BMP's). Provide security to back up the maintenance pursuant to the County Maintenance Plan Guidelines to the satisfaction of the Director of DPW and/or PDS.

DOCUMENTATION: The applicant shall process the agreement forms with [PDS, LDR] and pay the deposit and applicable review fees. **TIMING:** Prior to approval of any grading or improvement plan or construction permit, prior to use of the property in reliance of this permit; execution of the agreements and securities shall be completed and recorded. **MONITORING:** The [PDS, LDR] shall review the agreements/mechanisms for consistency with the condition and County Standards.

14. STRMWTR#2-EROSION CONTROL

INTENT: In order to Comply with all applicable stormwater regulations the activities proposed under this application are subject to enforcement under permits from the [State Construction General Permit, Order No. 2009-00090-DWQ](#), or subsequent order and the [County Watershed Protection Ordinance \(WPO\) No.10410, County Code Section 67.801 et. seq.](#), and all other applicable ordinances and standards for this priority project.

DESCRIPTION OF REQUIREMENT: The applicant shall maintain the appropriate on-site and offsite Best Management Practices pursuant to the approved Stormwater Quality Management Plan (SWQMP) and Erosion Control Plan including, but not limited to the erosion control measures, irrigation systems, slope protection, drainage systems, desilting basins, energy dissipators, and silt control measure.

- a. An agreement and instrument of credit shall be provided for an amount equal to the cost of this work as determined or approved by the [PDS, LDR], in accordance with the [County of San Diego Grading Ordinance Section 87.304](#). The cash deposit collected for grading, per the grading ordinance, will be used for emergency erosion measures. The developer shall submit a letter to [PDS, LDR] authorizing the use of this deposit for emergency measures.
- b. An agreement in a form satisfactory to County Counsel shall accompany the Instrument of Credit to authorize the County to unilaterally withdraw any part of or all the Instrument of Credit to accomplish any of the work agreed to if it is not accomplished to the satisfaction of the County PDS and/or DPW by the date agreed.

DOCUMENTATION: The applicant shall process an Erosion Control Plan and provide the letter of agreement and any additional security and/or cash deposit to the [PDS, LDR].

TIMING: Prior to approval of any grading or improvement plan or construction permit, and prior to use of the property in reliance of this permit, the Erosion Control Plan shall be approved and the agreement and securities shall be executed. **MONITORING:** The [PDS, LDR] shall ensure that the Erosion Control Plan adequately satisfies the requirements of the conditions to potentially perform the required erosion control and stormwater control measures proposed on all construction and grading plans. [DPW, PDCI] shall use the securities pursuant to the agreement to implement and enforce the required stormwater and erosion control measures pursuant to this condition during all construction phases as long as there are open and valid permits for the site.

15. AQ#1 – CONSTRUCTION EXHAUST EMISSIONS

INTENT: In order to mitigate for exhaust emissions. **DESCRIPTION OF REQUIREMENT:** The project shall comply with the following Air Quality measure:

- a. The project shall require the construction contractor to provide a construction fleet that uses heavy-duty diesel-powered equipment equipped with Tier 4 Final diesel engines. An exemption from these requirements may be granted by the County in the event that the applicant documents that equipment with the required tier is not reasonably available and corresponding reductions in criteria air pollutant emissions are achieved from other construction equipment. Before an exemption may be considered by the County, the applicant shall be required to demonstrate that three construction fleet owners/operators in the San Diego Region were contacted and that those owners/operators confirmed Tier 4 equipment could not be located within the San Diego region.
- b. The project shall require the construction contractor to provide a construction fleet that uses heavy-duty diesel-powered equipment equipped with diesel particulate filters (DPFs).
- c. Construction equipment shall be outfitted with best available control technology (BACT) devices certified by the California Air Resources Board. A copy of each unit's BACT documentation shall be provided to the County of San Diego at the time of mobilization of each applicable unit of equipment.

DOCUMENTATION: The applicant shall comply with the Air Quality requirements of this condition. The applicant shall show compliance with this measure by providing the construction bid/estimate from the construction contractor that will be used. **TIMING:** Prior to approval of any grading and or improvement plans and issuance of any Grading or Construction Permits. **MONITORING:** The [DPW, PDCI] shall make sure that the construction contractor complies with the Air Quality requirement of this condition. The [DPW, PDCI] shall contact the [PDS, PCC] if the applicant fails to comply with this condition.

16. AQ#2 – FUGITIVE DUST

INTENT: In order to mitigate for fugitive dust during construction activities.

DESCRIPTION OF REQUIREMENT: The project shall comply with the following Air Quality measures:

- a. All haul/dump trucks entering or leaving the site with soil or fill material must maintain at least two (2) feet of freeboard, cover loads of all haul/dump trucks securely, or secure transported material by watering or treating.
- b. Dust control measures of the Grading Ordinance will be enhanced with a minimum of two (2) daily applications of water to the construction areas between dozer/scrapper passes and on any unpaved roads within the project limits.
- c. Grading is to be terminated in winds in excess of 25 miles per hour (mph).
- d. Sweepers and water trucks shall be used to control dust and debris at public street access points and approach routes to construction sites.
- e. Dirt storage piles will be stabilized by chemical binders, tarps, fencing, or other suppression measures.
- f. Internal construction-roadways will be stabilized by paving, chip sealing or chemicals after rough grading.
- g. A 15-mph speed limit on unpaved surfaces shall be enforced.
- h. Disturbed areas shall be hydroseeded, landscaped, or developed as quickly as possible and as directed by the County and/or SDAPCD to reduce dust generation.
- i. Visible roadway dust as a result of active operations, spillage from transport trucks, erosion, or track-out/carry-out shall be minimized by the use of track-out grates,

gravel beds, or wheel-washing at each egress point and be removed at the conclusion of each work day when active operations cease, or every 24 hours for continuous operations. If a street sweeper is used to remove any track-out/carry-out, only respirable particulate matter (PM₁₀) -efficient street sweepers certified to meet the most current South Coast Air Quality Management District (SCAQMD) Rule 1186 requirements shall be used. The use of blowers for removal of track-out/carry-out is prohibited under any circumstances.

DOCUMENTATION: The applicant or designee shall comply with the Air Quality requirements of this condition. **TIMING:** The following actions shall occur throughout the duration of construction and grading. **MONITORING:** The [DPW, PDC] shall make sure that the grading contractor complies with the Air Quality requirements of this condition. The [DPW, PDC] shall contact the [PDS, PCC] if the applicant fails to comply with this condition.

BUILDING PERMIT: (Prior to approval of any building plan and the issuance of any building permit).

17. TRAF#1 – TRANSPORTATION IMPACT FEE: [PDS, LDR] [PDS, BD] [BP]

INTENT: In order to mitigate potential cumulative traffic impacts to less than significant, and to comply with the Transportation Impact Fee (TIF) Ordinance Number 77.201-77.219, the TIF shall be paid. **DESCRIPTION OF REQUIREMENT:** The Transportation Impact Fee (TIF) shall be paid pursuant to the County TIF Ordinance number 77.201-77.219 for Select Industrial Uses in the area based on Average Daily Trips (ADT) identified in the approved Traffic Study. The fee is required for the entire project, or it can be paid at building permit issuance for each phase of the project. The fee is calculated pursuant to the ordinance. **DOCUMENTATION:** The applicant shall pay the TIF at the [PDS, ZONING] and provide a copy of the receipt to the [PDS, Building Division Technician] at time of permit issuance. The cost of the fee shall be calculated at time of payment. **TIMING:** Prior to approval of any building plan and the issuance of any building permit or use of the premises in reliance of this permit, the TIF shall be paid. **MONITORING:** The [PDS, ZONING] shall calculate the fee pursuant to the ordinance and provide a receipt of payment for the applicant. [PDS, Building Division] shall verify that the TIF has been paid before the first building permit can be issued. The TIF shall be verified for each subsequent building permit issuance.

18. NOISE#1 - NOISE REQUIREMENT

INTENT: In order to comply with the [County of San Diego Noise Ordinance 36.404](#) as evaluated in the [County of San Diego Noise Guidelines for Determining Significance](#), the following design measures shall be implemented on the building plans and in the site design. **DESCRIPTION OF REQUIREMENT:** The following design elements and noise attenuation measures shall be implemented and indicated on the building plans and made conditions of its issuance: dual-pane windows with a minimum sound transmission class (STC) rating of 26 and mechanical ventilation as identified in the Noise Study prepared by LDN Engineering (August 3, 2017). **DOCUMENTATION:** The applicant shall place the design elements or notes on the building plans and submit the plans to [PDS, BPPR] for review and approval. **TIMING:** Prior to issuance of any building permit, the design elements and noise attenuation measures shall be incorporated into the building plans. **MONITORING:** The [PDS, BPPR] shall verify that the specific note(s), and design

elements, and noise attenuation measures have been placed on all sets of the building plans and made conditions of its issuance.

19. NOISE#2 - NOISE REQUIREMENT – MECHANICAL

INTENT: In order to reduce the impacts of the exterior sound levels from the project HVAC units on the adjacent parcels and to comply with the County of San Diego Noise Ordinance, Section 36.404, and the County of San Diego Noise Guidelines for Determining Significance, the following design measures shall be implemented on the building plans and in the site design. **DESCRIPTION OF REQUIREMENT:** The following design elements and noise attenuation measures shall be implemented and indicated on the building plans and made conditions of its issuance: HVAC units associated with the building shall incorporate visual screening by a parapet wall or barrier of equal or greater height in relation to the HVAC unit's elevation for noise screening purposes for the southern property line. **DOCUMENTATION:** The applicant shall place the design elements or notes on the building plans and submit the plans to [PDS, BPPR] for review and approval. **TIMING:** Prior to issuance of any building permit, the design elements and noise attenuation measures shall be incorporated into the building plans. **MONITORING:** The [PDS, BPPR] shall verify that the specific note(s), and design elements, and noise attenuation measures have been placed on all sets of the building plans and made conditions of its issuance.

20. AQ#3 – CONSTRUCTION ARCHITECTURAL COATINGS

INTENT: In order to reduce emissions of volatile organic compounds (VOC). **DESCRIPTION OF REQUIREMENT:** The project shall comply with the following Air Quality measure:

- a. Residential interior VOC coatings are to be less than or equal to 50 grams per liter (g/L).
- b. Residential exterior coatings are to be less than or equal to 100 g/L.
- c. Non-residential interior/exterior coatings are to be less than or equal to 100 g/L.

DOCUMENTATION: The applicant shall comply with the Air Quality requirements of this condition. **TIMING:** The following action shall occur throughout the duration of the construction activities involving the application of architectural coatings. **MONITORING:** The [DPW, PDCI] shall make sure that the construction contractor complies with the Air Quality requirement of this condition. The [DPW, PDCI] shall contact the [PDS, PCC] if the applicant fails to comply with this condition.

21. AQ#4 – WATER AND WASTE REDUCTION

INTENT: In order to reduce greenhouse gases (GHGs), the project shall include the following features. **DESCRIPTION OF REQUIREMENT:** The project shall implement or install the following measures to under County Ordinance Sections 68.508 through 68.518 and 2016 CALGreen Standards:

- a. In accordance with Part 11 of Title 24, California Green Building Code (CALGreen) criteria and state and local laws, at least 50 percent of operational waste would be diverted from landfills through reuse and recycling.
- b. Provide areas for storage and collection of recyclables and yard waste in accordance with 2016 CALGreen.

- c. The Project would provide a 20 percent water reduction from the statewide average in accordance with 2016 CALGreen.

DOCUMENTATION: The applicant or designee shall comply with the Air Quality requirements of this condition. **TIMING:** The following actions shall occur throughout the duration of project operations. **MONITORING:** The [PDS, PC] shall review building plans to ensure that the project meets the requirements of these conditions.

22. AQ#5 – SUSTAINABILITY DESIGN FEATURES

INTENT: In order to reduce emissions of greenhouse gases. **DESCRIPTION OF REQUIREMENT:** The project shall comply with the following Air Quality and Greenhouse Gas measures:

- a. The Project will be designed to meet 2016 Title 24 energy efficiency standards assuming construction begins before January 1, 2020. If constructed after January 1, 2020, the project would be required to meet the more energy efficient 2019 Title 24 standards.
- b. The Project will install, at least, a 90-kilowatt (kW) photovoltaic (PV) system.
- c. The Project will install, at least, two (2) Level II electric vehicle (EV) charging stations within the project parking area.

DOCUMENTATION: The applicant or designee shall comply with the Air Quality requirements of this condition. **TIMING:** The following actions shall occur throughout the duration of project operations. **MONITORING:** The [PDS, PC] shall review the building plans to ensure the project meets the requirements of this condition.

23. AQ#6 – COUNTY CLIMATE ACTION PLAN SATISFACTION

INTENT: In order to comply with the County's Climate Action Plan (CAP) measures, the project shall include the following features. **DESCRIPTION OF REQUIREMENT:** The project shall implement or install the following measures or features as required within Step 2 of the CAP Measure Consistency Checklist.

- a. **Measure 2:** The project would incorporate measures to reduce commute vehicle miles traveled (VMT) through a combination of transportation demand management (TDM) and parking strategies. Strategies implemented by the project will include:
 - i. The project will promote carpooling to and from the project site to reduce the number of staff commute and student pick-up/drop-off trips.
 - ii. The project will provide carpool- and vanpool-only parking spaces which would encourage commuters to carpool and vanpool to the Project site.
 - iii. The project will install two (2) Level II electric vehicle charging stations on-site.
 - iv. The project will install bicycle parking spaces in the form of bicycle racks which would provide bicycle parking for students and staff utilizing nearby bike routes.
- b. **Measure 6:** The project shall submit a Landscape Document Package that complies with the County's Water Conservation Ordinance and demonstrates a 40 percent reduction in current Maximum Applied Water Allowance for outdoor use.

DOCUMENTATION: The applicant shall comply with the requirements of this condition.

TIMING: Prior to the approval of the final map and prior to the approval of any plan and issuance of any permit. **MONITORING:** The [PDS, BPPR] shall verify that the Building Plans comply with **Measure 2** of this condition. The [PDS, LA] and [DPR, TC, PP] shall review the Landscape Plans for compliance with **Measure 6** of this condition.

OCCUPANCY: *(Prior to any occupancy, final grading release, or use of the premises in reliance of this permit).*

24. PLN#2 – INSPECTION FEE: [PDS, ZONING] [PDS, PCO] [UO][DPR, TC, PP].

INTENT: In order to comply with Zoning Ordinance Section 7362.e the Discretionary Inspection Fee shall be paid. **DESCRIPTION OF REQUIREMENT:** Pay the Discretionary Permit Inspection Fee at the [PDS, Zoning Counter] to cover the cost of inspection(s) of the property to monitor ongoing conditions associated with this permit. In addition, submit a letter indicating who should be contacted to schedule the inspection.

DOCUMENTATION: The applicant shall provide a receipt showing that the inspection fee has been paid along with updated contact information [PDS, PCC]. **TIMING:** Prior to any occupancy, final grading release, or use of the premises in reliance of this permit.

MONITORING: The [PDS, Zoning Counter] shall process an invoice and collect the fee for the Use Permit Compliance Inspection Fee. PDS will schedule an inspection within one year from the date that occupancy or use of the site was established.

25. PLN#3 – SITE PLAN IMPLEMENTATION [PDS, BI] [UO] [DPR, TC, PP].

INTENT: In order to comply with the approved project design indicated on the approved plot plan, the project shall be constructed as indicated on the approved building and construction plans. **DESCRIPTION OF REQUIREMENT:** The site shall conform to the approved plot plan and the building plans. This includes, but is not limited to: improving all parking areas and driveways, installing all required design features, installing all required lighting in proper locations, painting all structures with the approved colors, properly screening trash enclosures, installing required and approved signage in proper locations, including “No Parking” signage along the project frontage on the north and south sides of Chase Avenue (to the satisfaction of the Director of the Department of Public Works), installing required barrier net system on sports field, and removing all temporary construction facilities from the site. Additionally, the applicant shall work with the owner of the adjacent commercial shopping center to install “No School Parking or Drop-Off” signage within the shopping center parking lot. The applicant shall provide evidence of communication with the property owner to the County and maintain all agreed upon signage. **DOCUMENTATION:** The applicant shall ensure that the site conforms to the approved plot plan and building plans. **TIMING:** Prior to any occupancy, final grading release, or use of the premises in reliance of this permit, the site shall conform to the approved plans. **MONITORING:** The [PDS, Building Inspector] and DPR [TC, PP] shall inspect the site for compliance with the approved Building Plans.

26. ROADS#6–INSTALL STREETLIGHTS

INTENT: In order to promote orderly development and to comply with the Street Lighting Requirements of the [Centerline Ordinance 9974 \(Amended by Ord. 10224\), County Code Section 51.301 et seq., County of San Diego Board Policy I-18](#), street lights shall be installed and energized. **DESCRIPTION OF REQUIREMENT:** Install or arrange to install streetlights to County standards and the satisfaction of the Director of PDS, and deposit with PDS, a cash deposit sufficient to energize and operate the street lights until the

property has been transferred into Zone A. **DOCUMENTATION:** The applicant shall pay the Zone A Lighting District Annexation Fee at the [PDS, LDR], and arrange for the installation and energizing of the streetlights. **TIMING:** Prior to occupancy of the first structure built in association with this permit, final grading release, or use in the premises in reliance of this permit, the streetlights shall be installed, and all fees paid. **MONITORING:** The [PDS, LDR] shall calculate the fee pursuant to this condition and provide a receipt of payment for the applicant. The [PDS, LDR] shall ensure that the streetlights have been installed and all fees have been paid.

27. ROADS#7–ANNEX TO LIGHTING DISTRICT

INTENT: In order to promote orderly development and to comply with the Street Lighting Requirements of the [County of San Diego Board Policy I-18](#), and [The County of San Diego Public Road Standards](#), the property shall transfer into the lighting district. **DESCRIPTION OF REQUIREMENT:** Allow the transfer of the property subject of this permit into Zone A of the San Diego County Street Lighting District without notice or hearing and pay the cost to process such transfer. **DOCUMENTATION:** The applicant shall pay the Zone A Lighting District Annexation Fee at the [PDS, LDR]. The applicant shall provide the receipt to [PDS, PCC]. **TIMING:** Prior to occupancy of the first structure built in association with this permit or use in the premises in reliance of this permit, the fee shall be paid. **MONITORING:** The [PDS, LDR] shall calculate the fee pursuant to this condition and provide a receipt of payment for the applicant.

28. ROADS#8–SIGHT DISTANCE

INTENT: In order to provide an unobstructed view for safety while exiting the property and accessing a public road from the site, and to comply with the Design Standards of Section 6.1.E of the [County of San Diego Public Road Standards](#), an unobstructed sight distance shall be verified. **DESCRIPTION OF REQUIREMENT:**

- a. A registered civil engineer or a licensed land surveyor provides a certified signed statement that: "There is _____feet of unobstructed intersectional sight distance in the both directions from the proposed driveways along **Chase Avenue (SA 910.1)** in accordance with the methodology described in Table 5 of the March 2012 County of San Diego Public Road Standards. These sight distances exceed the required intersectional Sight Distance requirements of _____as described in Table 5 based on a speed of _____, which I have verified to be the higher of the prevailing speed or the minimum design speed of the road classification. I have exercised responsible charge for the certification as defined in Section 6703 of the Professional Engineers Act of the California Business and Professions Code."
- b. If the lines of sight fall within the existing public road right-of-way, the engineer or surveyor shall further certify: "Said lines of sight fall within the existing right-of-way and a clear space easement is not required."

DOCUMENTATION: The applicant shall complete the certifications and submit them to the [PDS, LDR] for review. **TIMING:** Prior to occupancy of the first structure built in association with this permit or use of the premises in reliance of this permit, and annually after that until the project is completely built, the sight distance shall be verified. **MONITORING:** The [PDS, LDR] shall verify the sight distance certifications for compliance with this condition.

29. STRMWTR#3–VERIFICATION OF STRUCTURAL BMPs

INTENT: In order to promote orderly development and to comply with the [County Watershed Protection Ordinance \(WPO\) No.10410, County Code Section 67.801 et. seq.](#), verification of Structural BMPs shall be completed. **DESCRIPTION OF REQUIREMENT:** Complete a Structural BMP Verification Form as shown in Attachment 10 of the PDP SWQMP. **DOCUMENTATION:** The applicant shall process the Structural BMP Verification Forms *with [DPW, PDC] or [PDS, BLDG]*. **TIMING:** Prior to any occupancy, final grading release, or use of the premises in reliance of this permit; execution of the Structural BMP Verification Form shall be completed. **MONITORING:** The *[PDS, LDR]* and *[DPW, WPP]* shall review the Structural BMP Verification Forms for consistency with the condition and County Standards.

30. STRMWTR#4–PROVISION OF STORMWATER DOCUMENTATION TO PROPERTY OWNER

INTENT: In order to promote orderly development and to comply with the [County Watershed Protection Ordinance \(WPO\) No.10410, County Code Section 67.801 et. seq.](#), stormwater documentation shall be provided to property owner. **DESCRIPTION OF REQUIREMENT:** Demonstrate that copies of the following documents are provided to the property owner and initial occupants:

- a. A copy of the project's approved SWQMP (with attached Operation & Maintenance Plan).
- b. A copy of project's recorded Maintenance Notification Agreement and/or Stormwater Facilities Maintenance Agreement and/or Private Road Maintenance Agreement showing the Structural BMPs pertaining to the property.
- c. Sample copies of the following:
 - 1) A Letter for Privately Owned Stormwater Treatment Control Best Management Practices Operation and Maintenance Verification.
 - 2) One Operation and Maintenance Verification Form for each type of Private Treatment Control BMP.

DOCUMENTATION: The applicant shall submit a letter stating that the above documentation has been submitted to the property owner and initial occupants. **TIMING:** Prior to any occupancy, final grading release, or use of the premises in reliance of this permit; provision of stormwater documents shall be completed. **MONITORING:** The *[PDS, LDR]* and *[DPW, WPP]* shall review the letter provided by the applicant for consistency with the condition and County Standards.

31. LNDSCP#2–CERTIFICATION OF INSTALLATION

INTENT: In order to provide adequate Landscaping that provides screening, and to comply with the [COSD Water Efficient Landscape Design Manual](#), the [COSD Water Conservation in Landscaping Ordinance](#), the [COSD Off-Street Parking Design Manual](#), and the COSD Grading ordinance, all landscaping shall be installed. **DESCRIPTION OF REQUIREMENT:** All landscaping shall be installed pursuant to the approved Landscape Documentation Package. This does not supersede any erosion control plantings that may be applied pursuant to [Section 87.417 and 87.418 of the County Grading Ordinance](#). These areas may be overlapping, but any requirements of a grading plan shall be complied with separately. The installation of the landscaping can be phased pursuant to construction of specific buildings or phases to the satisfaction of the *[PDS, LA, PCC]* *[DPR, TC, PP]*. **DOCUMENTATION:** The applicant shall submit to the *[PDS LA, PCC]*, a

Landscape Certificate of Completion from the project California licensed Landscape Architect, Architect, or Civil Engineer, that all landscaping has been installed as shown on the approved Landscape Documentation Package. The applicant shall prepare the Landscape Certificate of Completion using the Landscape Certificate of Completion Checklist, PDS Form #406. **TIMING:** Prior to any occupancy, final grading release, or use of the premises in reliance of this permit, the landscaping shall be installed. **MONITORING:** The [PDS, LA] shall verify the landscape installation upon notification of occupancy or use of the property, and notify the [PDS, PCC] [DPR, TC, PP] of compliance with the approved Landscape Documentation Package.

32. CULT#2 CULTURAL RESOURCES REPORT [PDS, FEE X2]

INTENT: In order to ensure that the Archaeological Monitoring occurred during the earth-disturbing activities, a final report shall be prepared. **DESCRIPTION OF REQUIREMENT:** A final Archaeological Monitoring and Data Recovery Report that documents the results, analysis, and conclusions of all phases of the Archaeological Monitoring Program shall be prepared. The report shall include the following items:

- a. DPR Primary and Archaeological Site forms.
- b. Daily Monitoring Logs
- c. Evidence that the disposition of all cultural materials collected during the survey, evaluation, and archaeological monitoring program have been completed as follows:
 - (1) All prehistoric cultural materials shall be curated at a San Diego curation facility or a culturally affiliated Tribal curation facility that meets federal standards per 36 CFR Part 79, and, therefore, would be professionally curated and made available to other archaeologists/researchers for further study. The collections and associated records, including title, shall be transferred to the San Diego curation facility or culturally affiliated Tribal curation facility and shall be accompanied by payment of the fees necessary for permanent curation. Evidence shall be in the form of a letter from the curation facility stating that the prehistoric archaeological materials have been received and that all fees have been paid.

or

Evidence that all prehistoric materials collected during the archaeological monitoring program have been returned to a Native American group of appropriate tribal affinity. Evidence shall be in the form of a letter from the Native American tribe to whom the cultural resources have been repatriated identifying that the archaeological materials have been received.

- (2) Historic materials shall be curated at a San Diego curation facility as described above and shall not be curated at a Tribal curation facility or repatriated. The collections and associated records, including title, shall be transferred to the San Diego curation facility and shall be accompanied by payment of the fees necessary for permanent curation. Evidence shall be in the form of a letter from the curation facility stating that the historic materials have been received and that all fees have been paid.
- d. If no cultural resources are discovered, a Negative Monitoring Report must be submitted stating that the grading monitoring activities have been completed. Grading Monitoring Logs must be submitted with the negative monitoring report.

DOCUMENTATION: The applicant's archaeologist shall prepare the final report and submit it to the [PDS, PPD] for approval. Once approved, a final copy of the report shall be submitted to the South Coastal Information Center (SCIC) and any culturally-affiliated tribe who requests a copy. **TIMING:** Prior to any occupancy, final grading release, or use of the premises in reliance of this permit, the final report shall be prepared. **MONITORING:** The [PDS, PPD] shall review the final report for compliance this condition and the report format guidelines. Upon acceptance of the report, [PDS, PPD] shall inform [PDS, LDR] and [DPW, PDCI], that the requirement is complete, and the bond amount can be relinquished. If the monitoring was bonded separately, then [PDS, PPD] shall inform [PDS or DPW FISCAL] to release the bond back to the applicant.

33. NOISE#3 - NOISE CONTROL DESIGN MEASURES

INTENT: In order to comply with the [County of San Diego Noise Ordinance 36.404](#) as evaluated in the [County of San Diego Noise Guidelines for Determining Significance](#), the following design measures shall be verified that they are constructed. **DESCRIPTION OF REQUIREMENT:** The following noise control design measure(s) shall be constructed pursuant to the approved building plans: dual-pane windows with a minimum sound transmission class (STC) rating of 26 and mechanical ventilation as identified in the Noise Study prepared by LDN Engineering (August 3, 2017). **TIMING:** Prior to any occupancy, final grading release, or use of the premises in reliance of this permit for Major Use Permit, PDS2015-MUP-15-027, the noise control measure shall be installed and operational. **MONITORING:** The [PDS, BI] shall verify that the noise control measures above have been constructed pursuant to the approved building plans and this permit's conditions.

34. NOISE#4 - NOISE CONTROL DESIGN MEASURES (MECHANICAL)

INTENT: In order to reduce the impacts of the exterior sound levels from the project site on the adjacent parcels and to comply with the County of San Diego Noise Ordinance 36.404 and the County of San Diego Noise Guidelines for Determining Significance, the following design measures shall be verified that they are constructed. **DESCRIPTION OF REQUIREMENT:** The following noise control design measure(s) shall be constructed pursuant to the approved building plans: HVAC units associated with the school building shall incorporate visual screening by a parapet wall or barrier of equal or greater height in relation to the HVAC unit elevation for noise screening purposes for the southern property line. **TIMING:** Prior to any occupancy, final grading release, or use of the premises in reliance of this permit for the school building, the noise control measure shall be installed and operational. **MONITORING:** The [PDS, BI] shall verify that the noise control measures above have been constructed pursuant to the approved building plans and this permit's conditions.

ONGOING: *(Upon establishment of use the following conditions shall apply during the term of this permit).*

35. PLN#4 – SITE CONFORMANCE [PDS, PCO] [OG] [DPR, TC, PP].

INTENT: In order to comply with Zoning Ordinance Section 7703, the site shall substantially comply with the approved plot plans and all deviations thereof, specific conditions and approved building plans. **DESCRIPTION OF REQUIREMENT:** The project shall conform to the approved landscape plan(s), building plans, and plot plan(s). This includes but is not limited to maintaining the following:

1. School shall begin at 8:30am and end at 3:45pm Monday through Friday.
 - a. Should Valhalla High School change its start time to 8:30am, Liberty Charter High School is permitted to revise its start time upon approval of a minor deviation to this Major Use Permit to avoid conflicting traffic patterns during morning drop off.
2. School administrators shall supervise drop-off and pick-up in designated areas.
3. School shall maintain and enforce “no parking” and “no drop-off” prohibitions along Chase Avenue where applicable.
4. Maintain enough space within the parking lot, approximately 700 linear feet, to queue 28 cars on-site during drop-off and pick-up hours.
5. All parents picking up students shall wait on site in appropriate parking areas.
6. Sports practices and games and other school sponsored and related after school activities on the sports field occurring after school hours and on weekends shall end by 7:30pm. Sports practices, games, and other school sponsored and related after school activities in the gymnasium or classrooms occurring after school hours and on weekends shall end by 10:30pm. Weekend morning noise amplified events shall be limited to 8 per season and the noise amplification system shut off by 8:00pm for all events on campus.
7. Sports field lighting shall be shut off by 8:00pm when in use.
8. Maintain required barrier net system behind northern and southern goal posts.
9. Maintain all parking and driveways areas.
10. Always maintain and water all landscaping.
11. Paint all necessary aesthetics design features.
12. Perimeter gates shall be locked when school is not in use.
13. Maintain all approved lighting, fencing and required signage.

Failure to conform to the approved plot plan(s); is an unlawful use of the land and will result in enforcement action pursuant to Zoning Ordinance Section 7703. **DOCUMENTATION:** The property owner and permittee shall conform to the approved plot plan. If the permittee or property owner chooses to change the site design in any way, they must obtain approval from the County for a Minor Deviation or a Modification pursuant to the County of San Diego Zoning Ordinance. **TIMING:** Upon establishment of the use, this condition shall apply for the duration of the term of this permit. **MONITORING:** The [PDS, Code Enforcement Division] is responsible for enforcement of this permit.

36. ROADS#9–SIGHT DISTANCE

INTENT: In order to provide an unobstructed view for safety while exiting the property and accessing a public road from the site, and to comply with the Design Standards of Section 6.1.(E) of the [County of San Diego Public Road Standards](#), an unobstructed sight distance shall be maintained for the life of this permit. **DESCRIPTION OF REQUIREMENT:** There shall be a minimum unobstructed sight distance in both directions along **Chase Avenue (SA 910.1)** from the project driveway openings for the life of this permit. **DOCUMENTATION:** A minimum unobstructed sight shall be maintained. The sight distance of adjacent driveways and street openings shall not be adversely affected by this project at any time. **TIMING:** Upon establishment of the use, this condition shall apply for the duration of the term of this permit. **MONITORING:** The [PDS, Code Compliance Division] is responsible for compliance of this permit.

37. STRMWTR#5–SELF-VERIFICATION OPERATION AND MAINTENANCE LETTER

INTENT: In order to promote orderly development and to comply with the [County Watershed Protection Ordinance \(WPO\) No.10410, County Code Section 67.801 et. seq.](#), an operation and maintenance verification form for each Structural BMPs shall be completed. **DESCRIPTION OF REQUIREMENT:** Every year the property owner shall receive from the County a BMP Verification Form to be completed for each privately owned Structural BMP. **DOCUMENTATION:** Every year the property owner shall file with the County the completed Structural BMP Verification Form stating the maintenance performed during the reporting period for each privately owned Structural BMP with [DPW, WPP]. **TIMING:** Upon establishment of the use, this condition shall apply for the duration of the term of this permit. **MONITORING:** The [DPW, WPP] is responsible for compliance of this permit.

38. NOISE#5 – ON-GOING SOUND LEVEL COMPLIANCE: [PDS, CODES]

INTENT: In order to comply with the applicable sections of Title 3, Division 6, Chapter 4 (County of San Diego Noise Ordinance), the site shall comply with the requirements of this condition. **DESCRIPTION OF REQUIREMENT:** The project shall conform to the following requirements:

- a. Major Use Permit associated activities shall comply with the one-hour average sound level limit property line requirement pursuant to the County Noise Ordinance, Section 36.404.
- b. The operations associated with this Major Use Permit shall conform to the daytime and nighttime sound level limits for uses as zoned pursuant to Section 36.404.

DOCUMENTATION: The property owner(s) and applicant shall conform to the ongoing requirements of this condition. Failure to conform to this condition may result in disturbing, excessive or offensive noise interfering with a person's right to enjoy life and property and is detrimental to the public health and safety pursuant to the applicable sections of Chapter 4. **TIMING:** Upon establishment of the use, this condition shall apply for the duration of the term of this permit. **MONITORING:** The [PDS, CODES] is responsible for enforcement of this permit.

GRADING PLAN NOTES

The following "Specific Environmental Condition Notes" below are to be placed on the Conceptual Grading and Development Plan, for implementation on the Final Grading and or Improvement Plans.

PRE-CONSTRUCTION GRADING AND/OR IMPROVEMENTS: *(Prior to any clearing, grubbing, trenching, grading, or any land disturbances.)*

(BIOLOGICAL RESOURCES)**1. BIO#2–RESOURCE AVOIDANCE [PDS, FEE X2]**

INTENT: In order to avoid impacts to avian species and raptors which are sensitive biological resources pursuant to the Migratory Bird Treaty Act (MBTA), a Resource Avoidance Area (RAA), shall be implemented on all plans. **DESCRIPTION OF REQUIREMENT:** There shall be no brushing, clearing and/or grading such that none will

be allowed during the breeding season of avian species and raptors. The breeding season is defined as occurring between January 15th and September 15th. The Director of PDS [PDS, PCC] may waive this condition, through written concurrence from the US Fish and Wildlife Service and the California Department of Fish and Wildlife, provided that no nesting avian species or raptors are present in the vicinity of the brushing, clearing or grading. **DOCUMENTATION:** The applicant shall provide a letter of agreement with this condition; alternatively, the applicant may submit a written request for waiver of this condition. Although, No Grading shall occur within the RAA until concurrence is received from the County and the Wildlife Agencies. **TIMING:** Prior to preconstruction conference and prior to any clearing, grubbing, trenching, grading, or any land disturbances and throughout the duration of the grading and construction, compliance with this condition is mandatory unless the requirement is waived by the County upon receipt of concurrence from the Wildlife Agencies. **MONITORING:** The [DPW, PDCI] shall not allow any grading in the RAA during the specified dates, unless a concurrence from the [PDS, PCC] is received. The [PDS, PCC] shall review the concurrence letter.”

(CULTURAL RESOURCES)

2. CULT#GR-1 ARCHAEOLOGICAL MONITORING – PRECONSTRUCTION MEETING [PDS, FEE X2]

INTENT: In order to comply with the County of San Diego Guidelines for Significance – Cultural Resources, an Archaeological Monitoring Program shall be implemented. **DESCRIPTION OF REQUIREMENT:** The County approved Project Archaeologist and Kumeyaay Native American Monitor shall attend the pre-construction meeting with the contractors to explain and coordinate the requirements of the archaeological monitoring program. The Project Archaeologist and Kumeyaay Native American Monitor shall monitor the original cutting of previously undisturbed deposits in all areas identified for development including off-site improvements. The Project Archaeologist and Kumeyaay Native American monitor shall also evaluate fill soils to ensure that they are clean of cultural resources. The archaeological monitoring program shall comply with the County of San Diego Guidelines for Determining Significance and Report Format and Content Requirements for Cultural Resources. **DOCUMENTATION:** The applicant shall have the contracted Project Archeologist and Kumeyaay Native American attend the preconstruction meeting to explain the monitoring requirements. **TIMING:** Prior to any clearing, grubbing, trenching, grading, or any land disturbances this condition shall be completed. **MONITORING:** The [DPW, PDCI] shall confirm the attendance of the approved Project Archaeologist.

DURING CONTRUCTION: *(The following actions shall occur throughout the duration of the grading construction).*

(CULTURAL RESOURCES)

3. CULT#GR-2 ARCHAEOLOGICAL MONITORING – DURING CONSTRUCTION [PDS, FEE X2]

INTENT: In order to comply with the County of San Diego Guidelines for Determining Significance and Report Format and Content Requirements for Cultural Resources, a Cultural Resource Grading Monitoring Program shall be implemented. **DESCRIPTION OF REQUIREMENT:** The Project Archaeologist and Kumeyaay Native American Monitor shall monitor the original cutting of previously undisturbed deposits in all areas identified

for development including off-site improvements. The archaeological monitoring program shall comply with the following requirements during earth-disturbing activities:

- a. During the original cutting of previously undisturbed deposits, the Project Archaeologist and Kumeyaay Native American Monitor shall be onsite as determined necessary by the Project Archaeologist. Inspections will vary based on the rate of excavation, the materials excavated, and the presence and abundance of artifacts and features. The frequency and location of inspections will be determined by the Project Archaeologist in consultation with the Kumeyaay Native American Monitor. Monitoring of cutting of previously disturbed deposits will be determined by the Project Archaeologist in consultation with the Kumeyaay Native American Monitor.
- b. In the event that previously unidentified potentially significant cultural resources are discovered:
 1. The Project Archaeologist or the Kumeyaay Native American monitor shall have the authority to divert or temporarily halt ground disturbance operations in the area of discovery to allow evaluation of potentially significant cultural resources.
 2. The Project Archaeologist, in consultation with the PDS Staff Archaeologist and the Kumeyaay Native American Monitor, shall determine the significance of the discovered resources.
 3. Construction activities will be allowed to resume in the affected area only after the PDS Staff Archaeologist has concurred with the evaluation.
 4. Isolates and clearly non-significant deposits shall be minimally documented in the field. Should the isolates and/or non-significant deposits not be collected by the Project Archaeologist, then the Kumeyaay Native American monitor may collect the cultural material for transfer to a Tribal Curation facility or repatriation program.
 5. A Research Design and Data Recovery Program (Program) is required to mitigate impacts to identified significant cultural resources. The Program shall include (1) reasonable efforts to preserve (avoidance) "unique" cultural resources or Sacred Sites; (2) the capping of identified Sacred Sites or unique cultural resources and placement of development over the cap, if avoidance is infeasible; and (3) data recovery for non-unique cultural resources. The preferred option is preservation (avoidance).
 6. The Research Design and Data Recovery Program shall be prepared by the Project Archaeologist in coordination with the Kumeyaay Native American Monitor.
 7. The County Archaeologist shall review and approve the Program, which shall be carried out using professional archaeological methods.
- c. If any human remains are discovered:
 1. The Property Owner or their representative shall contact the County Coroner and the PDS Staff Archaeologist.
 2. Upon identification of human remains, no further disturbance shall occur in the area of the find until the County Coroner has made the necessary findings as to origin.
 3. If the remains are determined to be of Native American origin, the Most Likely Descendant (MLD), as identified by the Native American Heritage Commission

- (NAHC), shall be contacted by the Property Owner or their representative in order to determine proper treatment and disposition of the remains.
4. The immediate vicinity where the Native American human remains are located is not to be damaged or disturbed by further development activity until consultation with the MLD regarding their recommendations as required by Public Resources Code Section 5097.98 has been conducted.
 5. Public Resources Code §5097.98, CEQA §15064.5 and Health & Safety Code §7050.5 shall be followed in the event that human remains are discovered.

DOCUMENTATION: The applicant shall implement the Archaeological Monitoring Program pursuant to this condition. **TIMING:** The following actions shall occur throughout the duration of the earth disturbing activities. **MONITORING:** The [DPW, PDCI] shall make sure that the Project Archeologist is on-site performing the monitoring duties of this condition. The [DPW, PDCI] shall contact the [PDS, PPD] if the Project Archeologist or applicant fails to comply with this condition.

4. Noise#GR-1 - TEMPORARY CONSTRUCTION NOISE

INTENT: In order to minimize temporary construction noise for grading operations associated with the project subdivision and to comply with County Noise Ordinance 36.409. **DESCRIPTION OF REQUIREMENT:** The project shall comply with the following temporary construction noise control measures and shall comply with Section 36.409:

- a. Turn off equipment when not in use.
- b. Equipment used in construction should be maintained in proper operating condition, and all loads should be properly secured, to prevent rattling and banging.
- c. Use equipment with effective mufflers
- d. Minimize the use of back up alarm.
- e. Equipment staging areas should be placed at locations away from noise sensitive receivers.

DOCUMENTATION: The applicant shall comply with the temporary construction noise measures of this condition. **TIMING:** The following actions shall occur throughout the duration of the grading construction. **MONITORING:** The [DPW, PDCI] shall make sure that the grading contractor complies with the construction noise control measures of this condition. The [DPW, PDCI] shall contact the [PDS, PCC] if the applicant fails to comply with this condition.

ROUGH GRADING: *(Prior to rough grading approval and issuance of any building permit).*

(CULTURAL RESOURCES)

5. CULT#GR-3 ARCHAEOLOGICAL MONITORING – ROUGH GRADING [PDS, FEE]

INTENT: In order to comply with the County of San Diego Guidelines for Determining Significance and Report Format and Content Requirements for Cultural Resources, an Archaeological Monitoring Program shall be implemented. **DESCRIPTION OF**

REQUIREMENT: The Project Archaeologist shall prepare one of the following reports upon completion of the earth-disturbing activities that require monitoring and communicate with local tribes as required below:

- a. If no archaeological resources are encountered during earth-disturbing activities, then submit a final Negative Monitoring Report substantiating that earth-disturbing activities are completed, and no cultural resources were encountered. Archaeological monitoring logs showing the date and time that the monitor was on site and any comments from the Kumeyaay Native American Monitor must be included in the Negative Monitoring Report.
- b. If archaeological resources were encountered during the earth disturbing activities, the Project Archaeologist shall provide an Archaeological Monitoring Report stating that the field monitoring activities have been completed, and that resources have been encountered. The report shall detail all cultural artifacts and deposits discovered during monitoring and the anticipated time schedule for completion of the curation and/or repatriation phase of the monitoring.

DOCUMENTATION: The applicant shall submit the Archaeological Monitoring Report to the [PDS, PPD] for review and approval. Once approved, a final copy of the report shall be submitted to the South Coastal Information Center and any culturally affiliated tribe that requests a copy of the report. **TIMING:** Upon completion of all earth-disturbing activities, and prior to Rough Grading Final Inspection (Grading Ordinance SEC 87.421.a.2), the report shall be completed. **MONITORING:** The [PDS, PPD] shall review the report or field monitoring memo for compliance with the project MMRP, and inform [DPW, PDCI] that the requirement is completed.

FINAL GRADING RELEASE: *(Prior to any occupancy, final grading release, or use of the premises in reliance of this permit).*

(CULTURAL RESOURCES)

6. CULT#GR-4 ARCHAEOLOGICAL MONITORING – FINAL GRADING [PDS, FEE]

INTENT: In order to comply with the County of San Diego Guidelines for Determining Significance and Report Format and Content Requirements for Cultural Resources, an Archaeological Monitoring Program shall be implemented. **DESCRIPTION OF REQUIREMENT:** The Project Archaeologist shall prepare a final report that documents the results, analysis, and conclusions of all phases of the Archaeological Monitoring Program if cultural resources were encountered during earth-disturbing activities. The report and communications shall include the following, if applicable:

- a. Department of Parks and Recreation Primary and Archaeological Site forms.
- b. Daily Monitoring Logs
- c. Evidence that the disposition of all cultural materials has been completed as follows:
 - (1) Evidence that all prehistoric materials collected during the archaeological monitoring program have been submitted to a San Diego curation facility or a

culturally affiliated Native American Tribal curation facility that meets federal standards per 36 CFR Part 79, and, therefore, would be professionally curated and made available to other archaeologists/researchers for further study. The collections and associated records, including title, shall be transferred to the San Diego curation facility or culturally affiliated Native American Tribal curation facility and shall be accompanied by payment of the fees necessary for permanent curation. Evidence shall be in the form of a letter from the curation facility stating that the prehistoric archaeological materials have been received and that all fees have been paid.

or

Evidence that all prehistoric materials collected during the archaeological monitoring program have been returned to a Native American group of appropriate tribal affinity. Evidence shall be in the form of a letter from the Native American tribe to whom the cultural resources have been repatriated identifying that the archaeological materials have been received.

(2) Historic materials shall be curated at a San Diego curation facility and shall not be curated at a Tribal curation facility or repatriated. The collections and associated records, including title, shall be transferred to the San Diego curation facility and shall be accompanied by payment of the fees necessary for permanent curation. Evidence shall be in the form of a letter from the curation facility stating that the historic materials have been received and that all fees have been paid.

d. If no cultural resources are discovered, a Negative Monitoring Report must be submitted stating that the archaeological monitoring activities have been completed. Grading Monitoring Logs must be submitted with the negative monitoring report.

DOCUMENTATION: The applicant’s archaeologist shall prepare the final report and submit it to the [PDS, PPD] for approval. Once approved, a final copy of the report shall be submitted to the South Coastal Information Center (SCIC) and any culturally affiliated tribe that requests a copy of the report. **TIMING:** Prior to any occupancy, final grading release, or use of the premises in reliance of this permit, the final report shall be prepared. **MONITORING:** The [PDS, PPD] shall review the final report for compliance with this condition and the report format guidelines. Upon acceptance of the report, [PDS, PPD] shall inform [PDS, LDR] and [DPW, PDCI], that the requirement is complete, and the bond amount can be relinquished. If the monitoring was bonded separately, then [PDS, PPD] shall inform [PDS or DPW FISCAL] to release the bond back to the applicant.

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MITIGATION MONITORING OR REPORTING PROGRAM (MMRP): Public Resources Code Section 21081.6 requires the County to adopt a Mitigation Monitoring or Reporting Program for any project approved with the adoption of a Mitigated Negative Declaration or with the certification of an Environmental Impact Report, for which changes in the project are required in order to avoid significant impacts. Section 21081.6(a)(1) states, in part:

The public agency shall adopt a reporting or monitoring program for the changes made to the project or conditions of project approval, adopted in order to mitigate or avoid significant effects on the environment. The reporting or monitoring program shall be designed to ensure compliance during project implementation.

Section 21081(b) further states:

A public agency shall provide {that} the measures to mitigate or avoid significant effects on the environment are fully enforceable through permit conditions, agreements, or other measures.

As indicated above, a Mitigation Monitoring or Reporting Program is required to assure that a project is implemented in compliance with all required mitigation measures. The Mitigation Monitoring or Reporting Program (MMRP) for this project is incorporated into the mitigation measures adopted as project conditions of approval. Each mitigation measure adopted as a condition of approval (COA) includes the following five components.

Intent: An explanation of why the mitigation measure (MM) was imposed on the project.

Description: A detailed description of the specific action(s) that must be taken to mitigate or avoid impacts.

Documentation: A description of the informational items that must be submitted by the applicant to the Lead Agency to demonstrate compliance with the COA.

Timing: The specific project milestone (point in progress) when the specific required actions are required to implemented.

Monitoring: This section describes the actions to be taken by the lead agency to assure implementation of the mitigation measure.

The following conditions of approval required to mitigate or avoid significant impacts on the environment are listed below and constitute the MMRP for this project:

Conditions: BIO#1, BIO#2, AQ#1, ROADS#3

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MAJOR USE PERMIT FINDINGS

Pursuant to Section 7358 of The Zoning Ordinance, the following findings in support of the granting of the Major Use Permit are made:

(a) *The location, size, design, and operating characteristics of the proposed use will be compatible with adjacent uses, residents, buildings, or structures with consideration given to*

1. *Harmony in scale, bulk, coverage, and density*

The applicant proposes a Major Use Permit for a charter school on a 7.7- acre parcel located at Chase Avenue and Jamacha Road, in the Valle De Oro Community Planning Area. The site is currently undeveloped. Surrounding land uses primarily consist of single family residential, residential commercial and a commercial center.

Scale and Bulk

The applicant proposes to construct a 48,000 square foot two-story building consisting of 22 classrooms, administrative offices, and a gymnasium. At full capacity the school would serve 450 students and have 33 faculty and staff. The building is proposed at a height of 32 feet with one tower at 38 feet. The project is designed to be similar in scale and bulk to the adjacent commercial center. The adjacent commercial center is comprised of three two-story buildings totaling 30,096 square feet, similar to the 48,000 square foot two-story building proposed for the charter school. Although slightly larger than the adjacent commercial center and larger than the adjacent residential homes, it appears as similar in bulk and scale because it is set back from the public road and is located near the center of the parcel.

The plans illustrate that the proposed charter school would be unobtrusive to the surrounding viewshed. The proposed two-story structure would be visible to adjacent residential land uses located to the south and the west. The nearest residence to the south is approximately 110 feet from the proposed school building. However, views from the south would be limited due to the proposed screening measures. Residences to the west are approximately 160-200 feet away from the proposed school building and are located on a slope looking down on to the school, giving these residences a view of the proposed project. However, these views will be limited due to the parking lot providing a buffer as well as the 15-30-foot tall trees proposed along the perimeter.

Coverage

The project would not result in a coverage that is out of character with the surroundings. The subject property is approximately 7.7 acres and is currently undeveloped. The surrounding area is characterized as single-family residential, residential-commercial, and commercial. Surrounding residential land uses coverage range from 3% to 14%, while the adjacent commercial center sites range from 24% to 39%. The coverage of the Project is approximately 12%, which is well within the range of the surrounding properties.

Density

The proposed project is a Major Use Permit for a charter high school and does not propose residential uses. The project is not subject to the density requirements.

2. *The availability of public facilities, services, and utilities*

All necessary public facilities and services are available as detailed in the service availability letters submitted for the project. The project will be served by sewer and water services by the Otay Water District. Fire service will be provided by the San Miguel Consolidated Fire Protection District. As such, all necessary public facilities and services are available.

3. *The harmful effect, if any, upon desirable neighborhood character:*

The proposed project is a Major Use Permit for a charter high school on a 7.7-acre site located at Chase Ave and Jamacha Road in the Valle de Oro community. The charter school would serve grades nine through twelve with class times between

the hours of 8:30AM and 3:45PM, Monday through Friday. At full capacity the school would serve 450 students and have 33 faculty and staff.

The surrounding land uses include single family residences and a commercial center. The Project proposes architectural features and colors that are like that of the adjacent commercial center. The campus will include landscaping of street trees and shrubs along the perimeter of the entire site including Chase Avenue and abutting the single-family homes to the west and south.

The conceptual landscape plan demonstrates conformance with State guidelines, County codes, and the Valle De Oro guidelines by using drought tolerant landscaping.

The Project is conditioned to improve Chase Avenue and install sidewalks and a bicycle lane. Additionally, the project is conditioned to install "no parking" signage on Chase Avenue along the project frontage and across the street to prevent parents from dropping students off across the street. To prevent school users from parking in the adjacent shopping center parking lot, the applicant is required to install signage prohibiting school parking in the lot upon coordination with the commercial shopping center property owners.

A photometric study was prepared for this project and determined the project will not have a significant effect related to dark skies or glare. The proposed lighting for the project will comply with both the San Diego Light Pollution Code (§59.108-59.110) and the County Zoning Ordinance. The proposed fixtures will have full cutoff optics to ensure they are fully shielded. The project is also be conditioned to turn off playing field lighting by 8:00 p.m. each night when in use.

A preliminary noise study was prepared to evaluate noise impacts associated with the development of the Project. The analysis found that the on-site and off-site noise levels complies with the County's community noise exposure level and that the Project will not cause any significant impacts to existing or future land uses. The project's operational characteristics, number of events and activities and hours of operation will be limited by the on-going conditions of approval to ensure the project does not exceed the allowable noise pursuant to the County's Noise Ordinance and General Plan Noise Element.

Based on the project's design features and conditioning, the environmental analysis has concluded that the project would not have a harmful effect upon the desirable neighborhood character.

4. *The generation of traffic and the capacity and physical character of surrounding streets:*

The applicant proposes a Major Use Permit for a charter high school serving grades nine through twelve on a 7.7-acre parcel located at Chase Avenue and Jamacha Road in the Valle De Oro Community Planning Area. The proposed charter school would generate 585 average daily trips (ADT) with 275 occurring in the AM and PM peak hours.

According to the Traffic Impact Study prepared for this project, there will be a direct traffic impact to Chase Avenue between the westernmost project driveway and Jamacha Road. To lessen this impact, the project is conditioned to widen and improve Chase Avenue to accommodate a westbound left-turn lane onto the project site.

Pedestrian safety for students approaching the site on foot or by bicycle has been addressed with conditioning to install sidewalks along the project frontage and at the intersection of Jamacha Road and Chase Avenue. The project is also required to install “no parking” signage along Chase Avenue to prevent parents from dropping off and picking up students across the street.

Access to the site would be provided by two driveways, one entrance and one exit. The project design includes approximately 700 feet of on-site queuing for drop-off and pick-up hours.

5. *The suitability of the site for the type and intensity of use or development, which is proposed:*

The project is a Major Use Permit for a charter high school. The proposed charter school will have a maximum capacity of 450 students. Land uses surrounding the site are single-family residential, commercial, including a commercial center located adjacent to the site. The proposed improvements to the access driveway, widening and improvements of Chase Avenue, as well as the installation of an on-site drop off and pick up zone for are designed to mitigate the impacts associated with the increased traffic ADT. As such, the proposed project, with the incorporation of design features, mitigation measures, and maximum limits on the proposed school’s capacity, the site is suitable for the intensity of the proposed development.

6. *Any other relevant impact of the proposed use:*

None identified.

(b) *The impacts, as described in Findings (a) above, and the location of the proposed use will be consistent with the San Diego County General Plan:*

The site is subject to the General Plan Regional Category Semi-Rural and Land Use Designation Semi-Rural Residential (SR-1). The SR-1 Land Use Designation is consistent with the RR (Rural Residential) zone that permits the school use type with the issuance of a MUP for Major Impact Services and Utilities, pursuant to Zoning Ordinance Section 2105. The proposed project would be consistent with General Plan Land Use Element Policy LU-17.2 for the compatibility of schools with adjoining uses through appropriate siting and adequate mitigation, and Policy LU-6.5 for minimizing the use of impervious surfaces and incorporating other low impact development techniques as well as a combination of site design, source control and stormwater best management practices.

(c) *That the requirements of the California Environmental Quality Act have been complied with:*

A Mitigated Negative Declaration has been prepared for this project and is on file at the Department of Planning & Development Services as Environmental Review Number PDS2015-ER-15-14-010.

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ORDINANCE COMPLIANCE NOTIFICATIONS: The project is subject to, but not limited to the following County of San Diego, State of California, and US Federal Government, Ordinances, Permits, and Requirements:

COMPLIANCE INSPECTION: In order to comply with Zoning Ordinance Section 7362.e the County shall inspect the Use Permit property for compliance with the terms of this Use Permit. The County Permit Compliance Officer will perform a site inspection and review the on-going conditions associated with this permit. The inspection shall be scheduled no later than the six months subsequent to establishing the intended use of the permit. If the County determines the applicant is not complying with the Major Use Permit terms and conditions the applicant shall allow the County to conduct follow up inspections more frequently than once every twelve months until the County determines the applicant is in compliance. The Property Owner/Permittee shall allow the County to inspect the property for which the Major Use Permit has been granted, at least once every twelve months, to determine if the Property Owner/Permittee is complying with all terms and conditions of the Use Permit. This requirement shall apply during the term of this permit.

STORMWATER ORDINANCE COMPLIANCE: In order to Comply with all applicable stormwater regulations the activities proposed under this application are subject to enforcement under permits from the [San Diego Regional Water Quality Control Board \(RWQCB\)](#) and the County of San Diego Watershed Protection, Stormwater Management, and Discharge Control [Ordinance No. 10410](#) and all other applicable ordinances and standards for the life of this permit. The project site shall be in compliance with all applicable stormwater regulations referenced above and all other applicable ordinances and standards. This includes compliance with the approved Stormwater Management Plan, all requirements for Low Impact Development (LID), Hydromodification, materials and wastes control, erosion control, and sediment control on the project site. Projects that involve areas 1 acre or greater require that during construction the property owner keeps the Storm Water Pollution Prevention Plan (SWPPP) onsite and update it as needed. The property owner and permittee shall comply with the requirements of the stormwater regulations referenced above.

LOW IMPACT DEVELOPMENT NOTICE: The San Diego Regional Water Quality Control Board (SDRWQCB) issued a new Municipal Stormwater Permit under the National Pollutant Discharge Elimination System (NPDES). The requirements of the Municipal Permit were implemented beginning in May 2013. *Project design shall be in compliance with the new Municipal Permit regulations.* The Low Impact Development (LID) Best Management Practices (BMP) Requirements of the Municipal Permit can be found at the following link:

http://www.sandiegocounty.gov/content/dam/sdc/dpw/WATERSHED_PROTECTION_PROGRAM/susmppdf/lid_handbook_2014sm.pdf

The County has provided a LID Handbook as a source for LID information and is to be utilized by County staff and outside consultants for implementing LID in our region. See link below: <http://www.sdcounty.ca.gov/dplu/docs/LID-Handbook.pdf>

STORMWATER COMPLIANCE NOTICE: Updated studies, including Hydro-modification Management Plans for Priority Development Projects, will be required prior to approval of grading and improvement plans for construction pursuant to County of San Diego Watershed Protection, Stormwater Management and Discharge Control Ordinance No. 10410 (N.S.), dated February 26, 2016 and BMP Design Manual. These requirements are subject to the MS4 Permit issued by the Regional Water Quality Control Board, Order No. R9-2013-0001 and any subsequent order. Additional studies and other action may be needed to comply with future MS4 Permits.

DRAINAGE: The project shall be in compliance with the County of San Diego Flood Damage Prevention Ordinance No. 10091, adopted December 8, 2010.

GRADING PERMIT REQUIRED: A grading permit is required prior to commencement of grading when quantities of excavation or fill results in the movement of material exceeding 200 cubic yards or eight feet (8') in vertical height of cut/fill, pursuant to [Section 87.201 of Grading Ordinance](#).

CONSTRUCTION PERMIT REQUIRED: A Construction Permit and/or Encroachment Permit are required for any and all work within the County road right-of-way. Contact PDS Construction/Road right-of-way Permits Services Section, (858) 694-3275, to coordinate County requirements. In addition, before trimming, removing or planting trees or shrubs in the County Road right-of-way, the applicant must first obtain a permit to remove plant or trim shrubs or trees from the Permit Services Section.

EXCAVATION PERMIT REQUIRED: An excavation permit is required for undergrounding and/or relocation of utilities within the County right-of-way.

TRANSPORTATION IMPACT FEE: The project is subject to County of San Diego Transportation Impact Fee (TIF) pursuant to County TIF Ordinance number 77.201 – 77.223. The Transportation Impact Fee (TIF) shall be paid. The fee is required for the entire project, or it can be paid at building permit issuance for each phase of the project. The fee is calculated pursuant to the ordinance at the time of building permit issuance. The applicant shall pay the TIF at the [PDS, LD Counter] and provide a copy of the receipt to the [PDS, BD] at time of permit issuance.

LIGHTING ORDINANCE COMPLIANCE: In order to comply with the County Lighting Ordinance 59.101 et seq. and Zoning Ordinance Sections 6322, 6324, and 6326, the onsite lighting shall comply with the approved plot plan(s), specific permit conditions and approved building plans associated with this permit. All light fixtures shall be designed and adjusted to reflect light downward, away from any road or street, and away from adjoining premises, and shall otherwise conform to the County Lighting Ordinance 59.101 et seq. and Zoning Ordinance Sections 6322, and 6324. The property owner and permittee shall conform to the approved plot plan(s), specific permit conditions, and approved building plans associated with this permit as they pertain to lighting. No additional lighting is permitted. If the permittee or property owner chooses to change the site design in any way, they must obtain approval from the County for a Minor Deviation or a Modification pursuant to the County of San Diego Zoning Ordinance.

NOISE ORDINANCE COMPLIANCE: In order to comply with the County Noise Ordinance 36.401 et seq. and the Noise Standards pursuant to the General Plan Noise Element (Table N-1 & N-2), the property and all of its uses shall comply with the approved plot plan(s), specific permit conditions and approved building plans associated with this permit. No loudspeakers, sound amplification systems, and project related noise sources shall produce noise levels in violation of the County Noise Ordinance. The property owner and permittee shall conform to the approved plot plan(s), specific permit conditions, and approved building plans associated with this permit as they pertain to noise generating devices or activities. If the permittee or property owner chooses to change the site design in any way, they must obtain approval from the County for a Minor Deviation or a Modification pursuant to the County of San Diego Zoning Ordinance.

NOTICE: This project has been found to conform to the San Diego County Multiple Species Conservation Program Subarea Plan, Biological Mitigation Ordinance and Implementing Agreement. Upon fulfillment of the requirements for permanent mitigation and management of preserved areas as outlined in Section 17.1 (A) of the County's Implementing Agreement for the Multiple Species Conservation Program (MSCP) Plan, Third Party Beneficiary Status can be attained for the project. Third party beneficiary status allows the property owner to perform "incidental take" under the State and Federal Endangered Species Acts, of species covered by the MSCP Plan while undertaking land development activities in conformance with an approval granted by the County in compliance with the County's Implementing Agreement.

NOTICE: The subject property contains habitat which may be used for nesting by migratory birds. Any grading, brushing or clearing conducted during the migratory bird breeding season, February 1 – August 31, has a potential to impact nesting or breeding birds in violation of the Migratory Bird Treaty Act. The applicant may submit evidence that nesting or breeding migratory birds will not be affected by the grading, brushing or clearing to these agencies: California Department of Fish and Wildlife, 3883 Ruffin Rd., San Diego, CA 92123, (858) 467-4201, <http://www.dfg.ca.gov/>; and United States Fish and Wildlife Service, 6010 Hidden Valley Rd, Carlsbad, CA 92011-4219, (760) 431-9440, <http://www.fws.gov/>.

NOTICE: To comply with State law, the applicant/owner must file the Notice of Determination (NOD) signed by the lead agency and remit required fees to the County Clerk's Office within five (5) working days of the date of project approval. Payment or sufficient proof of prior payment to the County Clerk is required at the time of filing. The filing of a NOD reduces the period of time the CEQA document can be challenged to **35 days**. However, if the NOD is not filed, this period is extended to **180 days**. The CDFW adjusts fees annually based on inflation. You must pay the amount effective January 1 of the year of the project decision.

NOTICE: THE ISSUANCE OF THIS PERMIT BY THE COUNTY OF SAN DIEGO DOES NOT AUTHORIZE THE APPLICANT FOR SAID PERMIT TO VIOLATE ANY FEDERAL, STATE, OR COUNTY LAWS, ORDINANCES, REGULATIONS, OR POLICIES INCLUDING, BUT NOT LIMITED TO, THE FEDERAL ENDANGERED SPECIES ACT AND ANY AMENDMENTS THERETO.

NOTICE: The 90-day period in which the applicant may file a protest of the fees, dedications or exactions begins on September 18, 2020.

NOTICE: The project will be required to pay Planning & Development Services Mitigation Monitoring and Condition Review Fee. The fee will be collected at the time of the first submittal for Condition Satisfaction to PDS, including Mitigation Monitoring requests. The amount of the

fee will be determined by the current Fee Ordinance requirement at the time of the first submittal and is based on the number of conditions that need to be satisfied. The fee will not apply to subsequent project approvals that require a separate submittal fee such as, Revegetation and Landscape Plans, Resource (Habitat) Management Plans, Habitat Loss Permits, Administrative Permits, Site Plans, and any other discretionary permit applications.

EXPLANATION OF COUNTY DEPARTMENT AND DIVISION ACRONYMS			
Planning & Development Services (PDS)			
Project Planning Division	PPD	Land Development Project Review Teams	LDR
Permit Compliance Coordinator	PCC	Project Manager	PM
Building Plan Process Review	BPPR	Plan Checker	PC
Building Division	BD	Map Checker	MC
Building Inspector	BI	Landscape Architect	LA
Zoning Counter	ZO		
Department of Public Works (DPW)			
Private Development Construction Inspection	PDCI	Environmental Services Unit Division	ESU
Department of Environmental Health (DEH)			
Land and Water Quality Division	LWQ	Local Enforcement Agency	LEA
Vector Control	VCT	Hazmat Division	HMD
Department of Parks and Recreation (DPR)			
Trails Coordinator	TC	Group Program Manager	GPM
Parks Planner	PP		
Department of General Service (DGS)			
Real Property Division	RP		

APPEAL PROCEDURE: Within ten calendar days after the date of this Decision of the Planning Commission, the decision may be appealed to the Board of Supervisors in accordance with [Section 7366 of the County Zoning Ordinance](#). An appeal shall be filed with the Director of Planning & Development Services or by mail with the Secretary of the Planning Commission within TEN CALENDAR DAYS of the date of this notice AND MUST BE ACCOMPANIED BY THE DEPOSIT OR FEE AS PRESCRIBED IN THE DEPARTMENT'S FEE SCHEDULE, PDS FORM #369, pursuant to Section 362 of the San Diego County Administrative Code. If the tenth day falls on a weekend or County holiday, an appeal will be accepted until 4:00 p.m. on the following day the County is open for business. Filing of an appeal will stay the decision of the Planning Commission until a hearing on your application is held and action is taken by the Board of Supervisors. Furthermore, the 90-day period in which the applicant may file a protest of the fees, dedications or exactions begins on the date of approval of this Decision.

ON MOTION of Commissioner _____, seconded by Commissioner _____, this Form of Decision is passed and approved by the of the County of San Diego, State of California, at a

regular meeting held on this 18th day of September, 2020, in County Operations Center, 5520 Overland Avenue, San Diego, California, by the following vote:

AYES:

NOES:

ABSTAIN:

ABSENT:

COUNTY OF SAN DIEGO PLANNING COMMISSION
MARK WARDLAW, SECRETARY

BY:

Darin Neufeld, Chief
Project Planning Division
Planning & Development Services

cc: Literacy First Charter Schools, Inc., 1012 East Bradley Avenue, El Cajon, CA 92020

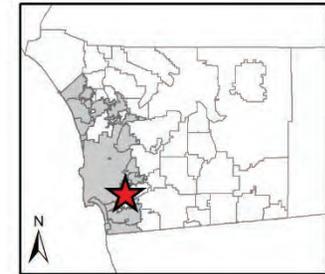
email cc:

Sean McLean, Team Leader, Land Development/Engineering, PDS
Denise Russell, Planning Manager, Project Planning, PDS
Valle De Oro Community Planning Group

**Attachment C –
PLANNING DOCUMENTATION**

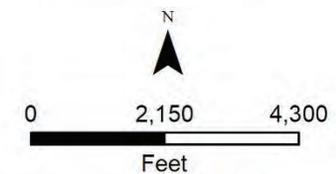
Liberty Charter High School Vicinity Map

Valle De Oro
Community Plan Area

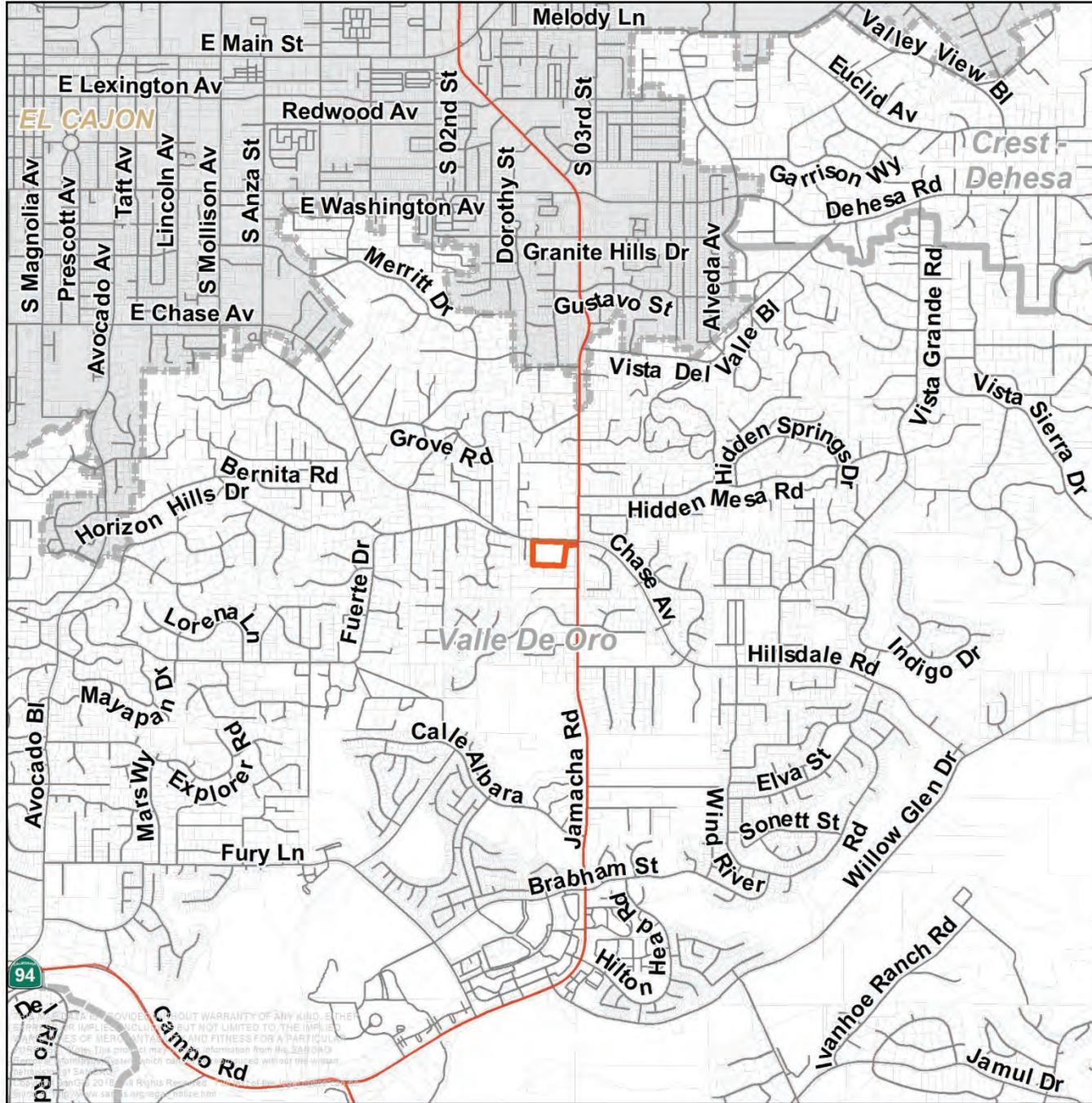


1:3,500,000

	Site
	Roads
	Parcels



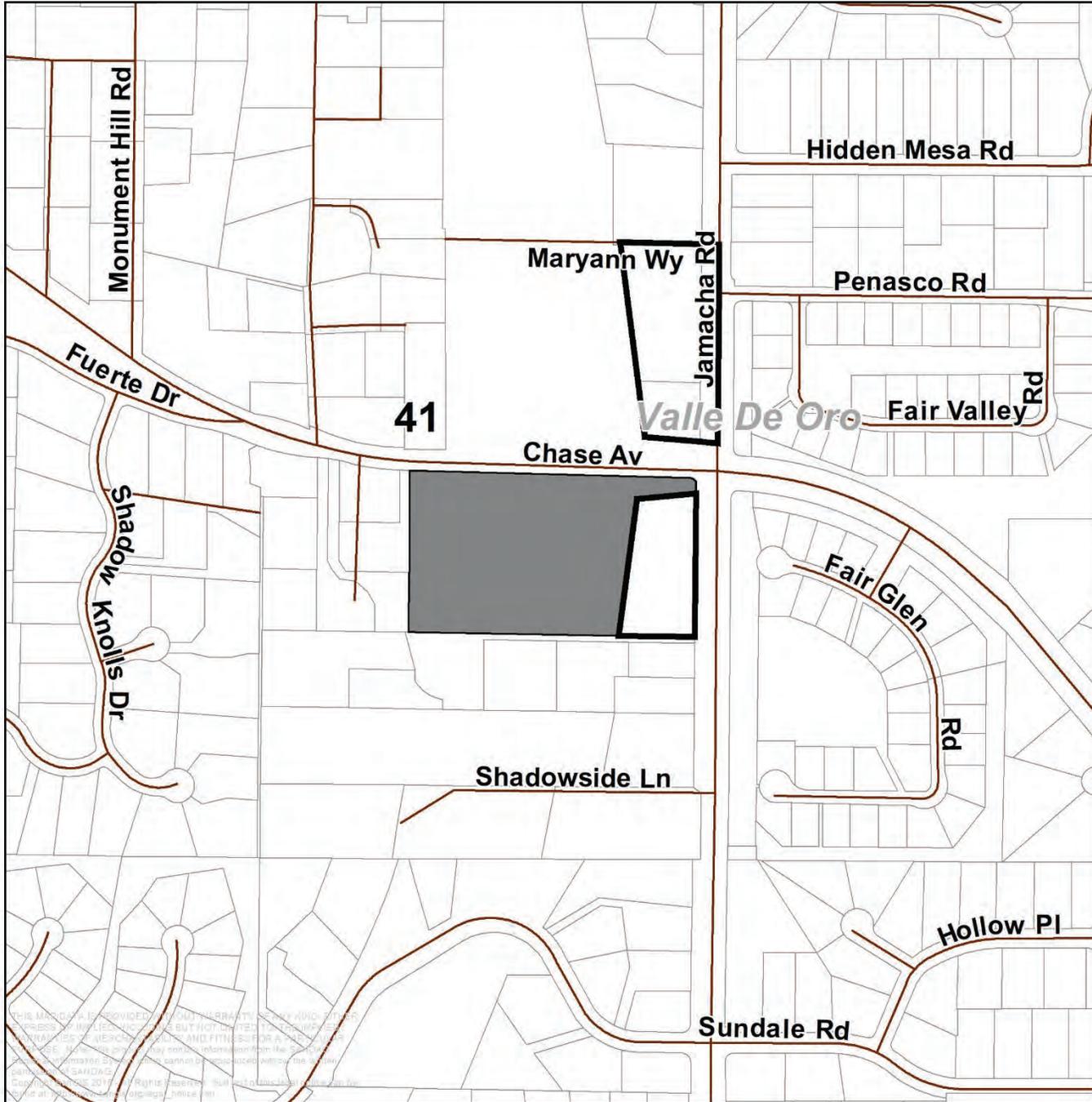
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Liberty Charter High School
General Plan

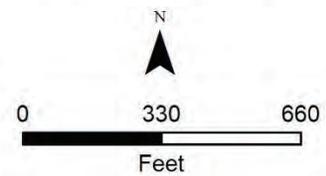
Valley De Oro
Community Plan Area

(41) Semi-Rural Residential (SR-0.5)

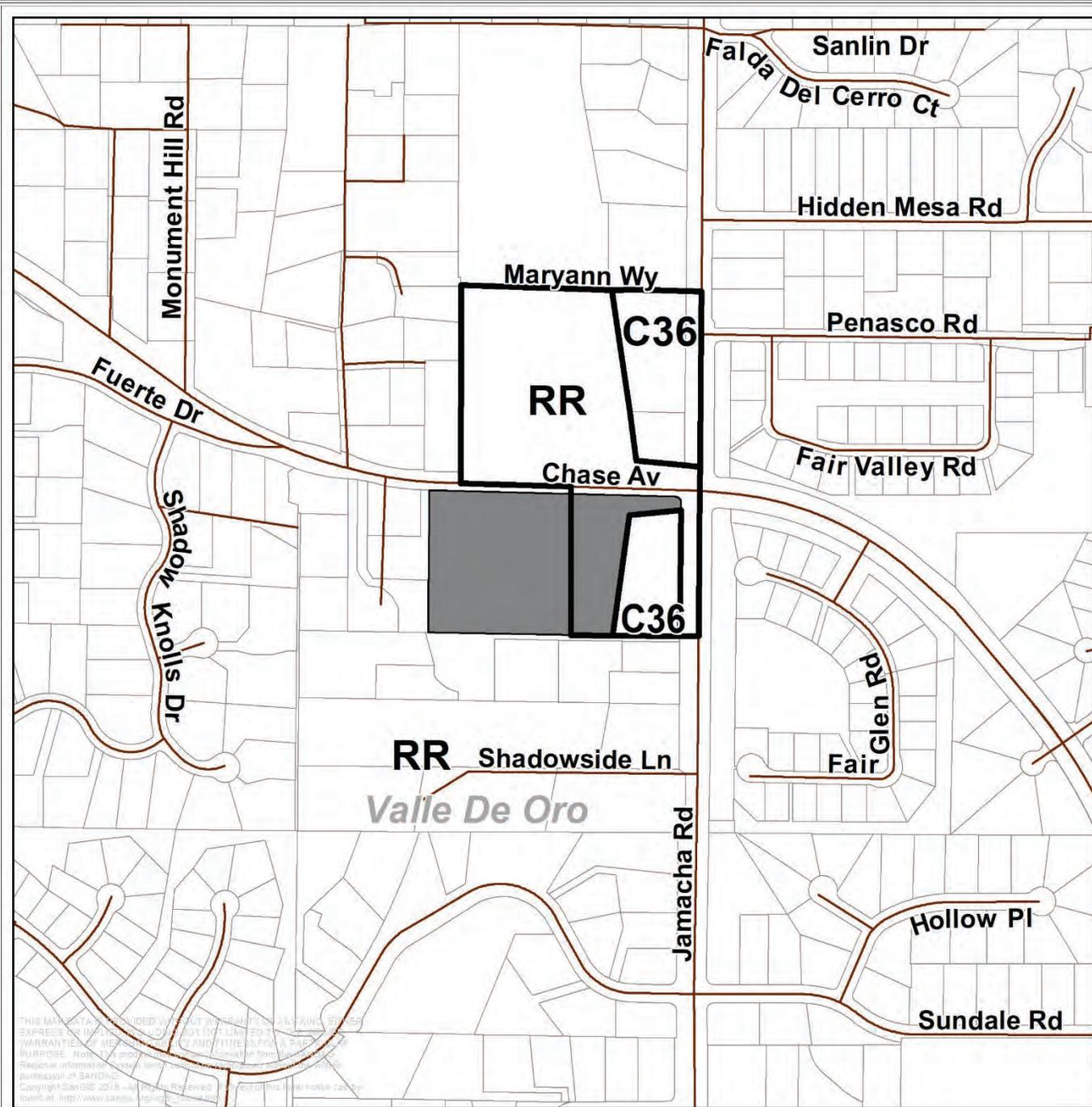


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	Site
	Roads
	Parcels
	Planning



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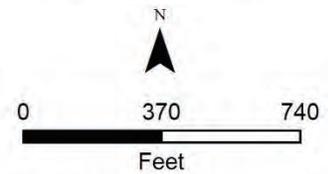
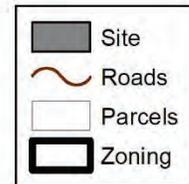


Liberty Charter High School Zoning

Valle De Oro Community Plan Area

C36 - Commercial

RR - Rural Residential



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Liberty Charter High School

Valle De Oro Community Plan Area



Site



Roads



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LAND USE & GIS SERVICES



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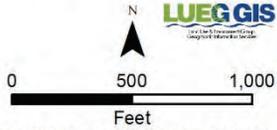
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Liberty Charter High School

Valle De Oro Community Plan Area

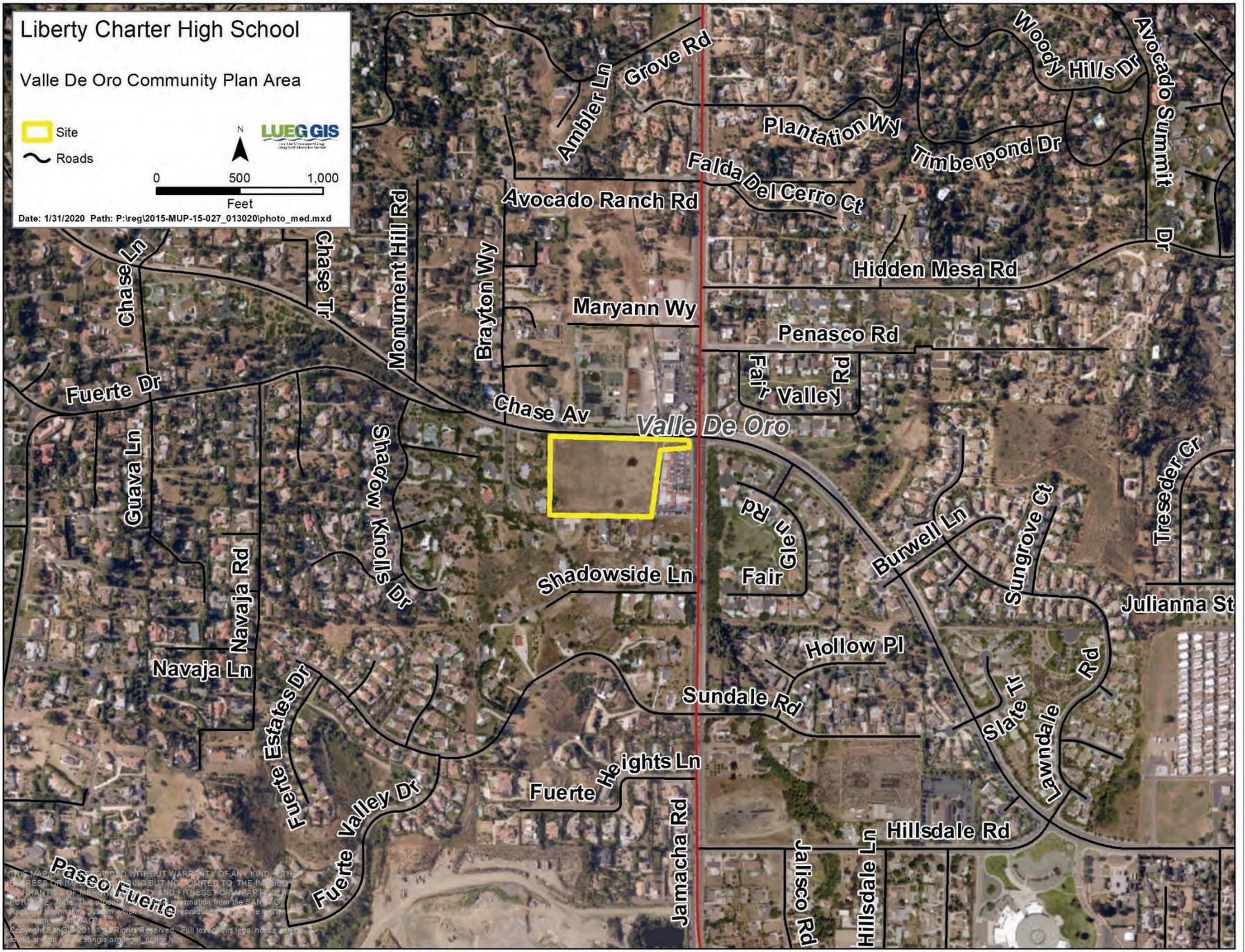
Site

Roads



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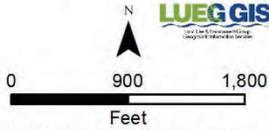
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Liberty Charter High School

Valle De Oro Community Plan Area

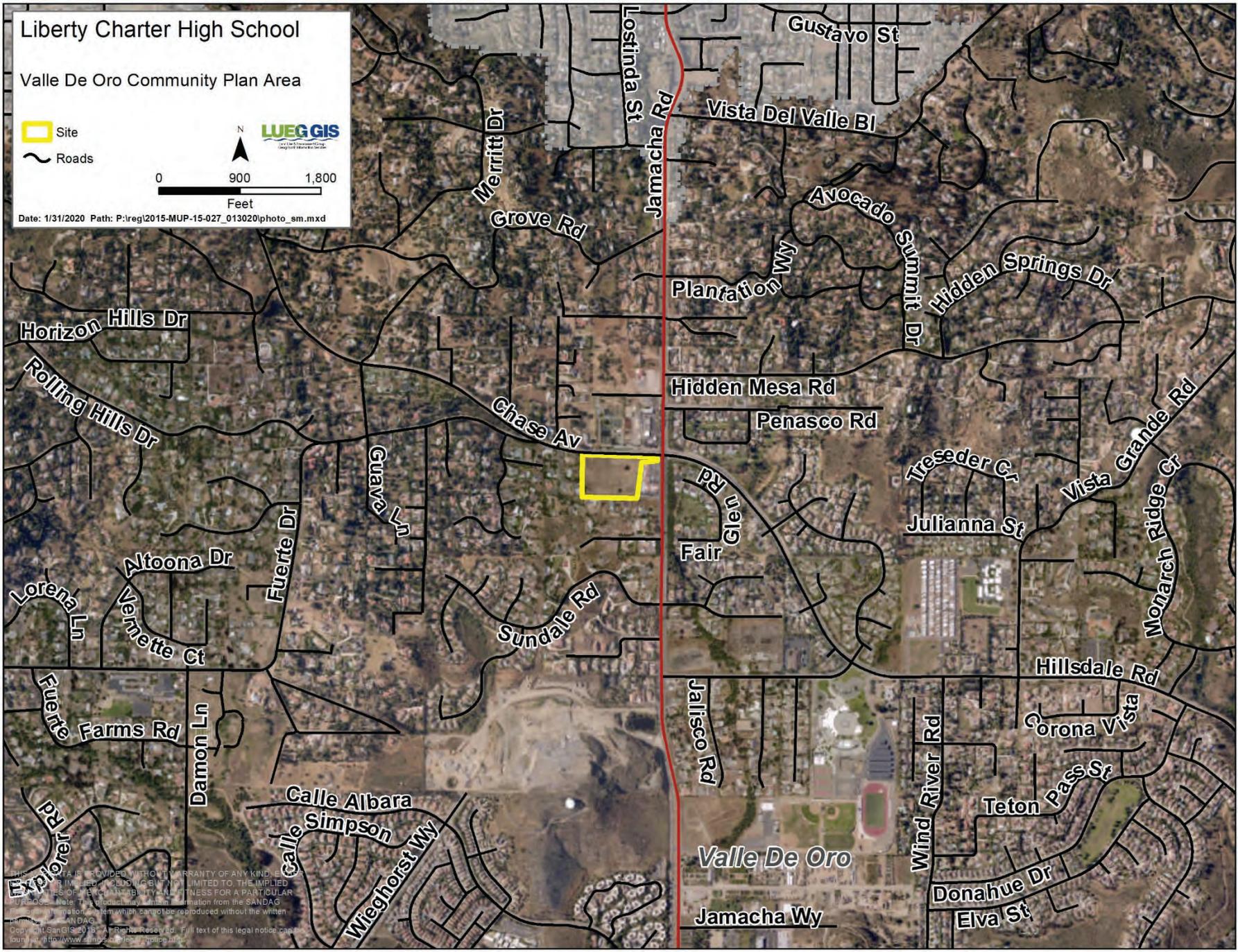
Site

Roads



LUeGGIS

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LIBERTY CHARTER HIGH SCHOOL

EL CAJON, CA

2000
ARCHITECTURE / PLANNING / INTERIORS
1000 S. GARDEN AVENUE, SUITE 100, SAN ANTONIO, TEXAS 78205
WWW.2000ARCHITECTS.COM



PROJECT DIRECTORY

BUILDING OWNERS:
MAX HOLDINGS, LLC
1770 FOURTH AVE.
SAN DIEGO, CA 92101

APPLICANT:
LIBERTY FIRST CHARTER SCHOOLS, INC.
1012 EAST BRADLEY AVE.
EL CAJON, CA 92029
TELEPHONE: (619) 596-5905

ARCHITECT:
PACIFIC CORNERSTONE ARCHITECTS, INC.
11705 SERRANO VALLEY ROAD, SUITE 100
SAN DIEGO, CA 92121
TELEPHONE: (658) 677-9880
FAX: (658) 677-9886
CONTACT: KEVIN PERRY
E-MAIL: kevin@pcornstone.com

CONSULTANT:
KARN ENGINEERING AND SURVEYING
129 WEST 190 STREET
FALLBROOK, CA 95028
TELEPHONE: (760) 728-1134
FAX: (760) 728-0209
CONTACT: SCOTT HARRY
E-MAIL: scottharry@karnengineering.com

LANDSCAPE ARCHITECT:
CAROL CORNELIUS
1044 ROYAL RD, #123
EL CAJON, CA 92021
TELEPHONE: (619) 251-6372
CONTACT: CAROL CORNELIUS
E-MAIL: cornelcar@earthlink.net

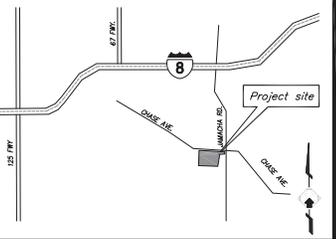
SHEET INDEX

SHEET TITLE	DATE	REVISIONS
1-10 COVER SHEET	06/15/15	CLIENT REVIEW
C-1 PRELIMINARY GRADING PLAN	06/29/15	CLIENT REVIEW
L-1-1 LANDSCAPE CONCEPT PLAN	09/17/15	M.P. SUBMITTAL
L-1-2 LANDSCAPE DETAILS AND LEGEND	04/06/16	M.P. RE-SUBMITTAL
A5-1-0 SITE PLAN	02/10/17	M.P. RE-SUBMITTAL
A5-1-1 FIRST FLOOR PLAN	07/21/17	M.P. RE-SUBMITTAL
A5-1-2 SECOND FLOOR PLAN	07/21/17	M.P. RE-SUBMITTAL
A5-1-3 ROOF PLAN	07/21/17	M.P. RE-SUBMITTAL
A5-1-4 EXTERIOR ELEVATIONS	07/21/17	M.P. RE-SUBMITTAL
A5-1-5 EXTERIOR ELEVATIONS-COURTYARDS	07/21/17	M.P. RE-SUBMITTAL
A5-1-6 CONCEPTUAL PROJECT PERSPECTIVES	07/21/17	M.P. RE-SUBMITTAL
SL-1 SITE LIGHTING PLAN	02/08/18	M.P. RE-SUBMITTAL

ZONE INFORMATION

USE REGULATIONS:	RR
ANIMAL REGULATIONS:	J / O
LOT SIZE:	0.5 AC.
BUILDING TYPE:	C
HEIGHT:	0
SETBACKS:	O / G
OPEN SPACE:	0 / A
SPECIAL AREA REGULATIONS:	NONE

VICINITY MAP



PROJECT DESCRIPTION

PROPOSED CONSTRUCTION OF A NEW CHARTER HIGH SCHOOL, SPORTS FIELD (WITH LIGHT POLES), PARKING LOT (WITH LIGHT POLES), AND ALL SITE IMPROVEMENTS TO SUPPORT. THE BUILDING WILL BE 2-STORY, APPROX. 46,000 SF TOTAL, WITH CAPACITY TO HOUSE APPROX. 33 FACULTY / STAFF AND NO MORE THAN 450 STUDENTS.

* SEE SITE PLAN SHEET **ASS-2** FOR ADDITIONAL INFORMATION

LEGAL DESCRIPTION:

A PORTION OF THE SOUTHEAST QUARTER OF THE NORTHEAST QUARTER OF SECTION 24, TOWNSHIP 16 SOUTH, RANGE 1 WEST, SAN BERNARDINO MERIDIAN, ACCORDING TO OFFICIAL PLAT THEREOF, AS SHOWN ON RECORD OF SURVEY MAP NO. 21359.

* APN: 498-330-38-00
* SITE AREA: 8.83 ACRES

GENERAL NOTES

- REFER TO LANDSCAPE DRAWINGS FOR LOCATION OF ALL LANDSCAPE, IRRIGATION, HARDSCAPE, ETC. GENERAL CONTRACTOR SHALL USE LANDSCAPE PLANS FOR BIDDING THESE ITEMS.
- REFER TO ALL DRAWINGS FOR CURBS, FINISH, SET DRAINAGE, CONTROLS, ETC.
- GENERAL CONTRACTOR SHALL COORDINATE SITE PLAN DATA SHOWN ON THE ARCHITECTURAL, CIVIL, ELECTRICAL AND LANDSCAPE DRAWINGS AND NOTIFY THE ARCHITECT FOR CLARIFICATION OF ANY DISCREPANCIES PRIOR TO ANY CONSTRUCTION.
- REFER TO ALL DRAWINGS FOR ALL ON-SITE ELECTRICAL.
- ALL UNKNOWN PROPERTY LINES, EASEMENTS AND EASEMENTS, BOTH EXISTING AND PROPOSED, ARE SHOWN ON THIS SITE PLAN.
- YARDS USED FOR AREA INCREASES SHALL BE PERMANENTLY MAINTAINED.
- PROVIDE CONCRETE PAD @ ALL TRASH QUARTERS. SEE PLAN FOR LOCATION AND EXTENT.
- PROVIDE 4" WIDE WHITE PAINTED CAL TRANS HIGHWAY CHANNEL STRIPES @ ALL PARKING.
- REFERENCE ARCHITECTURAL SITE PLAN FOR UNASPHALTED ACCESS.
- SETBACK FOR P.U. AND P.U.C. SHALL BE FOUR FEET FROM CURB AND 3' CONCRETE PAD POURED HAVING 10% SLOPE @ ALL DRAWINGS.
- ALL DIMENSIONS TO CURBS ARE TO FRONT FACE OF CURB.
- GENERAL CONTRACTOR SHALL FIELD VERIFY ALL EXISTING SITE CONDITIONS, PROPERTY DIMENSIONS, ETC. PRIOR TO ANY CONSTRUCTION AND SHALL NOTIFY THE ARCHITECT FOR CLARIFICATION OF ANY DISCREPANCIES.
- PRIOR TO THE ISSUANCE OF ANY BUILDING PERMITS, THE APPLICANT SHALL OBTAIN A GRADING PERMIT FROM THE COUNTY OF SAN DIEGO (REFERRED TO AS AN "ENGINEERING PERMIT") FOR THE GRADING PROPOSED FOR THIS PROJECT. ALL GRADING SHALL CONFORM TO THE REQUIREMENTS IN ACCORDANCE WITH THE COUNTY OF SAN DIEGO MUNICIPAL CODE IN A MANNER SATISFACTORY TO THE COUNTY ENGINEER.
- THE DRAINAGE SYSTEM PROPOSED WITH THIS DEVELOPMENT IS SUBJECT TO APPROVAL BY THE COUNTY ENGINEER.

MISC. PLAN CHECK NOTES

- NOTE STATE HEALTH & SAFETY CODE SEC. 17610.5 BANS THE USE OF CHLORINATED POLYVINYL CHLORIDE (CPVC) FOR INTERIOR WATER-SUPPLY PIPING.
- BUILDING BRIDGE AND MONUMENT BRIDGE ARE NOT PART OF THIS PROJECT AND WILL BE PROVIDED UNDER A SEPARATE PERMIT IF APPLICABLE.
- A COVERING THE PROJECT SHALL COMPLY WITH ALL CURRENT STREET LIGHTING STANDARDS ACCORDING TO THE COUNTY OF SAN DIEGO MUNICIPAL CODE. THIS MAY REQUIRE BUT NOT BE LIMITED TO INSTALLATION OF NEW STREET LIGHTS, UPWARD WALL MOUNT FROM LOW PRESSURE TO HIGH PRESSURE SODIUM VAPOR AND/OR UPGRADING WATTAGE.
- ALL BRIDGE ASSOCIATED WITH THE PROJECT WILL NOT A PART OF THIS APPLICATION AND WILL BE PROVIDED UNDER A SEPARATE PERMIT.
- ANY PROPOSED WALL, FENCE (WHICH INCLUDES RETAINING WALLS), SHALL COMPLY WITH THE FENCE REGULATIONS OF THE COUNTY OF SAN DIEGO MUNICIPAL CODE.
- A FIRE ALARM SYSTEM SHALL BE INSTALLED PER IFPA-72 STANDARDS.
- A FIRE SPRINKLER SYSTEM SHALL BE INSTALLED PER IFPA-13 STANDARDS AND SAN MICHAEL CONSOLIDATED FIRE PROTECTION DISTRICT STANDARDS.



Date Issued	Remarks
06/15/15	CLIENT REVIEW
06/29/15	CLIENT REVIEW
09/17/15	M.P. SUBMITTAL
04/06/16	M.P. RE-SUBMITTAL
02/10/17	M.P. RE-SUBMITTAL
07/21/17	M.P. RE-SUBMITTAL
07/21/17	M.P. RE-SUBMITTAL
02/08/18	M.P. RE-SUBMITTAL

CONCEPTUAL DRAWINGS PREPARED BY THE CONTRACTOR. THESE DRAWINGS ARE NOT TO BE USED FOR CONSTRUCTION OR FOR ANY OTHER PURPOSE WITHOUT THE WRITTEN APPROVAL OF THE ARCHITECT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND FOR VERIFYING THE ACCURACY OF ALL INFORMATION PROVIDED TO THE ARCHITECT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND FOR VERIFYING THE ACCURACY OF ALL INFORMATION PROVIDED TO THE ARCHITECT.



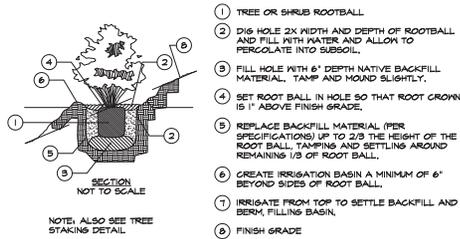
PCA Project No. 15-135
Sheet Title: COVER SHEET

Sheet No. **T1.0**

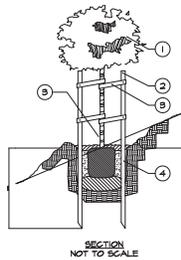
1 - 67

THE DRAWING AND SPECIFICATIONS ARE THE PROPERTY OF PACIFIC CORNERSTONE ARCHITECTS, INC. AND SHALL BE KEPT IN CONFIDENCE. NO PART OF THESE DRAWINGS SHALL BE REPRODUCED OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, INCLUDING PHOTOCOPYING, RECORDING, OR BY ANY INFORMATION STORAGE AND RETRIEVAL SYSTEMS WITHOUT THE WRITTEN PERMISSION OF PACIFIC CORNERSTONE ARCHITECTS, INC. ANY VIOLATION OF THESE TERMS SHALL BE CONSIDERED A BREACH OF CONTRACT AND SUBJECT TO LEGAL ACTION.

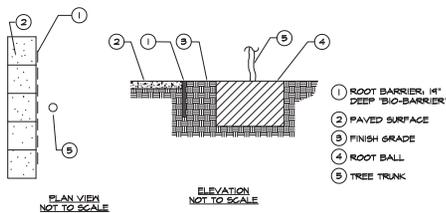
THESE DRAWINGS AND SPECIFICATIONS ARE THE PROPERTY AND COPYRIGHT OF CAROL CORNELIUS, L.L.C. AND SHALL NOT BE USED OR REPRODUCED WITHOUT THE WRITTEN PERMISSION OF CAROL CORNELIUS, L.L.C. ANY VIOLATION SHALL BE SUBJECT TO THE PENALTIES OF FEDERAL AND STATE LAWS. THESE DRAWINGS SHALL BE PROVIDED TO THE CLIENT FOR INFORMATION ONLY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING ALL INFORMATION AND SHALL BE RESPONSIBLE FOR THE SUCCESS OF THE PROJECT. CAROL CORNELIUS, L.L.C. HAS THE RIGHT TO REUSE INFORMATION FROM THESE DRAWINGS IN ANY OF ITS OTHER PROJECTS.



(A) Tree / Shrub Planting NOT TO SCALE



(B) Tree Staking NOT TO SCALE



(C) Root Barrier NOT TO SCALE

- 1 TREE OR SHRUB ROOTBALL
- 2 DIG HOLE 2X WIDTH AND DEPTH OF ROOTBALL AND FILL WITH WATER AND ALLOW TO PERCOLATE INTO SUBSOIL.
- 3 FILL HOLE WITH 6" DEPTH NATIVE BACKFILL MATERIAL. TAMP AND MOULD SLIGHTLY.
- 4 SET ROOT BALL IN HOLE SO THAT ROOT CROWN IS 1" ABOVE FINISH GRADE.
- 5 REPLACE BACKFILL MATERIAL (PER SPECIFICATIONS) UP TO 2/3 THE HEIGHT OF THE ROOT BALL, TAMPING AND BETTLING AROUND REMAINING 1/3 OF ROOT BALL.
- 6 CREATE IRRIGATION BASIN A MINIMUM OF 6" BEYOND SIDES OF ROOT BALL.
- 7 IRRIGATE FROM TOP TO SETTLE BACKFILL AND BERM, FILLING BASIN.
- 8 FINISH GRADE

- 1 TREE PLANTED PER DETAIL
 - 2 STAKE BELOW CROWN OF TREE
 - 3 GORDED RUBBER TREE TIE - 2 PER STAKE, SECURE WITH 16d GALVANIZED NAIL
 - 4 2" DIAMETER X 10' LONG LODGEPOLE PINE STAKE (2 TOTAL)
- NOTE: ALSO SEE TREE/SHRUB PLANTING DETAIL.

- 1 ROOT BARRIER, 14" DEEP "BIO-BARRIER"
- 2 PAVED SURFACE
- 3 FINISH GRADE
- 4 ROOT BALL
- 5 TREE TRUNK

PLANT LEGEND

SYMBOL	BOTANICAL NAME	COMMON NAME	SIZE
TREES SUCH AS:			
+	CHILOPSIS LINEARIS	DESERT WILLOW	24" BOX
+	CERCIS OCCIDENTALIS	WESTERN REDBUD	24" BOX
+	LOPHOSTEMON CONFERTA	BRISBANE BOX	24" BOX
+	OLEA EUROPAEA	SWAN HILL OLIVE	24" BOX
+	QUERCUS AGRIIFOLIA	COAST LIVE OAK	24" BOX
LARGE SHRUBS (8' TALL AND UP) SUCH AS:			
+	AREBUTUS UNEDO	STRAWBERRY TREE	5 GALLON
+	HETEROPHELES AREBUTIFOLIA	TOYON	5 GALLON
+	PHYOTIA FRAGRIS	CHINESE PHOTINIA	5 GALLON
MEDIUM SHRUBS (4' TO 8' TALL AND UP) SUCH AS:			
+	CARPENTERIA CALIFORNICA	BUSH ANEMONE	5 GALLON
+	CISTIS PURPUREA	ORCHID RESCUE	1 GALLON
+	COLEONEMA FILICRUM	PINK BREATH OF HEAVEN	5 GALLON
+	LANTANA DWARF PINK	LANTANA	1 GALLON
+	NANDINA COMPACTA	HEAVENLY BAMBOO	1 GALLON
+	NEPHROLEPS COERIFOLIA	SWORD FERN	1 GALLON
+	PITTIOSPORUM TOBIRA	TOBIRA	5 GALLON
+	RHAPHIOLIPS I, BALLERINA	INDIA HAYTHORN	5 GALLON
+	RHAPHIOLIPS I, JACK EVANS	INDIA HAYTHORN	5 GALLON
+	ROMNEYA GOLDTIER	MATILDA POPPY	5 GALLON
SMALL SHRUBS (3' TALL AND LOWER) SUCH AS:			
+	HELIANTHEMUM NUN-KALARUM	BURROSE	1 GALLON
+	HENROGALLIS YELLOW	DAY LILY	1 GALLON
+	MONARDELLA VILLOSA	GOYOTE MINT	2" LINERS
+	OENOTHERA CALIFORNICA	GALIP, EVENING PRIMROSE	2" LINERS
+	PENSTEMON H. MARGARITA BOF	FOOTHILL PENSTEMON	2" LINERS
+	SALVIA GRESENI	AUTUMN SAGE	1 GALLON
+	TRACHELOSPERMUM JASMINOIDES	STAR JASMINE	1 GALLON
GROUND COVERS SUCH AS:			
+	TRACHELOSPERMUM JASMINOIDES	STAR JASMINE	FLTS # 24" OC.
+	MYOPORUM PARVIFOLIUM PUTAH GRECK	PROSTRATE MYOPORUM	FLTS. # 18" OC.
+	BACCHARIS PILLULARIS	GOYOTE BRUSH	FLTS. # 18" OC.
+	ASPLOSIS PALENS (FOR SIDES OF BASIN) THINGRASS (BMP I & 2)		FLTS. # 18" OC.
+	MULDENBERGIA RIGENS/BOTTOM OF BASIN/DEERGRASS (BMP I & 2)		FLTS. # 18" OC.
+	MARATHON III	DWARF FESCUE	BY 500



Date Issued	Revised
05/15/15	GENE REVIEW
05/20/15	GENE REVIEW
09/17/15	M.P. SEMITEL
04/06/16	M.P. RE-SEMTEL
02/10/17	M.P. RE-SEMTEL
07/21/17	M.P. RE-SEMTEL
02/08/18	M.P. RE-SEMTEL

CONCEPTUAL DRAWINGS
 THESE DRAWINGS ARE PRELIMINARY AND FOR INFORMATION ONLY. THEY ARE NOT TO BE USED FOR CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING ALL INFORMATION AND SHALL BE RESPONSIBLE FOR THE SUCCESS OF THE PROJECT. CAROL CORNELIUS, L.L.C. HAS THE RIGHT TO REUSE INFORMATION FROM THESE DRAWINGS IN ANY OF ITS OTHER PROJECTS.

Proposed Project:
 LIBERTY CHARTER
 HIGH SCHOOL
 - MAJOR USE PERMIT -



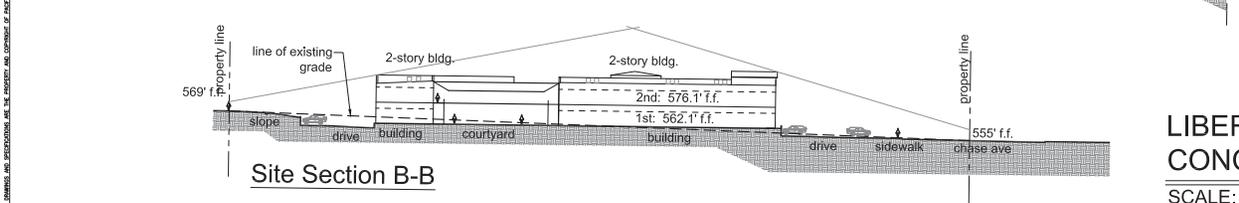
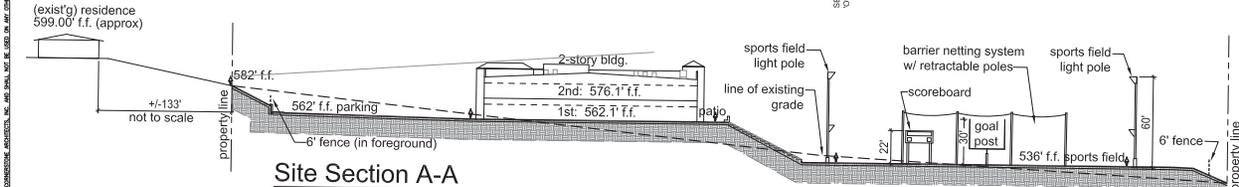
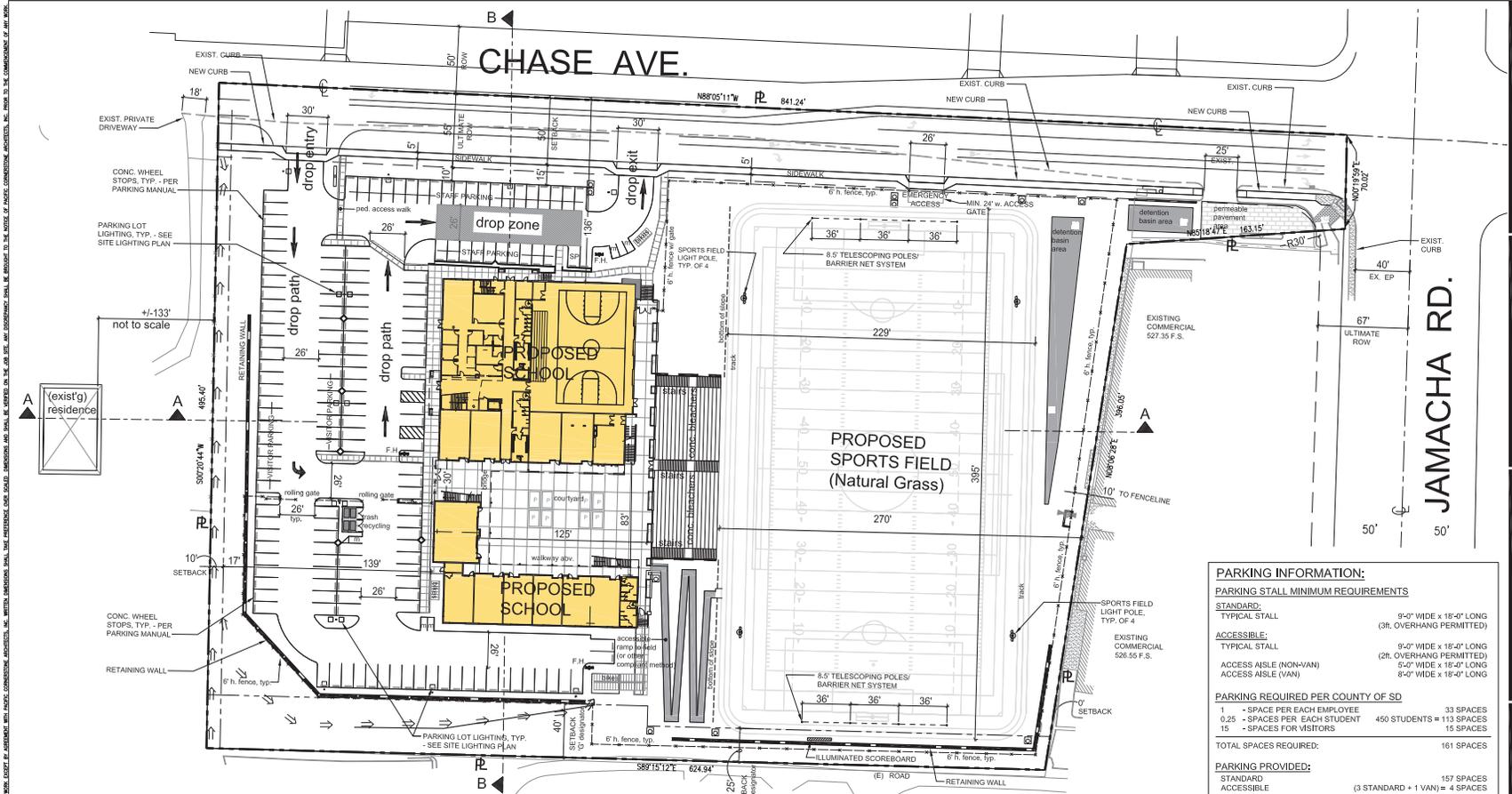
PCA Project No. 18-138

Sheet Title:
 LANDSCAPE DETAILS
 AND LEGEND

Sheet No.

L2.0

DATE: 4-21-15
 DRAWN BY: CF
 JOB NUMBER: 14004
 FILE: GDM/LIBERTY FIRST LCP
 REVISIONS:



PARKING INFORMATION:

PARKING STALL MINIMUM REQUIREMENTS

STANDARD:
 TYPICAL STALL 9'-0" WIDE x 18'-0" LONG
 (3% OVERHANG PERMITTED)

ACCESSIBLE:
 TYPICAL STALL 9'-0" WIDE x 18'-0" LONG
 (2% OVERHANG PERMITTED)

ACCESS AISLE (NON-VAN) 8'-0" WIDE x 18'-0" LONG
 ACCESS AISLE (VAN) 8'-0" WIDE x 18'-0" LONG

PARKING REQUIRED PER COUNTY OF SD

1 - SPACE PER EACH EMPLOYEE 33 SPACES
 0.25 - SPACES PER EACH STUDENT 450 STUDENTS = 113 SPACES
 15 - SPACES FOR VISITORS 15 SPACES

TOTAL SPACES REQUIRED: 161 SPACES

PARKING PROVIDED:

STANDARD 157 SPACES
 ACCESSIBLE (3 STANDARD + 1 VAN) = 4 SPACES

161 SPACES

MOTORCYCLE PARKING PROVIDED = 6 SPACES

BICYCLE PARKING PROVIDED (0.1 PER STUDENT) = 45 BIKE SPACES

SEATING CAPACITY:
 BLEACHERS: 800 MAX.
 GYM: 500 MAX.

PARKING REQUIRED (BASED ON SEATING CAPACITY):
 ► BLEACHERS: 800 X 2 = 160 STALLS
 ► GYM: 500 X 2 = 100 STALLS

► NOTE THESE EVENTS DO NOT HAPPEN AT THE SAME TIME, NOR DURING SCHOOL HOURS, MAX REQUIRED 160 STALLS.

TOTAL PARKING PROVIDED IS: 161 SPACES

SITE LIGHTING NOTE
 ALL OUTDOOR LIGHTING FIXTURES SHALL BE SHADED ON TOP SO THAT ALL LIGHT IS DIRECTED DOWNWARD.



Date Issued 06/15/15
Client GLEF REVIEW
Drawn 06/29/15
Checked 09/11/15
Scale M.P. SE-BMTL
Project 04/06/15 M.P. RE-SBMTL
Revision 02/10/17 M.P. RE-SBMTL
 07/21/17 M.P. RE-SBMTL
 02/08/18 M.P. RE-SBMTL

CONCEPTUAL DRAWINGS are provided for information only. They are not to be used for construction. The contractor shall be responsible for all details and dimensions. The contractor shall verify all dimensions and conditions on site before construction. The contractor shall be responsible for all details and dimensions. The contractor shall verify all dimensions and conditions on site before construction.

Proposed Project:
 LIBERTY CHARTER
 HIGH SCHOOL
 -MAJOR USE PERMIT-



PCA Project No. 15-135

Sheet Title:
 SITE PLAN

Sheet No.
AS1.0

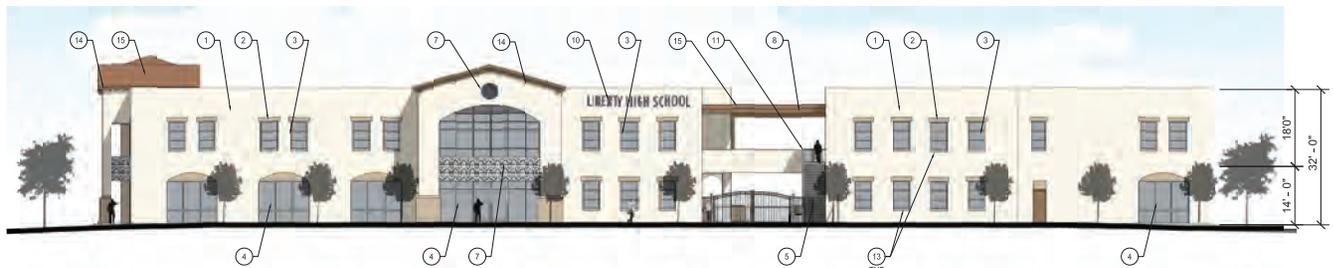
LIBERTY - LITERACY 1st.
CONCEPTUAL SITE PLAN

SCALE: 1"=30'-0"

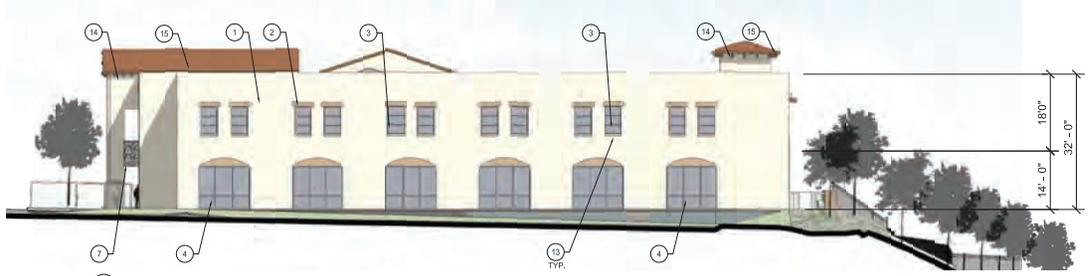


1-71

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A WEST ELEVATION



B SOUTH ELEVATION



C EAST ELEVATION



D NORTH ELEVATION

EXTERIOR ELEVATIONS

SCALE: 3/32" = 1'-0"

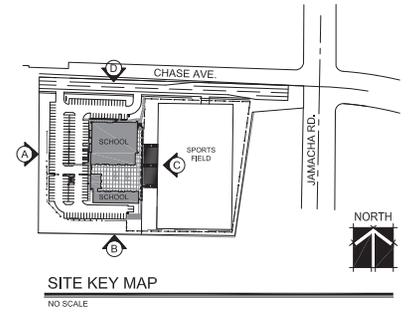
ELEVATION LEGEND

NOTE: MOST ALL REVNOTES WILL BE REFERENCED ON EACH BUILDING ELEVATION

- 1 CONCRETE WALL PANEL - COLOR 1
- 2 CONCRETE WALL PANEL - COLOR 2
- 3 DUAL GLAZED - WINDOW SYSTEM W/ ACCENT MULLIONS
- 4 DUAL GLAZED - STOREFRONT WINDOW DOOR SYSTEM
- 5 METAL AND CONCRETE STAR W/ STEEL SUPPORTS AND DECORATIVE TILE
- 6 STEEL GATE
- 7 DECORATIVE TILES
- 8 CANOPY FEATURE
- 9 GLASS AND ALUMINUM FIREHOUSE DOORS
- 10 LOGO / SIGNAGE W/ ACCENT LIGHTING
- 11 METAL RAILING AND SUPPORT POSTS
- 12 ELDERADO STONE - CAPISTRANO ADDBE
- 13 CHAMFERED EDGE
- 14 HEAVY TIMBER RAFTERS
- 15 TWO PIECE CLAY TILE ROOF BY REDLANDS OR EQ. 8020 MIX CAFE ANTIQUA OLD HACIENDA OR APPROVED EQUAL CONCRETE TILE 20% BOOSTED
- 16 ORNAMENTAL TRELIS

*MEASURED AT THE AVERAGE HEIGHT OF THE HIGHEST GABLE OF A PITCH OR HIPPED ROOF. PER ORDINANCE NO. 1048 SAN DIEGO COUNTY ZONING ORDINANCE

TOWERS, GABLES, SPIRES, STEEPLES, BUNDECKS, SCENERY LOFTS, CUPOLAS, AND SIMILAR STRUCTURES AND NECESSARY MECHANICAL APPURTENANCES PROVIDED, HOWEVER, THAT NO SUCH STRUCTURE MAY EXTEND MORE THAN 20 FEET ABOVE THE MAXIMUM HEIGHT SPECIFIED BY THE APPLICABLE HEIGHT DESIGNATOR R OF COMBUSTIBLE MATERIALS. PER ORDINANCE NO. 10072 SAN DIEGO COUNTY ZONING ORDINANCE



SITE KEY MAP

NO SCALE



Date Issued	Remarks
06/15/15	CLIENT REVIEW
06/29/15	CLIENT REVIEW
09/11/15	M.P. SUBMITTAL
04/06/16	M.P. RE-SUBMITTAL
02/10/17	M.P. RE-SUBMITTAL
07/21/17	M.P. RE-SUBMITTAL
02/08/18	M.P. RE-SUBMITTAL

CONCEPTUAL DRAWINGS PREPARED BY THE CONTRACTOR. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVING ALL LOCAL, STATE, FEDERAL AND COUNTY REGULATIONS AND ORDINANCES THAT APPLY TO THIS PROJECT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVING ALL LOCAL, STATE, FEDERAL AND COUNTY REGULATIONS AND ORDINANCES THAT APPLY TO THIS PROJECT.

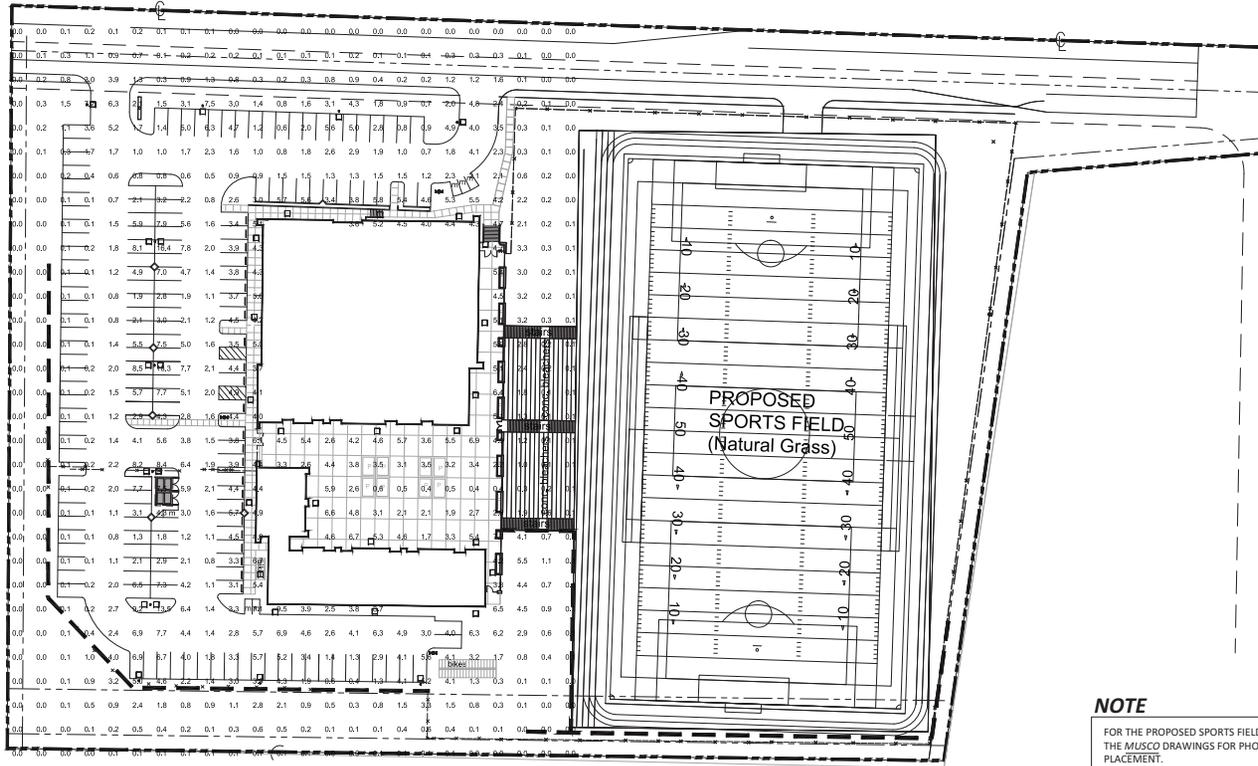
Proposed Project:
 LIBERTY CHARTER HIGH SCHOOL
 -MAJOR USE PERMIT-

PCA Project No. 15-135
 Sheet Title:
EXTERIOR ELEVATIONS

Sheet No.
A4.0

1-75

CHASE AVE.



JAMACHA RD.

NOTE

FOR THE PROPOSED SPORTS FIELD LIGHTING, PLEASE REFER TO THE MUSCO DRAWINGS FOR PHOTOMETRICS AND LIGHT POLE PLACEMENT.

**ELECTRICAL
SITE LIGHTING
PLAN**

SCALE: 1" = 30'-0"

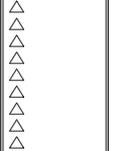


**HAMANN
CONSTRUCTION
ELECTRICAL DIVISION**

CR Seuss

CARL R. SEUSS
2000 PIONEER WAY
EL CAJON, CA 92020
602-455-8888
FAX: 602-455-8889
ELECTRICAL DIVISION

DATE: MARCH 15, 2016
DRAWN BY: CARL SEUSS



3/22/2016 2:41:03 PM

project: NEW LIGHTING FOR
LIBERTY HIGH SCHOOL

SL-1

**Attachment D –
ENVIRONMENTAL DOCUMENTATION**

**CEQA Technical Studies are available
on the PDS website:**

**[https://www.sandiegocounty.gov/pds/
ceqa/MUP-15-027.html](https://www.sandiegocounty.gov/pds/ceqa/MUP-15-027.html)**



County of San Diego

MARK WARDLAW
DIRECTOR

PLANNING & DEVELOPMENT SERVICES
5510 OVERLAND AVENUE, SUITE 310, SAN DIEGO, CA 92123
www.sdcounty.ca.gov/pds

KATHLEEN FLANNERY
ASSISTANT DIRECTOR

MITIGATED NEGATIVE DECLARATION

PROJECT NAME: Liberty Charter High School

RECORD ID: PDS2015-MUP-15-027

ENVIRONMENTAL LOG NO.: PDS2015-ER-15-14-010

This Document is Considered Draft Until it is Adopted by the Appropriate County of San Diego Decision-Making Body.

This Mitigated Negative Declaration is comprised of this form along with the Environmental Initial Study that includes the following:

- a. Initial Study – Environmental Checklist Form
 - b. Technical Studies and Reports as referenced
1. California Environmental Quality Act Mitigated Negative Declaration Findings:

Find, that this Mitigated Negative Declaration reflects the decision-making body's independent judgment and analysis, and; that the decision-making body has reviewed and considered the information contained in this Mitigated Negative Declaration and the comments received during the public review period; and that revisions in the project plans or proposals made by or agreed to by the project applicant would avoid the effects or mitigate the effects to a point where clearly no significant effects would occur; and, on the basis of the whole record before the decision-making body (including this Mitigated Negative Declaration) that there is no substantial evidence that the project as revised will have a significant effect on the environment.

2. Required Mitigation Measures:

Refer to the attached Environmental Initial Study for the rationale for requiring the following measures:

BIOLOGICAL RESOURCES

1. BIO#1 - OFF-SITE MITIGATION – NON-NATIVE GRASSLAND

INTENT: In order to mitigate for the impacts to 5.26 acres of Non-native grassland habitat, which is a sensitive biological resource pursuant to CEQA, and the [Biological Mitigation Ordinance \(BMO\)](#), off-site mitigation (0.5:1 ratio) shall be acquired.

DESCRIPTION OF REQUIREMENT: The applicant shall purchase habitat credit or

provide for the conservation of 2.62 acres of Tier III habitat or higher, located within the South County MSCP Subarea as indicated below.

- a. **Option 1:** If purchasing Mitigation Credit, the mitigation bank shall be approved by the California Department of Fish & Wildlife. The following evidence of purchase shall include the following information to be provided by the mitigation bank:
 1. A copy of the purchase contract referencing the project name and numbers for which the habitat credits were purchased.
 2. Submit a Bill of Sale or Credit Sales receipt
 3. If not stated explicitly in the purchase contract, a separate letter must be provided identifying the entity responsible for the long-term management and monitoring of the preserved land.
 4. To ensure the land will be protected in perpetuity, evidence must be provided that a dedicated conservation easement or similar land constraint has been placed over the mitigation land.
 5. An accounting of the status of the mitigation bank. This shall include the total amount of credits available at the bank, the amount required by this project and the amount remaining after utilization by this project.
- b. **Option 2:** If habitat credit cannot be purchased in a mitigation bank, then the applicant shall provide for the conservation habitat of the same amount and type of land located within the South County MSCP Subarea in San Diego County as indicated below:
 1. Prior to purchasing the land for the proposed mitigation, the location should be pre-approved by [PDS].
 2. A Resource Management Plan (RMP) shall be prepared and approved pursuant to the County of San Diego Biological Report Format and Content Requirements to the satisfaction of the Director of PDS. If the offsite mitigation is proposed to be managed by DPR, the RMP shall also be prepared and approved to the satisfaction of the Director of DPR.
 3. An open space easement over the land shall be dedicated to the County of San Diego or like agency to the satisfaction of the Director of PDS. The land shall be protected in perpetuity.
 4. The purchase and dedication of the land and the selection of the Resource Manager and establishment of an endowment to ensure funding of annual ongoing basic stewardship costs shall be complete prior to the approval of the RMP.
 5. In lieu of providing a private habitat manager, the applicant may contract with a federal, state or local government agency with the primary mission of resource management to take fee title and manage the mitigation land). Evidence of satisfaction must include a copy of the contract with the agency, and a written statement from the agency that (1) the land contains the

specified acreage and the specified habitat, or like functioning habitat, and (2) the land will be managed by the agency for conservation of natural resources in perpetuity.

DOCUMENTATION: The applicant shall purchase the off-site mitigation credits and provide the evidence to the [PDS, PPD] for review and approval. If the offsite mitigation is proposed to be owned or managed by DPR, the applicant must provide evidence to the [PDS PPD] that [DPR, GPM] agrees to this proposal. It is recommended that the applicant submit the mitigation proposal to the [PDS, PPD], for pre-approval. If option #2 is selected, a RMP shall be prepared and an application for the RMP shall be submitted to the [PDS, ZONING]. **TIMING:** Prior to approval of any plan or issuance of any permit, and prior to use of the premises in reliance of this permit, the mitigation shall occur. **MONITORING:** The [PDS, PPD] shall review the mitigation purchase for compliance with this condition. Upon request from the applicant [PDS, PPD] can pre-approve the location and type of mitigation only. The credits shall be purchased before the requirement can be completed. If the applicant chooses option #2, then the [PDS, ZONING] shall accept an application for an RMP, and [PDS, PPD] [DPR, GPM] shall review the RMP submittal for compliance with this condition and the RMP Guidelines.

2. **BIO#2-RESOURCE AVOIDANCE [PDS, FEE X2]**

INTENT: In order to avoid impacts to avian species and raptors which are sensitive biological resources pursuant to the Migratory Bird Treaty Act (MBTA), a Resource Avoidance Area (RAA), shall be implemented on all plans. **DESCRIPTION OF REQUIREMENT:** There shall be no brushing, clearing and/or grading such that none will be allowed during the breeding season of avian species and raptors. The breeding season is defined as occurring between January 15th and September 15th. The Director of PDS [PDS, PCC] may waive this condition, through written concurrence from the US Fish and Wildlife Service and the California Department of Fish and Wildlife, provided that no nesting avian species or raptors are present in the vicinity of the brushing, clearing or grading. **DOCUMENTATION:** The applicant shall provide a letter of agreement with this condition; alternatively, the applicant may submit a written request for waiver of this condition. Although, No Grading shall occur within the RAA until concurrence is received from the County and the Wildlife Agencies. **TIMING:** Prior to preconstruction conference and prior to any clearing, grubbing, trenching, grading, or any land disturbances and throughout the duration of the grading and construction, compliance with this condition is mandatory unless the requirement is waived by the County upon receipt of concurrence from the Wildlife Agencies. **MONITORING:** The [DPW, PDC] shall not allow any grading in the RAA during the specified dates, unless a concurrence from the [PDS, PCC] is received. The [PDS, PCC] shall review the concurrence letter.”

AIR QUALITY

3. **AQ#1 – CONSTRUCTION EXHAUST EMISSIONS**

INTENT: In order to mitigate for exhaust emissions. **DESCRIPTION OF REQUIREMENT:** The project shall comply with the following Air Quality measure:

- a. The project shall require the construction contractor to provide a construction fleet that uses heavy-duty diesel-powered equipment equipped with Tier 4 Final diesel engines. An exemption from these requirements may be granted by the County if corresponding

reductions in criteria air pollutant emissions are achieved from other construction equipment.

- b. The project shall require the construction contractor to provide a construction fleet that uses heavy-duty diesel-powered equipment equipped with diesel particulate filters (DPFs).
- c. Construction equipment shall be outfitted with best available control technology (BACT) devices certified by the California Air Resources Board. A copy of each unit's BACT documentation shall be provided to the County of San Diego at the time of mobilization of each applicable unit of equipment.

DOCUMENTATION: The applicant shall comply with the Air Quality requirements of this condition. The applicant shall show compliance with this measure by providing the construction bid/estimate from the construction contractor that will be used. **TIMING:** Prior to approval of any grading and or improvement plans and issuance of any Grading or Construction Permits. **MONITORING:** The [DPW, PDCI] shall make sure that the construction contractor complies with the Air Quality requirement of this condition. The [DPW, PDCI] shall contact the [PDS, PCC] if the applicant fails to comply with this condition.

TRANSPORTATION

4. ROADS#3-ROAD IMPROVEMENTS

INTENT: In order to promote orderly development and to comply with the [County of San Diego Board Policy I-18](#), the [County Public Road Standards](#), and the [County Community Trails Master Plan](#), *Chase Avenue* shall be improved. **DESCRIPTION OF REQUIREMENT:**

- ~~a. Improve or agree to improve and provide security for Chase Avenue, along the project frontage in accordance with Public Road Standards for a 4.1B Major Road, to a graded width of fifty-five feet (55') and to an improved width of forty-five feet (45') from centerline with asphalt concrete pavement over approved base with Portland cement concrete curb, gutter and sidewalk. The face of curb shall be located forty-five feet (45') from official centerline. Provide transition, tapers, traffic striping to match existing pavement. All of the above shall be to the satisfaction of the Director of Planning & Development Services.~~
- a. Improve or agree to improve and provide security for **Chase Avenue (SA 910.1)**, along the project frontage in accordance with Public Road Standards for a 4.1B Major Road with a Class IV Bikeway and a raised median with intermittent turn lanes, to a graded width of fifty-five feet (55') and to an improved width of forty-five feet (45') from centerline with asphalt concrete pavement over approved base with Portland cement concrete curb, gutter and sidewalk. The face of curb shall be located forty-five feet (45') from official centerline. A Parking Prohibition shall be proposed on both sides of **Chase Avenue (SA 910.1)** along the project frontage. Provide signage and striping for school zone. Provide transition, tapers, traffic striping to match existing pavement. All of the above shall be to the satisfaction of the Directors of Planning & Development Services (PDS) and the Department of Public Works (DPW).
- b. Improve or agree to improve and provide security for the intersection of **Chase Avenue (SA 910.1)** and **Jamacha Road (SF 1399)** in accordance with Public Road Standards, CALTRANS standards and requirements and Americans with Disabilities

Act (ADA) standards to include ADA pedestrian ramps at all four corners, crosswalks, markings, signage, striping, countdown pedestrian traffic signal heads, and pedestrian push buttons with audible functionality. Provide transition, tapers, traffic striping to match existing pavement. All of the above shall be to the satisfaction of the Directors of PDS, DPW and CALTRANS.

- c. Asphalt concrete surfacing material shall be hand-raked and compacted to form smooth tapered connections along all edges including those edges adjacent to soil. The edges of asphalt concrete shall be hand-raked at 45 degrees or flatter, so as to provide a smooth transition next to existing soil, including those areas scheduled for shoulder backing.

All plans and improvements shall be completed pursuant to the [County of San Diego Public Road Standards](#), the PDS [Land Development Improvement Plan Checking Manual](#) and the Community Trails Master Plan. The improvements shall be completed within 24 months from the approval of the improvement plans, execution of the agreements, and acceptance of the securities. **DOCUMENTATION:** The applicant shall complete the following:

- d. Process and obtain approval of Improvement Plans to improve Chase Avenue (SA 910.1).
- e. Process and obtain approval of Improvement Plans to improve the intersection of **Chase Avenue (SA 910.1)** and **Jamacha Road (SF 1399)**.
- f. Provide Secured agreements. The required security shall be in accordance with [Section 7613 of the Zoning Ordinance](#).
- g. Pay all applicable inspection fees with [DPW, PDC].
- h. If the applicant is a representative, then one of the following is required: a corporate certificate indicating those corporation officers authorized to sign for the corporation, or a partnership agreement recorded in this County indicating who is authorized to sign for the partnership.
- i. Obtain approval for the design and construction of all driveways, turnarounds, pathways and private easement road improvements to the satisfaction of the San Miguel Fire Protection District and the [PDS, LDR].
- j. Obtain a Construction Permit for any work within the County road right-of-way. PDS Construction/Road right-of-way Permits Services Section should be contacted at (858) 694-3275 to coordinate departmental requirements. Also, before trimming, removing or planting trees or shrubs in the County Road right-of-way, the applicant must first obtain a permit to remove, plant or trim shrubs or trees from the Permit Services Section.

TIMING: Prior to approval of any occupancy permits, and prior to use of the premises in reliance of this permit, the plans shall be approved, and securities must be provided.

MONITORING: The [PDS, LDR] and [DPR, TC] shall review the plans for consistency

with the condition and County Standards and Community Trails Master Plan. Upon approval of the plans [PDS, LDR] shall request the required securities and improvement agreements. The securities and improvement agreements shall be approved by the Director of PDS before any work can commence.

5. TRAF#1 – TRANSPORTATION IMPACT FEE: [PDS, LDR] [PDS, BD] [BP]

INTENT: In order to mitigate potential cumulative traffic impacts to less than significant, and to comply with the Transportation Impact Fee (TIF) Ordinance Number 77.201-77.219, the TIF shall be paid. **DESCRIPTION OF REQUIREMENT:** The Transportation Impact Fee (TIF) shall be paid pursuant to the County TIF Ordinance number 77.201-77.219 for Select Industrial Uses in the area based on Average Daily Trips (ADT) identified in the approved Traffic Study. The fee is required for the entire project, or it can be paid at building permit issuance for each phase of the project. The fee is calculated pursuant to the ordinance. **DOCUMENTATION:** The applicant shall pay the TIF at the [PDS, ZONING] and provide a copy of the receipt to the [PDS, Building Division Technician] at time of permit issuance. The cost of the fee shall be calculated at time of payment. **TIMING:** Prior to approval of any building plan and the issuance of any building permit or use of the premises in reliance of this permit, the TIF shall be paid. **MONITORING:** The [PDS, ZONING] shall calculate the fee pursuant to the ordinance and provide a receipt of payment for the applicant. [PDS, Building Division] shall verify that the TIF has been paid before the first building permit can be issued. The TIF shall be verified for each subsequent building permit issuance.

3. Critical Project Design Elements That Must Become Conditions of Approval:

The following project design elements were either proposed in the project application or the result of compliance with specific environmental laws and regulations and were essential in reaching the conclusions within the attached Environmental Initial Study. While the following are not technically mitigation measures, their implementation must be assured to avoid potentially significant environmental effects.

1. GEN#1 – COST RECOVERY: [PDS, DPW, DEH, DPR], [GP, CP, BP, UO]

INTENT: In order to comply with Section 362 of Article XX of the San Diego County Administrative Code, Schedule B.5, existing deficit accounts associated with processing this permit shall be paid. **DESCRIPTION OF REQUIREMENT:** The applicant shall pay off all existing deficit accounts associated with processing this permit. **DOCUMENTATION:** The applicant shall provide a receipt to Planning & Development Services, Zoning Counter, which shows that all discretionary deposit accounts have been paid. No permit can be issued if there are deficit deposit accounts. **TIMING:** Prior to the approval of any plan and prior to the issuance of any permit and prior to use in reliance of this permit, all fees and discretionary deposit accounts shall be paid. **MONITORING:** The PDS Zoning Counter shall review the receipts and verify that all PDS, DPW, DEH, and DPR deposit accounts have been paid.

2. GEN#2 – RECORDATION OF DECISION: [PDS], [GP, CP, BP, UO]

INTENT: In order to comply with Section 7019 of the Zoning Ordinance, the Permit Decision shall be recorded to provide constructive notice to all purchasers, transferees, or other successors to the interests of the owners named, of the rights and obligations created by this permit. **DESCRIPTION OF REQUIREMENT:** The applicant shall sign,

notarize with an 'all purpose acknowledgement' and return the original Recordation Form to PDS. **DOCUMENTATION:** Signed and notarized original Recordation Form. **TIMING:** Prior to the approval of any plan and prior to the issuance of any permit and prior to use in reliance of this permit, a signed and notarized copy of the Decision shall be recorded by PDS at the County Recorder's Office. **MONITORING:** The PDS Zoning Counter shall verify that the Decision was recorded and that a copy of the recorded document is on file at PDS.

3. LNDCP#1–LANDSCAPE DOCUMENTATION PACKAGE

INTENT: In order to provide adequate Landscaping that provides screening, a landscape plan shall be prepared. **DESCRIPTION OF REQUIREMENT:** The Landscape Plans shall be prepared pursuant to the [COSD Water Efficient Landscape Design Manual](#) and the [COSD Water Conservation in Landscaping Ordinance](#), the [COSD Parking Design Manual](#), the COSD Climate Action Plan and the COSD Grading Ordinance. All Plans shall be prepared by a California licensed Landscape Architect, Architect, or Civil Engineer, and include the following information:

- a. Indication of the proposed width of any adjacent public right-of-way, and the locations of any required improvements and any proposed plant materials to be installed or planted therein. The applicant shall also obtain a permit approving the variety, location, and spacing of all trees proposed to be planted within said right(s) -of-way. A copy of this permit and a letter stating that all landscaping within the said right(s) -of-way shall be maintained by the landowner(s) shall be submitted to PDS.
- b. A complete planting plan including the names, sizes, and locations of all plant materials, including trees, shrubs, and groundcover. Wherever appropriate, native or naturalizing plant materials shall be used which can thrive on natural moisture. These plants shall be irrigated only to establish the plantings.
- c. A complete watering system including the location, size, and type of all backflow prevention devices, pressure, and non-pressure water lines, valves, and sprinkler heads in those areas requiring a permanent, and/or temporary irrigation system.
- d. The watering system configuration shall indicate how water flow, including irrigation runoff, low head drainage, overspray or other similar conditions will not impact adjacent property, non-irrigated areas, structures, walkways, roadways or other paved areas, including trails and pathways by causing water to flow across, or onto these areas.
- e. Spot elevations of the hardscape, building and proposed fine grading of the installed landscape.
- f. The location and detail of all walls, fences, and walkways shall be shown on the plans, including height from grade and type of material. A lighting plan and light standard details shall be included in the plans (if applicable) and shall be in compliance with the [County's Light Pollution Code](#).
- g. No landscaping material or irrigation or other infrastructure shall be located within a proposed trail easement or designated pathway.
- h. Parking areas shall be landscaped and designed pursuant to the [Parking Design Manual](#) and the County Zoning Ordinance Section 6793.b

- i. Additionally, the following items shall be addressed as part of the Landscape Documentation Package:
- j. All required street trees shall be planted outside of the public right-of-way on private property. If tree planting is proposed within the public right-of-way, a copy of an Encroachment Maintenance and Removal Agreement issued by the Department of Public Works shall be included with this submittal.
- k. Screening shrubs, mature height reaching 6 feet minimum, 15 gallon minimum container size, low water use, such as *Arbutus unedo* (Strawberry Tree) and *Heteromeles Arbutifolia* (Toyon) shall be provided at perimeter planting areas on the south, the west and especially the east, where adjacent to viewing areas from Scenic Highway Jamacha Road.
- l. Provide a six-foot-high solid fence or wall at the west and south property lines of the site. Provide construction details on the landscape plans reflecting the proposed condition.
- m. All proposed planting shall be clearly delineated on the planting plan and noted in the plant legend with one unique symbol for each tree, shrub, vine, and one unique hatch for each ground cover plant etc.
- n. Please be aware that the County's Climate Action Plan (CAP) was approved last February (2/14/18) and now requires an ETAF value of 0.42 be used within the MAWA formula instead of the current 0.55 for residential projects. Measure W-1.2 (Reduce Outdoor Water Use) of the CAP requires a 40% reduction in outdoor water use as compared to the baseline year of the CAP (2014). The ETAF used during 2014 was 0.7. 40% of that is 0.42. The calculations on the Water Efficient Landscape Worksheet, submitted during Final Landscape Plans, must reflect the 0.42 value.

DOCUMENTATION: The applicant shall prepare the Landscape Plans using the [Landscape Documentation Package Checklist](#) (PDS Form #404), and pay all applicable review fees. **TIMING:** Prior to approval of any plan, issuance of any permit, and prior to use of the premises in reliance of this permit, the Landscape Documentation Package shall be prepared and approved. **MONITORING:** The [PDS, LA] and [DPR, TC, PP] shall review the Landscape Documentation Package for compliance with this condition.

4. CULT#1 ARCHAEOLOGICAL MONITORING [PDS, FEE X 2]

INTENT: In order to mitigate for potential impacts to undiscovered buried archaeological resources, an Archaeological Monitoring Program and potential Data Recovery Program shall be implemented pursuant to the County of San Diego Guidelines for Determining Significance for Cultural Resources and the California Environmental Quality Act (CEQA). **DESCRIPTION OF REQUIREMENT:** A County Approved Principal Investigator (PI) known as the "Project Archaeologist," shall be contracted to perform archaeological monitoring and a potential data recovery program during all grading, clearing, grubbing, trenching, and construction activities. The archaeological monitoring program shall include the following:

- a. The Project Archaeologist shall perform the monitoring duties before, during and after construction pursuant to the most current version of the County of San Diego Guidelines for Determining Significance and Report Format and Requirements for Cultural Resources. The Project Archaeologist and Kumeyaay Native American

monitor shall also evaluate fill soils to determine that they are clean of cultural resources. The contract or letter of acceptance provided to the County shall include an agreement that the archaeological monitoring will be completed, and a Memorandum of Understanding (MOU) between the Project Archaeologist and the County of San Diego shall be executed. The contract or letter acceptance shall include a cost estimate for the monitoring work and reporting.

- b. The Project Archeologist shall provide evidence that a Kumeyaay Native American has been contracted to perform Native American Monitoring for the project.
- c. The cost of the monitoring shall be added to the grading bonds or bonded separately.

DOCUMENTATION: The applicant shall provide a copy of the Archaeological Monitoring Contract or letter of acceptance, cost estimate, and MOU to the [PDS, PPD]. Additionally, the cost amount of the monitoring work shall be added to the grading bond cost estimate.

TIMING: Prior to approval of any grading and or improvement plans and issuance of any Grading or Construction Permits. **MONITORING:** The [PDS, PPD] shall review the contract or letter of acceptance, MOU and cost estimate or separate bonds for compliance with this condition. The cost estimate should be forwarded to [PDS, LDR], for inclusion in the grading bond cost estimate, and grading bonds and the grading monitoring requirement shall be made a condition of the issuance of the grading or construction permit.

5. **ROADS#1-ENCROACHMENT PERMIT**

INTENT: In order to ensure that improvements for the intersection of Chase Avenue (SA 910.1) and Jamacha Road (SF 1399) comply with the County of San Diego Public Road Standards, and The Caltrans Facility Standards and Requirements an encroachment permit(s) shall be obtained and implemented. **DESCRIPTION OF REQUIREMENT:** A permit shall be obtained from CALTRANS for the improvements to be made within the Caltrans' right-of-way. The improvements will include the upgrade of the existing pedestrian curb ramps, replacement of the existing pedestrian signal heads to be countdown and the replacement of the existing pedestrian push buttons to meet ADA requirements. A copy of the permit and evidence from the issuing agency that all requirements of the permit have been met shall be submitted to the [PDS, LDR].

DOCUMENTATION: The applicant shall obtain the encroachment permit(s) and provide a copy of the permit, proof of payment, and evidence that all the requirements of the permit have been met, to the [PDS, LDR]. The applicant should contact the CALTRANS Permit Office at (619) 688-6843 for additional details. **TIMING:** Prior to occupancy or use of the premises in reliance of this permit, the encroachment permit shall be obtained.

MONITORING: The [PDS, LDR] shall review the permit for compliance with this condition and the applicable improvement plans and implement any conditions of the permit in the County improvement plans.

6. **ROADS#2-ROAD DEDICATION**

INTENT: In order to promote orderly development and to comply with the [County of San Diego Board Policy I-18](#), the [County Public Road Standards](#), and the [Community Trails Master Plan](#), road right of way shall be dedicated to the County. **DESCRIPTION OF REQUIREMENT:** Grant by separate document to the County of San Diego an easement for road purposes that provides:

- a. A one-half right-of-way width of fifty-five feet (55') from centerline, plus slope rights and drainage easements for **Chase Avenue (SA 910.1)** along the frontage of the project in accordance with Mobility Element 4.1B Major Road Standards.
- b. A one-half right-of-way width of sixty-seven feet (67') from centerline, plus slope rights and drainage easements for **Jamacha Road (SF 1399)** along the frontage of the project in accordance with Mobility Element 6.2 Prime Arterial Standards.
- c. A thirty-foot (30') radius at the intersection of Chase Avenue and Jamacha Road.

The grant of right-of-way shall be free of any burdens or encumbrances, which would interfere with the purpose for which it is required and shall be accepted for public use. The affected utility company/district shall enter into a joint use agreement with the County of San Diego to the satisfaction of the Director of PDS. **DOCUMENTATION:** The applicant shall prepare the legal descriptions of the easements, and submit them for preparation with the [DGS, RP], and pay all applicable fees associated with preparation of the documents. Upon Recordation of the easements, the applicant shall provide copies of the easement documents to the [PDS, LDR] for review. **TIMING:** Prior to approval of any plan or issuance of any permit, and prior to use of the premises in reliance of this permit the easements shall be executed and recorded. **MONITORING:** The [DGS, RP] shall prepare, approve the easement documents for recordation, and forward the recorded copies to [PDS, LDR] for review and approval. The [PDS, LDR] shall review the easements to assure compliance with this condition.

7. ROADS#4–SIGHT DISTANCE

INTENT: In order to provide an unobstructed view for safety while exiting the property and accessing a public road from the site, and to comply with the Design Standards of Section 6.1.(E) of the [County of San Diego Public Road Standards](#), an unobstructed sight distance shall be verified. **DESCRIPTION OF REQUIREMENT:**

- a. A registered civil engineer or a licensed land surveyor provides a certified signed statement that: "There is _____ feet of unobstructed intersectional sight distance in both directions from each of the proposed driveways along **Chase Avenue (SA 910.1)** in accordance with the methodology described in Table 5 of the March 2012 County of San Diego Public Road Standards. These sight distances exceed the required intersectional Sight Distance requirements of _____ as described in Table 5 based on a speed of _____, which I have verified to be the higher of the prevailing speed or the minimum design speed of the road classification. I have exercised responsible charge for the certification as defined in Section 6703 of the Professional Engineers Act of the California Business and Professions Code."
- b. If the lines of sight fall within the existing public road right-of-way, the engineer or surveyor shall further certify: "Said lines of sight fall within the existing right-of-way and a clear space easement is not required."

DOCUMENTATION: The applicant shall complete the certifications and submit them to the [PDS, LDR] for review. **TIMING:** Prior to the approval of any plan, issuance of any permit, and prior to occupancy or use of the premises in reliance of this permit, the sight distance shall be verified. **MONITORING:** The [PDS, LDR] shall verify the sight distance certifications.

8. ROADS#5–RELINQUISH ACCESS

INTENT: In order to promote orderly development and to comply with the [Mobility Element of the General Plan](#) access shall be relinquished. **DESCRIPTION OF REQUIREMENT:**

- a. Relinquish access rights onto **Chase Avenue (SA 910.1)** and **Jamacha Road (SF 1399)** with the exception of the driveways as shown on the approved plot plan. The access relinquishment shall be free of any burdens or encumbrances, which would interfere with the purpose for which it is required. Only the three access point(s) are permitted along **Chase Avenue (SA 910.1)** as indicated on the approved plot plan.
- b. The access relinquishment shall be free of any burdens or encumbrances, which would interfere with the purpose for which it is required.

DOCUMENTATION: The applicant shall prepare the legal descriptions of the easement(s), submit them for preparation with the [DGS, RP], and pay all applicable fees associated with preparation of the documents. **TIMING:** Prior to approval of any plan or issuance of any permit, and prior to use of the premises in reliance of this permit the access shall be relinquished. **MONITORING:** The [DGS, RP] shall prepare the relinquishment documents and forward a copy of the documents to [PDS, LDR] for preapproval. [DGS, RP] shall forward copies of the recorded documents to [PDS, LDR]. The [PDS, LDR] shall review the documents for compliance with this condition.

9. ROADS#6–INSTALL STREETLIGHTS

INTENT: In order to promote orderly development and to comply with the Street Lighting Requirements of the [Centerline Ordinance 9974 \(Amended by Ord. 10224\)](#), [County Code Section 51.301 et seq.](#), [County of San Diego Board Policy I-18](#), street lights shall be installed and energized. **DESCRIPTION OF REQUIREMENT:** Install or arrange to install streetlights to County standards and the satisfaction of the Director of PDS, and deposit with PDS, a cash deposit sufficient to energize and operate the street lights until the property has been transferred into Zone A. **DOCUMENTATION:** The applicant shall pay the Zone A Lighting District Annexation Fee at the [PDS, LDR], and arrange for the installation and energizing of the streetlights. **TIMING:** Prior to occupancy of the first structure built in association with this permit, final grading release, or use in the premises in reliance of this permit, the streetlights shall be installed and all fees paid. **MONITORING:** The [PDS, LDR] shall calculate the fee pursuant to this condition and provide a receipt of payment for the applicant. The [PDS, LDR] shall ensure that the streetlights have been installed and all fees have been paid.

10. ROADS#7–ANNEX TO LIGHTING DISTRICT

INTENT: In order to promote orderly development and to comply with the Street Lighting Requirements of the [County of San Diego Board Policy I-18](#), and [The County of San Diego Public Road Standards](#), the property shall transfer into the lighting district. **DESCRIPTION OF REQUIREMENT:** Allow the transfer of the property subject of this permit into Zone A of the San Diego County Street Lighting District without notice or hearing and pay the cost to process such transfer. **DOCUMENTATION:** The applicant shall pay the Zone A Lighting District Annexation Fee at the [PDS, LDR]. The applicant shall provide the receipt to [PDS, PCC]. **TIMING:** Prior to occupancy of the first structure

built in association with this permit or use in the premises in reliance of this permit, the fee shall be paid. **MONITORING:** The [PDS, LDR] shall calculate the fee pursuant to this condition and provide a receipt of payment for the applicant.

11. ROADS#8–SIGHT DISTANCE

INTENT: In order to provide an unobstructed view for safety while exiting the property and accessing a public road from the site, and to comply with the Design Standards of Section 6.1.E of the County of San Diego Public Road Standards, an unobstructed sight distance shall be verified. **DESCRIPTION OF REQUIREMENT:**

- a. A registered civil engineer or a licensed land surveyor provides a certified signed statement that: "There is _____feet of unobstructed intersectional sight distance in the both directions from the proposed driveways along **Chase Avenue (SA 910.1)** in accordance with the methodology described in Table 5 of the March 2012 County of San Diego Public Road Standards. These sight distances exceed the required intersectional Sight Distance requirements of _____as described in Table 5 based on a speed of _____, which I have verified to be the higher of the prevailing speed or the minimum design speed of the road classification. I have exercised responsible charge for the certification as defined in Section 6703 of the Professional Engineers Act of the California Business and Professions Code."
- b. If the lines of sight fall within the existing public road right-of-way, the engineer or surveyor shall further certify: "Said lines of sight fall within the existing right-of-way and a clear space easement is not required."

DOCUMENTATION: The applicant shall complete the certifications and submit them to the [PDS, LDR] for review. **TIMING:** Prior to occupancy of the first structure built in association with this permit or use of the premises in reliance of this permit, and annually after that until the project is completely built, the sight distance shall be verified. **MONITORING:** The [PDS, LDR] shall verify the sight distance certifications for compliance with this condition.

12. ROADS#9–SIGHT DISTANCE

INTENT: In order to provide an unobstructed view for safety while exiting the property and accessing a public road from the site, and to comply with the Design Standards of Section 6.1.(E) of the County of San Diego Public Road Standards, an unobstructed sight distance shall be maintained for the life of this permit. **DESCRIPTION OF REQUIREMENT:** There shall be a minimum unobstructed sight distance in both directions along **Chase Avenue (SA 910.1)** from the project driveway openings for the life of this permit. **DOCUMENTATION:** A minimum unobstructed sight shall be maintained. The sight distance of adjacent driveways and street openings shall not be adversely affected by this project at any time. **TIMING:** Upon establishment of the use, this condition shall apply for the duration of the term of this permit. **MONITORING:** The [PDS, Code Compliance Division] is responsible for compliance of this permit.

13. PLN#1 – PLAN CONFORMANCE: [DPW, ESU] [GP, IP,] [DPR, TC, PP]

INTENT: In order to implement the required mitigation measures for the project, the required plot plans shall conform to the approved Conceptual Grading and Development Plan pursuant to Section 87.207 of the County Grading Ordinance. **DESCRIPTION OF**

REQUIREMENT: The plot plans shall conform to the approved Conceptual Grading and Development Plan. All conditions, requirements, mitigation measures and information stated on the sheets of the plans shall be made conditions of the permit's issuance and shall be implemented pursuant to the adopted Mitigation Monitoring and Reporting Program (MMRP) of this Permit. No deviation of the requirements can be made without modification of this permit. **DOCUMENTATION:** The applicant shall submit the grading plans and improvement plans, which conform to the conceptual development plan for the project. **TIMING:** Prior to approval of any grading or improvement plan and prior to issuance of any grading or construction permit, the notes and items shall be placed on the plans as required. **MONITORING:** The [DPW, Environmental Services Unit Division, DPR, TC, or PDS, Building Division for Minor Grading] shall verify that the grading and or improvement plan requirements have been implemented on the final grading and or improvement plans as applicable. The environmental mitigation notes shall be made conditions of the issuance of said grading or construction permit.

14. UTILITIES#1–PAVEMENT CUT POLICY

INTENT: In order to prohibit trench cuts for undergrounding of utilities in all new, reconstructed, or resurfaced paved County-maintained roads for a period of three years following project surfacing, and to comply with [County Policy RO-7](#) adjacent property owners shall be notified and solicited for their participation in the extension of utilities. **DESCRIPTION OF REQUIREMENT:** All adjacent property owners shall be notified who may be affected by this policy and are considering development of applicable properties, this includes requesting their participation in the extension of utilities to comply with this policy. No trench cuts for undergrounding of utilities in all new, reconstructed, or resurfaced paved County-maintained roads for a period of three years following project surface. **DOCUMENTATION:** The applicant shall sign a statement that they are aware of the County of San Diego Pavement Cut Policy and submit it to the [PDS LDR] for review. **TIMING:** Prior to approval of any grading or improvement plan and prior to issuance of any grading or construction permit, and prior to use of the property in reliance of this permit, the letters must be submitted for approval. **MONITORING:** [PDS, LDR] shall review the signed letters to determine compliance with the condition.

15. STRMWTR#1–STORMWATER FACILITIES MAINTENANCE AGREEMENT

INTENT: In order to promote orderly development and to comply with the [County Flood Damage Prevention Ordinance 10091 \(Title 8, Division 11\)](#), and the [County Watershed Protection Ordinance \(WPO\) No.10385, County Code Section 67.801 et. seq.](#), the maintenance agreements shall be completed. **DESCRIPTION OF REQUIREMENT:**

- a. The private storm drain system shall be maintained by a maintenance mechanism such as a homeowners association or other private entity to the satisfaction of the Director of DPW and/or PDS.
- b. Establish a maintenance agreement / mechanism (to include easements) to assure maintenance of the Category 2 post-construction best management practices (BMP's). Provide security to back up the maintenance pursuant to the County Maintenance Plan Guidelines to the satisfaction of the Director of DPW and/or PDS.

DOCUMENTATION: The applicant shall process the agreement forms with [PDS, LDR] and pay the deposit and applicable review fees. **TIMING:** Prior to approval of any grading

or improvement plan or construction permit, prior to use of the property in reliance of this permit; execution of the agreements and securities shall be completed and recorded. **MONITORING:** The [PDS, LDR] shall review the agreements/mechanisms for consistency with the condition and County Standards.

16. STRMWTR#2–EROSION CONTROL

INTENT: In order to Comply with all applicable stormwater regulations the activities proposed under this application are subject to enforcement under permits from the [State Construction General Permit, Order No. 2009-00090-DWQ](#), or subsequent order and the [County Watershed Protection Ordinance \(WPO\) No.10410, County Code Section 67.801 et. seq.](#), and all other applicable ordinances and standards for this priority project.

DESCRIPTION OF REQUIREMENT: The applicant shall maintain the appropriate on-site and offsite Best Management Practices pursuant to the approved Stormwater Quality Management Plan (SWQMP) and Erosion Control Plan including, but not limited to the erosion control measures, irrigation systems, slope protection, drainage systems, desilting basins, energy dissipators, and silt control measure.

- a. An agreement and instrument of credit shall be provided for an amount equal to the cost of this work as determined or approved by the [PDS, LDR], in accordance with the [County of San Diego Grading Ordinance Section 87.304](#). The cash deposit collected for grading, per the grading ordinance, will be used for emergency erosion measures. The developer shall submit a letter to [PDS, LDR] authorizing the use of this deposit for emergency measures.
- b. An agreement in a form satisfactory to County Counsel shall accompany the Instrument of Credit to authorize the County to unilaterally withdraw any part of or all the Instrument of Credit to accomplish any of the work agreed to if it is not accomplished to the satisfaction of the County PDS and/or DPW by the date agreed.

DOCUMENTATION: The applicant shall process an Erosion Control Plan and provide the letter of agreement and any additional security and/or cash deposit to the [PDS, LDR].

TIMING: Prior to approval of any grading or improvement plan or construction permit, and prior to use of the property in reliance of this permit, the Erosion Control Plan shall be approved and the agreement and securities shall be executed. **MONITORING:** The [PDS, LDR] shall ensure that the Erosion Control Plan adequately satisfies the requirements of the conditions to potentially perform the required erosion control and stormwater control measures proposed on all construction and grading plans. [DPW, PDC] shall use the securities pursuant to the agreement to implement and enforce the required stormwater and erosion control measures pursuant to this condition during all construction phases as long as there are open and valid permits for the site.

17. STRMWTR#3–VERIFICATION OF STRUCTURAL BMPs

INTENT: In order to promote orderly development and to comply with the [County Watershed Protection Ordinance \(WPO\) No.10410, County Code Section 67.801 et. seq.](#), verification of Structural BMPs shall be completed. **DESCRIPTION OF REQUIREMENT:**

Complete a Structural BMP Verification Form as shown in Attachment 10 of the PDP SWQMP. **DOCUMENTATION:** The applicant shall process the Structural BMP Verification Forms *with* [DPW, PDC] *or* [PDS, BLDG]. **TIMING:** Prior to any occupancy, final grading release, or use of the premises in reliance of this permit; execution of the Structural BMP Verification Form shall be completed. **MONITORING:** The [PDS, LDR]

and [DPW, WPP] shall review the Structural BMP Verification Forms for consistency with the condition and County Standards.

18. STRMWTR#4–PROVISION OF STORMWATER DOCUMENTATION TO PROPERTY OWNER

INTENT: In order to promote orderly development and to comply with the [County Watershed Protection Ordinance \(WPO\) No.10410, County Code Section 67.801 et. seq.](#), stormwater documentation shall be provided to property owner. **DESCRIPTION OF REQUIREMENT:** Demonstrate that copies of the following documents are provided to the property owner and initial occupants:

- a. A copy of the project's approved SWQMP (with attached Operation & Maintenance Plan).
- b. A copy of project's recorded Maintenance Notification Agreement and/or Stormwater Facilities Maintenance Agreement and/or Private Road Maintenance Agreement showing the Structural BMPs pertaining to the property.
- c. Sample copies of the following:
 - 1) A Letter for Privately Owned Stormwater Treatment Control Best Management Practices Operation and Maintenance Verification.
 - 2) One Operation and Maintenance Verification Form for each type of Private Treatment Control BMP.

DOCUMENTATION: The applicant shall submit a letter stating that the above documentation has been submitted to the property owner and initial occupants. **TIMING:** Prior to any occupancy, final grading release, or use of the premises in reliance of this permit; provision of stormwater documents shall be completed. **MONITORING:** The [PDS, LDR] and [DPW, WPP] shall review the letter provided by the applicant for consistency with the condition and County Standards.

19. STRMWTR#5–SELF-VERIFICATION OPERATION AND MAINTENANCE LETTER

INTENT: In order to promote orderly development and to comply with the [County Watershed Protection Ordinance \(WPO\) No.10410, County Code Section 67.801 et. seq.](#), an operation and maintenance verification form for each Structural BMPs shall be completed. **DESCRIPTION OF REQUIREMENT:** Every year the property owner shall receive from the County a BMP Verification Form to be completed for each privately owned Structural BMP. **DOCUMENTATION:** Every year the property owner shall file with the County the completed Structural BMP Verification Form stating the maintenance performed during the reporting period for each privately owned Structural BMP with [DPW, WPP]. **TIMING:** Upon establishment of the use, this condition shall apply for the duration of the term of this permit. **MONITORING:** The [DPW, WPP] is responsible for compliance of this permit.

20. AQ#2 – FUGITIVE DUST

INTENT: In order to mitigate for fugitive dust during construction activities. **DESCRIPTION OF REQUIREMENT:** The project shall comply with the following Air Quality measures:

- a. All haul/dump trucks entering or leaving the site with soil or fill material must maintain at least two (2) feet of freeboard, cover loads of all haul/dump trucks securely, or secure transported material by watering or treating.
- b. Dust control measures of the Grading Ordinance will be enhanced with a minimum of two (2) daily applications of water to the construction areas between dozer/scrapper passes and on any unpaved roads within the project limits.
- c. Grading is to be terminated in winds in excess of 25 miles per hour (mph).
- d. Sweepers and water trucks shall be used to control dust and debris at public street access points and approach routes to construction sites.
- e. Dirt storage piles will be stabilized by chemical binders, tarps, fencing, or other suppression measures.
- f. Internal construction-roadways will be stabilized by paving, chip sealing or chemicals after rough grading.
- g. A 15-mph speed limit on unpaved surfaces shall be enforced.
- h. Disturbed areas shall be hydroseeded, landscaped, or developed as quickly as possible and as directed by the County and/or SDAPCD to reduce dust generation.
- i. Visible roadway dust as a result of active operations, spillage from transport trucks, erosion, or track-out/carry-out shall be minimized by the use of track-out grates, gravel beds, or wheel-washing at each egress point and be removed at the conclusion of each work day when active operations cease, or every 24 hours for continuous operations. If a street sweeper is used to remove any track-out/carry-out, only respirable particulate matter (PM₁₀) -efficient street sweepers certified to meet the most current South Coast Air Quality Management District (SCAQMD) Rule 1186 requirements shall be used. The use of blowers for removal of track-out/carry-out is prohibited under any circumstances.

DOCUMENTATION: The applicant or designee shall comply with the Air Quality requirements of this condition. **TIMING:** The following actions shall occur throughout the duration of construction and grading. **MONITORING:** The [DPW, PDCI] shall make sure that the grading contractor complies with the Air Quality requirements of this condition. The [DPW, PDCI] shall contact the [PDS, PCC] if the applicant fails to comply with this condition.

21. NOISE#1 - NOISE REQUIREMENT

INTENT: In order to comply with the [County of San Diego Noise Ordinance 36.404](#) as evaluated in the [County of San Diego Noise Guidelines for Determining Significance](#), the following design measures shall be implemented on the building plans and in the site design. **DESCRIPTION OF REQUIREMENT:** The following design elements and noise attenuation measures shall be implemented and indicated on the building plans and made conditions of its issuance: dual-pane windows with a minimum sound transmission class (STC) rating of 26 and mechanical ventilation as identified in the Noise Study prepared by LDN Engineering (August 3, 2017). **DOCUMENTATION:** The applicant shall place the design elements or notes on the building plans and submit the plans to [PDS, BPPR] for review and approval. **TIMING:** Prior to issuance of any building permit, the design elements and noise attenuation measures shall be incorporated into the building plans. **MONITORING:** The [PDS, BPPR] shall verify that the specific note(s), and design elements, and noise attenuation measures have been placed on all sets of the building plans and made conditions of its issuance.

22. NOISE#2 - NOISE REQUIREMENT – MECHANICAL

INTENT: In order to reduce the impacts of the exterior sound levels from the project HVAC units on the adjacent parcels and to comply with the County of San Diego Noise Ordinance, Section 36.404, and the County of San Diego Noise Guidelines for Determining Significance, the following design measures shall be implemented on the building plans and in the site design. **DESCRIPTION OF REQUIREMENT:** The following design elements and noise attenuation measures shall be implemented and indicated on the building plans and made conditions of its issuance: HVAC units associated with the building shall incorporate visual screening by a parapet wall or barrier of equal or greater height in relation to the HVAC unit's elevation for noise screening purposes for the southern property line. **DOCUMENTATION:** The applicant shall place the design elements, or notes on the building plans and submit the plans to [PDS, BPPR] for review and approval. **TIMING:** Prior to issuance of any building permit, the design elements and noise attenuation measures shall be incorporated into the building plans. **MONITORING:** The [PDS, BPPR] shall verify that the specific note(s), and design elements, and noise attenuation measures have been placed on all sets of the building plans and made conditions of its issuance.

23. AQ#3 – CONSTRUCTION ARCHITECTURAL COATINGS

INTENT: In order to reduce emissions of volatile organic compounds (VOC). **DESCRIPTION OF REQUIREMENT:** The project shall comply with the following Air Quality measure:

- a. Residential interior VOC coatings are to be less than or equal to 50 grams per liter (g/L).
- b. Residential exterior coatings are to be less than or equal to 100 g/L.
- c. Non-residential interior/exterior coatings are to be less than or equal to 100 g/L.

DOCUMENTATION: The applicant shall comply with the Air Quality requirements of this condition. **TIMING:** The following action shall occur throughout the duration of the construction activities involving the application of architectural coatings. **MONITORING:** The [DPW, PDCI] shall make sure that the construction contractor complies with the Air Quality requirement of this condition. The [DPW, PDCI] shall contact the [PDS, PCC] if the applicant fails to comply with this condition.

24. AQ#4 – WATER AND WASTE REDUCTION

INTENT: In order to reduce greenhouse gases (GHGs), the project shall include the following features. **DESCRIPTION OF REQUIREMENT:** The project shall implement or install the following measures to under County Ordinance Sections 68.508 through 68.518 and 2016 CALGreen Standards:

- b. In accordance with Part 11 of Title 24, California Green Building Code (CALGreen) criteria and state and local laws, at least 50 percent of operational waste would be diverted from landfills through reuse and recycling.
- c. Provide areas for storage and collection of recyclables and yard waste in accordance with 2016 CALGreen.
- d. The Project would provide a 20 percent water reduction from the statewide average in accordance with 2016 CALGreen.

DOCUMENTATION: The applicant or designee shall comply with the Air Quality requirements of this condition. **TIMING:** The following actions shall occur throughout the duration of project operations. **MONITORING:** The [PDS, PC] shall review building plans to ensure that the project meets the requirements of these conditions.

25. AQ#5 – SUSTAINABILITY DESIGN FEATURES

INTENT: In order to reduce emissions of greenhouse gases. **DESCRIPTION OF REQUIREMENT:** The project shall comply with the following Air Quality and Greenhouse Gas measures:

- a. The Project will be designed to meet 2016 Title 24 energy efficiency standards assuming construction begins before January 1, 2020. If constructed after January 1, 2020, the project would be required to meet the more energy efficient 2019 Title 24 standards.
- b. The Project will install, at least, a 90-kilowatt (kW) photovoltaic (PV) system.
- c. The Project will install, at least, two (2) Level II electric vehicle (EV) charging stations within the project parking area.

DOCUMENTATION: The applicant or designee shall comply with the Air Quality requirements of this condition. **TIMING:** The following actions shall occur throughout the duration of project operations. **MONITORING:** The [PDS, PC] shall review the building plans to ensure the project meets the requirements of this condition.

26. AQ#6 – COUNTY CLIMATE ACTION PLAN SATISFACTION

INTENT: In order to comply with the County's Climate Action Plan (CAP) measures, the project shall include the following features. **DESCRIPTION OF REQUIREMENT:** The project shall implement or install the following measures or features as required within Step 2 of the CAP Measure Consistency Checklist.

- a. **Measure 2:** The project would incorporate measures to reduce commute vehicle miles traveled (VMT) through a combination of transportation demand management (TDM) and parking strategies. Strategies implemented by the project will include:
 - i. The project will promote carpooling to and from the project site to reduce the number of staff commute and student pick-up/drop-off trips.
 - ii. The project will provide carpool- and vanpool-only parking spaces which would encourage commuters to carpool and vanpool to the Project site.
 - iii. The project will install two (2) Level II electric vehicle charging stations on-site.
 - iv. The project will install bicycle parking spaces in the form of bicycle racks which would provide bicycle parking for students and staff utilizing nearby bike routes.
- b. **Measure 6:** The project shall submit a Landscape Document Package that complies with the County's Water Conservation Ordinance and demonstrates a 40 percent reduction in current Maximum Applied Water Allowance for outdoor use.

DOCUMENTATION: The applicant shall comply with the requirements of this condition. **TIMING:** Prior to the approval of the final map and prior to the approval of any plan and issuance of any permit. **MONITORING:** The [PDS, BPPR] shall verify that the Building

Plans comply with **Measure 2** of this condition. The [PDS, LA] and [DPR, TC, PP] shall review the Landscape Plans for compliance with **Measure 6** of this condition.

27. PLN#2 – INSPECTION FEE: [PDS, ZONING][PDS, PCO] [UO][DPR, TC, PP].

INTENT: In order to comply with Zoning Ordinance Section 7362.e the Discretionary Inspection Fee shall be paid. **DESCRIPTION OF REQUIREMENT:** Pay the Discretionary Permit Inspection Fee at the [PDS, Zoning Counter] to cover the cost of inspection(s) of the property to monitor ongoing conditions associated with this permit. In addition, submit a letter indicating who should be contacted to schedule the inspection.

DOCUMENTATION: The applicant shall provide a receipt showing that the inspection fee has been paid along with updated contact information [PDS, PCC]. **TIMING:** Prior to any occupancy, final grading release, or use of the premises in reliance of this permit.

MONITORING: The [PDS, Zoning Counter] shall process an invoice and collect the fee for the Use Permit Compliance Inspection Fee. PDS will schedule an inspection within one year from the date that occupancy or use of the site was established.

28. PLN#3 – SITE PLAN IMPLEMENTATION [PDS, BI] [UO] [DPR, TC, PP].

INTENT: In order to comply with the approved project design indicated on the approved plot plan, the project shall be constructed as indicated on the approved building and construction plans. **DESCRIPTION OF REQUIREMENT:** The site shall conform to the approved plot plan and the building plans. This includes, but is not limited to: improving all parking areas and driveways, installing all required design features, installing all required lighting in proper locations, painting all structures with the approved colors, properly screening trash enclosures, installing required and approved signage in approved locations, including “No Parking” signage along the project frontage on the north and south sides of Chase Avenue (to the satisfaction of the Director of the Department of Public Works), installing required barrier net system on sports field, and removing all temporary construction facilities from the site. Additionally, the applicant shall work with the owner of the adjacent commercial shopping center to install “No School Parking or Drop-Off” signage within the shopping center parking lot. The applicant shall provide evidence of communication with the property owner to the County and maintain all agreed upon signage. **DOCUMENTATION:** The applicant shall ensure that the site conforms to the approved plot plan and building plans. **TIMING:** Prior to any occupancy, final grading release, or use of the premises in reliance of this permit, the site shall conform to the approved plans. **MONITORING:** The [PDS, Building Inspector] and DPR [TC, PP] shall inspect the site for compliance with the approved Building Plans.

29. LNDSCP#2–CERTIFICATION OF INSTALLATION

INTENT: In order to provide adequate Landscaping that provides screening, and to comply with the [COSD Water Efficient Landscape Design Manual](#), the [COSD Water Conservation in Landscaping Ordinance](#), the [COSD Off-Street Parking Design Manual](#), and the COSD Grading ordinance, all landscaping shall be installed. **DESCRIPTION OF REQUIREMENT:** All of the landscaping shall be installed pursuant to the approved Landscape Documentation Package. This does not supersede any erosion control plantings that may be applied pursuant to [Section 87.417 and 87.418 of the County Grading Ordinance](#). These areas may be overlapping, but any requirements of a grading plan shall be complied with separately. The installation of the landscaping can be phased pursuant to construction of specific buildings or phases to the satisfaction of the [PDS, LA, PCC] [DPR, TC, PP]. **DOCUMENTATION:** The applicant shall submit to the [PDS, LA, PCC], a Landscape Certificate of Completion from the project California licensed

Landscape Architect, Architect, or Civil Engineer, that all landscaping has been installed as shown on the approved Landscape Documentation Package. The applicant shall prepare the Landscape Certificate of Completion using the Landscape Certificate of Completion Checklist, PDS Form #406. **TIMING:** Prior to any occupancy, final grading release, or use of the premises in reliance of this permit, the landscaping shall be installed. **MONITORING:** The [PDS, LA] shall verify the landscape installation upon notification of occupancy or use of the property, and notify the [PDS, PCC] [DPR, TC, PP] of compliance with the approved Landscape Documentation Package.

30. CULT#2 CULTURAL RESOURCES REPORT [PDS, FEE X2]

INTENT: In order to ensure that the Archaeological Monitoring occurred during the earth-disturbing activities, a final report shall be prepared. **DESCRIPTION OF REQUIREMENT:** A final Archaeological Monitoring and Data Recovery Report that documents the results, analysis, and conclusions of all phases of the Archaeological Monitoring Program shall be prepared. The report shall include the following items:

- a. DPR Primary and Archaeological Site forms.
- b. Daily Monitoring Logs
- c. Evidence that the disposition of all cultural materials collected during the survey, evaluation, and archaeological monitoring program have been completed as follows:
 - (1) All prehistoric cultural materials shall be curated at a San Diego curation facility or a culturally affiliated Tribal curation facility that meets federal standards per 36 CFR Part 79, and, therefore, would be professionally curated and made available to other archaeologists/researchers for further study. The collections and associated records, including title, shall be transferred to the San Diego curation facility or culturally affiliated Tribal curation facility and shall be accompanied by payment of the fees necessary for permanent curation. Evidence shall be in the form of a letter from the curation facility stating that the prehistoric archaeological materials have been received and that all fees have been paid.

or

Evidence that all prehistoric materials collected during the archaeological monitoring program have been returned to a Native American group of appropriate tribal affinity. Evidence shall be in the form of a letter from the Native American tribe to whom the cultural resources have been repatriated identifying that the archaeological materials have been received.

- (2) Historic materials shall be curated at a San Diego curation facility as described above and shall not be curated at a Tribal curation facility or repatriated. The collections and associated records, including title, shall be transferred to the San Diego curation facility and shall be accompanied by payment of the fees necessary for permanent curation. Evidence shall be in the form of a letter from the curation facility stating that the historic materials have been received and that all fees have been paid.
- d. If no cultural resources are discovered, a Negative Monitoring Report must be submitted stating that the grading monitoring activities have been completed. Grading Monitoring Logs must be submitted with the negative monitoring report.

DOCUMENTATION: The applicant's archaeologist shall prepare the final report and submit it to the [PDS, PPD] for approval. Once approved, a final copy of the report shall be submitted to the South Coastal Information Center (SCIC) and any culturally-affiliated tribe who requests a copy. **TIMING:** Prior to any occupancy, final grading release, or use of the premises in reliance of this permit, the final report shall be prepared. **MONITORING:** The [PDS, PPD] shall review the final report for compliance this condition and the report format guidelines. Upon acceptance of the report, [PDS, PPD] shall inform [PDS, LDR] and [DPW, PDCI], that the requirement is complete, and the bond amount can be relinquished. If the monitoring was bonded separately, then [PDS, PPD] shall inform [PDS or DPW FISCAL] to release the bond back to the applicant.

31. NOISE#3 - NOISE CONTROL DESIGN MEASURES

INTENT: In order to comply with the [County of San Diego Noise Ordinance 36.404](#) as evaluated in the [County of San Diego Noise Guidelines for Determining Significance](#), the following design measures shall be verified that they are constructed. **DESCRIPTION OF REQUIREMENT:** The following noise control design measure(s) shall be constructed pursuant to the approved building plans: dual-pane windows with a minimum sound transmission class (STC) rating of 26 and mechanical ventilation as identified in the Noise Study prepared by Jeremy Loudon (August 3, 2017). **TIMING:** Prior to any occupancy, final grading release, or use of the premises in reliance of this permit for Major Use Permit, PDS2015-MUP-15-027, the noise control measure shall be installed and operational. **MONITORING:** The [PDS, B] shall verify that the noise control measures above have been constructed pursuant to the approved building plans and this permit's conditions.

32. NOISE#4 - NOISE CONTROL DESIGN MEASURES (MECHANICAL)

INTENT: In order to reduce the impacts of the exterior sound levels from the project site on the adjacent parcels and to comply with the County of San Diego Noise Ordinance 36.404 and the County of San Diego Noise Guidelines for Determining Significance, the following design measures shall be verified that they are constructed. **DESCRIPTION OF REQUIREMENT:** The following noise control design measure(s) shall be constructed pursuant to the approved building plans: HVAC units associated with the school building shall incorporate visual screening by a parapet wall or barrier of equal or greater height in relation to the HVAC unit elevation for noise screening purposes for the southern property line. **TIMING:** Prior to any occupancy, final grading release, or use of the premises in reliance of this permit for the school building, the noise control measure shall be installed and operational. **MONITORING:** The [PDS, B] shall verify that the noise control measures above have been constructed pursuant to the approved building plans and this permit's conditions.

33. PLN#4 – SITE CONFORMANCE [PDS, PCO] [OG] [DPR, TC, PP].

INTENT: In order to comply with Zoning Ordinance Section 7703, the site shall substantially comply with the approved plot plans and all deviations thereof, specific conditions and approved building plans. **DESCRIPTION OF REQUIREMENT:** The project shall conform to the approved landscape plan(s), building plans, and plot plan(s). This includes but is not limited to maintaining the following: ~~all parking and driveways areas, watering all landscaping at all times, painting all necessary aesthetics design features, and all lighting wall/fencing and required signage.~~

1. School shall begin at 8:30am and end at 3:45pm Monday through Friday.
 - a. Should Valhalla High School change its start time to 8:30am, Liberty Charter High School is permitted to revise its start time upon approval of a minor deviation to this Major Use Permit to avoid conflicting traffic patterns during morning drop off.
2. School administrators shall supervise drop-off and pick-up in designated areas.
3. School shall maintain and enforce "no parking" and "no drop-off" prohibitions along Chase Avenue where applicable.
4. Maintain enough space within the parking lot, approximately 700 linear feet, to queue 28 cars on-site during drop-off and pick-up hours.
5. All parents picking up students shall wait on site in appropriate parking areas.
6. Sports practices and games and other school sponsored and related after school activities on the sports field occurring after school hours and on weekends shall end by 7:30pm. Sports practices, games, and other school sponsored and related after school activities in the gymnasium or classrooms occurring after school hours and on weekends shall end by 10:30pm. Weekend morning noise amplified events shall be limited to 8 per season and the noise amplification system shut off by 8:00pm for all events on campus.
7. Sports field lighting shall be shut off by 8:00pm when in use.
8. Maintain required barrier net system behind northern and southern goal posts.
9. Maintain all parking and driveways areas.
10. Always maintain and water all landscaping.
11. Paint all necessary aesthetics design features.
12. Perimeter gates shall be locked when school is not in use.
13. Maintain all approved lighting, fencing and required signage.

Failure to conform to the approved plot plan(s); is an unlawful use of the land and will result in enforcement action pursuant to Zoning Ordinance Section 7703.

DOCUMENTATION: The property owner and permittee shall conform to the approved plot plan. If the permittee or property owner chooses to change the site design in any way, they must obtain approval from the County for a Minor Deviation or a Modification pursuant to the County of San Diego Zoning Ordinance. **TIMING:** Upon establishment of the use, this condition shall apply for the duration of the term of this permit.

MONITORING: The [PDS, Code Enforcement Division] is responsible for enforcement of this permit.

34. NOISE#5 – ON-GOING SOUND LEVEL COMPLIANCE: [PDS, CODES] [OG]

INTENT: In order to comply with the applicable sections of Title 3, Division 6, Chapter 4 (County of San Diego Noise Ordinance), the site shall comply with the requirements of this condition. **DESCRIPTION OF REQUIREMENT:** The project shall conform to the following requirements:

- a. Major Use Permit associated activities shall comply with the one-hour average sound level limit property line requirement pursuant to the County Noise Ordinance, Section 36.404.
- b. The operations associated with this Major Use Permit shall conform to the daytime and nighttime sound level limits for uses as zoned pursuant to Section 36.404.

DOCUMENTATION: The property owner(s) and applicant shall conform to the ongoing requirements of this condition. Failure to conform to this condition may result in disturbing, excessive or offensive noise interfering with a person’s right to enjoy life and property and is detrimental to the public health and safety pursuant to the applicable sections of Chapter 4. **TIMING:** Upon establishment of the use, this condition shall apply for the duration of the term of this permit. **MONITORING:** The [PDS, CODES] is responsible for enforcement of this permit.

ADOPTION STATEMENT: This Mitigated Negative Declaration was adopted, and above California Environmental Quality Act findings made by the:

on _____

Darin Neufeld, Chief
Project Planning Division

MW:DN:dr



County of San Diego

MARK WARDLAW
DIRECTOR

PLANNING & DEVELOPMENT SERVICES
5510 OVERLAND AVENUE, SUITE 310, SAN DIEGO, CA 92123
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PHONE (858) 694-2962 • FAX (858) 694-2555

KATHLEEN FLANNERY
ASSISTANT DIRECTOR

~~December 12, 2019~~ September 18, 2020

CEQA Initial Study - Environmental Checklist Form (Based on the State CEQA Guidelines, Appendix G)

1. Title; Project Number(s); Environmental Log Number:

Liberty Charter High School; PDS2015-MUP-15-027; PDS2015-ER-15-14-010

2. Lead agency name and address:

County of San Diego, Planning & Development Services
5510 Overland Avenue, Suite 110
San Diego, CA 92123-1239

3. a. Contact: Denise Russell, Project Manager

b. Phone number: (858) 694-2019

c. E-mail: denise.russell@sdcounty.ca.gov.

4. Project location:

1530 Jamacha Road, El Cajon, Valle de Oro Community Planning Area, Unincorporated
San Diego County (APN# 498-330-39-00)

Thomas Guide Coordinates: Page 1272, Grid A/2

5. Project Applicant name and address:

Literacy First Charter Schools, Inc.
1012 East Bradley Avenue
El Cajon, CA 92020

6. General Plan

Community Plan:

Valle De Oro

Land Use Designation:

Semi-Rural 0.5 (SR-0.5)

Density:

1 du/0.5 gross acre(s)

Floor Area Ratio (FAR)

N/A

7. Zoning
- | | |
|--------------------------|------------------------|
| Use Regulation: | Rural Residential (RR) |
| Minimum Lot Size: | 0.5 acre(s) |
| Special Area Regulation: | N/A |

8. Project Description:

The project is a Major Use Permit for a charter high school for grades nine through twelve, operated by Literacy First Charter Schools (LFCS). LFCS operates under a charter issued by the County Department of Education, which requires that they operate within the boundaries of the Grossmont Union High School District. The existing Liberty Charter High School is located approximately eight miles west of the proposed site, at 8425 Palm Street in Lemon Grove. LFCS currently leases the former Palm Middle School campus from the Lemon Grove School District, and the high school will be relocated to the proposed location once constructed to better serve the local high school population in El Cajon and Rancho San Diego. At full capacity, the proposed school would serve 450 students and have 33 faculty and staff. The school year would take place from August through June, and school hours would be from 8:00 a.m. to 3:30 p.m. The school would include a 48,000 square-foot, two-story building that would house 22 classrooms, the administrative office, and a gymnasium. Other on-site amenities would include 161 parking spaces, an outdoor common area and patios, and a sports field located in the eastern portion of the site. Access to the site would be provided by a one-way circulation driveway (two driveways total) connecting to Chase Avenue. Approximately 700 linear feet of drop-off/pick-up area would be provided, accommodating 28 vehicles queuing on-site at any given time. A total of 161 parking spaces would be provided including three accessible parking spaces and one van accessible space. A minimum of 45 bicycle spaces would also be provided. Grading would consist of 23,500 cubic yards of balanced cut and fill.

The project site is 7.7 acres and is located at the intersection of Chase Avenue and Jamacha Road in the Valle de Oro Community Planning Area, within unincorporated San Diego County. The site is subject to the Semi-Rural General Plan Regional Category, Semi-Rural 0.5 Land Use Designation (SR-0.5), and to the Rural Residential (RR) Zoning Regulations. Schools are classified as Major Impact Services and Utilities under the Zoning Ordinance, and a Major Use Permit is required pursuant to the RR Zoning Regulations.

The site is currently undeveloped. The project would be served by sewer and imported water from the Otay Water District. Fire protection and emergency services would be provided by the San Miguel Fire Protection District. No extension of sewer or water utilities will be required by the project.

The proposed project includes construction of at least a 90-Kilowatt (KW) solar/photovoltaic system and would install low flow water fixtures throughout the development. Two level II electric vehicle charging stations would be installed within the common parking area, and dedicated parking would be included for vanpool/clean vehicle and carpool only.

CEQA Guidelines Section 15126.4(c)

As discussed in further detail under Section VIII. Greenhouse Gas Emissions, the County of San Diego's Climate Action Plan (CAP) has been under litigation since December 2018. Because the County was granted a writ ordering the approval of the CAP and its EIR to be set aside, and enjoining reliance on the County's CAP mitigation measure M-GHG-1, the CAP and it's EIR were still in place during the appeal while the proposed project was circulated for public disclosure. However, given the legal instability concerning the County's CAP during review of the proposed project, the Greenhouse Gas analysis prepared for the proposed project did not rely on the CAP to streamline the Project's environmental analysis under CEQA Guidelines Section 15183.5. Rather, the proposed project's significant determination used the criteria informed by the CEQA Guidelines Section 15064.4 and mitigation strategies informed by CEQA Guidelines Section 15126.4(c) that are independent of the CAP. The proposed project was found to have a less than significant impact to Greenhouse Gas emissions.

Due to the change in circumstance of the County's CAP, this Initial Study checklist has been updated in red-colored tracked changes to disclose additional details of how the proposed project would result in a less than significant impact to Greenhouse Gas Emissions. In addition, language has been added to clarify the road improvements from the traffic analysis that were circulated for public disclosure. Pursuant to CEQA Guidelines Section §15073.5, if a project would meet one of the four circumstances listed below, the project would not be required to recirculate the Negative Declaration:

CEQA Guidelines Section §15073.5

(c) Recirculation is not required under the following circumstances:

(1) Mitigation measures are replaced with equal or more effective measures pursuant to Section 5074.1.

- The updates incorporated into the Initial Study Checklist include further discussion of design features, clarification of road improvements, as well as required local, state and federal regulations. No mitigation measures have been updated. Therefore, the proposed project does not meet this circumstance.

(2) New project revisions are added in response to written or verbal comments on the project's effects identified in the proposed negative declaration which are new avoidable significant effects.

- No updates made within this Initial Study Checklist were a result of written or verbal comments. Therefore, the proposed project does not meet this circumstance.

(3) Measures or conditions of project approval are added after circulation of the negative declaration which are not required by CEQA, which do not create new significant environmental effects and are not necessary to mitigate an avoidable significant effect.

- No new measures or conditions of project approval were added after circulation of the negative declaration. Therefore, the proposed project does not meet this circumstance.

(4) New information is added to the negative declaration which merely clarifies, amplifies, or makes insignificant modification to the negative declaration.

- The updates that have been made to the Initial Study Checklist are further clarification on the proposed project design features, road improvements, as well as required local, state and federal regulations for which the proposed project must comply. Therefore, the proposed project meets this circumstance.

The updates of this Initial Study checklist are mere clarifications and have been found to meet CEQA Guidelines Section §15073.5, circumstance (4). Therefore, no recirculation of the Initial Study Checklist is required.

9. Surrounding land uses and setting (Briefly describe the project’s surroundings):

Land uses surrounding the project site include residential and commercial uses. The topography of the project site and adjacent land is relatively flat with gentle slopes. The site is bound by Chase Avenue to the north and a private driveway to the south. A commercial shopping center is directly east of the site. Interstate 8 is approximately three miles north of the site, and State Route 54/Jamacha Road is directly east on the other side of the commercial shopping center.

10. Other public agencies whose approval is required (e.g., permits, financing approval, or participation agreement):

Permit Type/Action	Agency
Landscape Plans	County of San Diego
Major Use Permit	County of San Diego
Grading Permit	County of San Diego
Improvement Plans	County of San Diego
Annexation to a City or Special District	Local Agency Formation Commission (LAFCO)
National Pollutant Discharge Elimination System (NPDES) Permit	RWQCB
General Construction Storm Water Permit	RWQCB
Waste Discharge Requirements Permit	RWQCB
Water District Approval	Otay Water District
Sewer District Approval	Otay Water District
Fire District Approval	San Miguel Fire Protection District

11. Have California Native American tribes traditionally and culturally affiliated with the project area requested consultation pursuant to Public Resources Code §21080.3.1? If so, has consultation begun?

YES

NO

Note: Conducting consultation early in the CEQA process allows tribal governments, public lead agencies, and project proponents to discuss the level of environmental review, identify and address potential adverse impacts to tribal cultural resources, and to reduce the potential for delay and conflict in the environmental review process (see Public Resources Code §21083.3.2). Information is also available from the Native American Heritage Commission's Sacred Lands File per Public Resources Code §5097.96 and the California Historical Resources Information System administered by the California Office of Historic Preservation. Please also note that Public Resources Code §21082.3(e) contains provisions specific to confidentiality.

ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED: The environmental factors checked below would be potentially affected by this project and involve at least one impact that is a “Potentially Significant Impact” or a “Less Than Significant With Mitigation Incorporated,” as indicated by the checklist on the following pages.

- | | | |
|--|--|--|
| <input type="checkbox"/> <u>Aesthetics</u> | <input type="checkbox"/> <u>Agriculture and Forest Resources</u> | <input checked="" type="checkbox"/> <u>Air Quality</u> |
| <input checked="" type="checkbox"/> <u>Biological Resources</u> | <input type="checkbox"/> <u>Cultural Resources</u> | <input type="checkbox"/> <u>Energy Use</u> |
| <input type="checkbox"/> <u>Geology & Soils</u> | <input type="checkbox"/> <u>Greenhouse Gas Emissions</u> | <input type="checkbox"/> <u>Hazards & Haz. Materials</u> |
| <input type="checkbox"/> <u>Hydrology & Water Quality</u> | <input type="checkbox"/> <u>Land Use & Planning</u> | <input type="checkbox"/> <u>Mineral Resources</u> |
| <input type="checkbox"/> <u>Noise</u> | <input type="checkbox"/> <u>Population & Housing</u> | <input type="checkbox"/> <u>Public Services</u> |
| <input type="checkbox"/> <u>Recreation</u> | <input checked="" type="checkbox"/> <u>Transportation</u> | <input type="checkbox"/> <u>Tribal Cultural Resources</u> |
| <input checked="" type="checkbox"/> <u>Utilities & Service Systems</u> | <input type="checkbox"/> <u>Wildfire</u> | <input type="checkbox"/> <u>Mandatory Findings of Significance</u> |

DETERMINATION: (To be completed by the Lead Agency)
On the basis of this initial evaluation:

- On the basis of this Initial Study, Planning & Development Services finds that the proposed project COULD NOT have a significant effect on the environment, and a NEGATIVE DECLARATION will be prepared.
- On the basis of this Initial Study, Planning & Development Services finds that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because revisions in the project have been made by or agreed to by the project proponent. A MITIGATED NEGATIVE DECLARATION will be prepared.
- On the basis of this Initial Study, Planning & Development Services finds that the proposed project MAY have a significant effect on the environment, and an ENVIRONMENTAL IMPACT REPORT is required.

~~December 12, 2019~~ September 18, 2020

Signature

Date

Denise Russell
Printed Name

Land Use/Environmental Planner
Title

INSTRUCTIONS ON EVALUATION OF ENVIRONMENTAL IMPACTS

1. A brief explanation is required for all answers except “No Impact” answers that are adequately supported by the information sources a lead agency cites in the parentheses following each question. A “No Impact” answer is adequately supported if the referenced information sources show that the impact simply does not apply to projects like the one involved (e.g., the project falls outside a fault rupture zone). A “No Impact” answer should be explained where it is based on project-specific factors as well as general standards (e.g., the project will not expose sensitive receptors to pollutants, based on a project-specific screening analysis).
2. All answers must take account of the whole action involved, including off-site as well as on-site, cumulative as well as project-level, indirect as well as direct, and construction as well as operational impacts.
3. Once the lead agency has determined that a particular physical impact may occur, then the checklist answers must indicate whether the impact is potentially significant, Less Than Significant With Mitigation Incorporated, or less than significant. “Potentially Significant Impact” is appropriate if there is substantial evidence that an effect may be significant. If there are one or more “Potentially Significant Impact” entries when the determination is made, an EIR is required.
4. “Less Than Significant With Mitigation Incorporated” applies where the incorporation of mitigation measures has reduced an effect from “Potentially Significant Impact” to a “Less Than Significant Impact.” The lead agency must describe the mitigation measures, and briefly explain how they reduce the effect to a less than significant level.
5. Earlier analyses may be used where, pursuant to the tiering, program EIR, or other CEQA process, an effect has been adequately analyzed in an earlier EIR or negative declaration. Section 15063(c)(3)(D). In this case, a brief discussion should identify the following:
 - a) Earlier Analysis Used. Identify and state where they are available for review.
 - b) Impacts Adequately Addressed. Identify which effects from the above checklist were within the scope of and adequately analyzed in an earlier document pursuant to applicable legal standards, and state whether such effects were addressed by mitigation measures based on the earlier analysis.
 - c) Mitigation Measures. For effects that are “Less Than Significant With Mitigation Incorporated,” describe the mitigation measures that were incorporated or refined from the earlier document and the extent to which they address site-specific conditions for the project.
6. Lead agencies are encouraged to incorporate into the checklist references to information sources for potential impacts (e.g., general plans, zoning ordinances). Reference to a previously prepared or outside document should, where appropriate, include a reference to the page or pages where the statement is substantiated.
7. The explanation of each issue should identify:
 - a) The significance criteria or threshold, if any, used to evaluate each question; and
 - b) The mitigation measure identified, if any, to reduce the impact to less than significance

I. AESTHETICS.

Except as provided in Public Resources Code Section 21099, would the project:

a) Have a substantial adverse effect on a scenic vista?

- Potentially Significant Impact
- Less Than Significant With Mitigation Incorporated
- Less than Significant Impact
- No Impact

A vista is a view from a particular location or composite views along a roadway or trail. Scenic vistas often refer to views of natural lands but may also be compositions of natural and developed areas, or even entirely of developed and unnatural areas, such as a scenic vista of a rural town and surrounding agricultural lands. What is scenic to one person may not be scenic to another, so the assessment of what constitutes a scenic vista must consider the perceptions of a variety of viewer groups.

The items that can be seen within a vista are visual resources. Adverse impacts to individual visual resources or the addition of structures or developed areas may or may not adversely affect the vista. Determining the level of impact to a scenic vista requires analyzing the changes to the vista as a whole and to individual visual resources.

No Impact

The project site is located near the intersection of Jamacha Road and Chase Avenue in the Valle de Oro planning area. Based on a site visit by County staff on December 4, 2015 the proposed project is not located near or within, or visible from, a scenic vista and will not substantially change the composition of an existing scenic vista in a way that would adversely alter the visual quality or character of the view. Therefore, the proposed project would not have an adverse effect on a scenic vista.

The project would not result in cumulative impacts on a scenic vista because the proposed project viewshed and past, present and future projects within that viewshed were evaluated to determine their cumulative effects. Section XXI. Mandatory Findings of Significance includes a comprehensive list of the projects considered. Those projects listed in Section XXI would not contribute to a cumulative impact because they do not have impacts related to aesthetics, include landscaping, or are not located within the proposed project’s scenic vista. Therefore, the project would not result in adverse project or cumulative impacts on a scenic vista.

b) Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway?

- Potentially Significant Impact
- Less Than Significant With Mitigation Incorporated
- Less than Significant Impact
- No Impact

State scenic highways refer to those highways that are officially designated by the California Department of Transportation (Caltrans) as scenic ([Caltrans - California Scenic Highway Program](#)). Generally, the area defined within a State scenic highway is the land adjacent to and

visible from the vehicular right-of-way. The dimension of a scenic highway is usually identified using a motorist’s line of vision, but a reasonable boundary is selected when the view extends to the distant horizon. The scenic highway corridor extends to the visual limits of the landscape abutting the scenic highway.

No Impact

Based on a site visit completed by staff on December 4, 2015, the proposed project is not located near or visible within the composite viewshed of a State scenic highway and would not damage or remove visual resources within a State scenic highway. The project site is currently undeveloped. Vegetation includes Non-native grassland and non-native vegetation, as well as developed and disturbed lands. The project site is infill development and is adjacent to Chase Avenue and Jamacha Road and is surrounded by residential and commercial development. Therefore, the proposed project would not have any substantial adverse effect on a scenic resource within a State scenic highway.

The project would not result in cumulative impacts on a scenic vista because the proposed project viewshed and past, present and future projects within that viewshed were evaluated to determine their cumulative effects. Section XXI. Mandatory Findings of Significance includes a comprehensive list of the projects considered. Those projects listed in Section XVII are located within the scenic vista’s viewshed and would not contribute to a cumulative impact because they are not located within a state scenic highway. Therefore, the project would not result in any adverse project or cumulative level effect on a scenic resource within a State scenic highway.

c) Substantially degrade the existing visual character or quality of public views of the site and its surroundings? (Public views are those that are experienced from publicly accessible vantage point). If the project is in an urbanized area, would the project conflict with applicable zoning and other regulations governing scenic quality?

- | | |
|---|--|
| <input type="checkbox"/> Potentially Significant Impact | <input checked="" type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input type="checkbox"/> No Impact |

Visual character is the objective composition of the visible landscape within a viewshed. Visual character is based on the organization of the pattern elements line, form, color, and texture. Visual character is commonly discussed in terms of dominance, scale, diversity and continuity. Visual quality is the viewer’s perception of the visual environment and varies based on exposure, sensitivity and expectation of the viewers.

Less than Significant Impact

The project site is within an urbanized area of the Valle de Oro community (El Cajon), and is located at the intersection of Chase Avenue and Jamacha Road. The existing visual character and quality of the project site is undeveloped, and the surrounding area can be characterized as highly developed with land use types such as single-family residential and commercial uses. The project site is relatively flat, and the surrounding area is relatively flat with gentle sloping topography. Viewer groups of the Project site include those traveling along Chase Avenue and Jamacha Road. The viewer exposure is either limited due to travel speed or is extended for vehicles stopped at the traffic light of Chase Avenue and Jamacha Road.

The proposed Project would not detract from or contrast with the existing visual character and/or quality of the surrounding area for the following reasons: the height, setbacks, and design of the proposed school are consistent with the height designator, setbacks and design of the surrounding area; and landscaping has been incorporated within the project along Chase Avenue and Jamacha Road for screening purposes from public roads. The location, size, and design of the proposed use would be compatible with adjacent uses due to the following reasons: the proposed school is similar to surrounding commercial development (east, northeast) and is smaller in scale than Valhalla High School (approximately 0.5 mile southeast). Homes in the area include single-family and estate residential. Viewer exposure to the Project would not be a significant effect since the Project is proposed in a highly developed area and has been designed in a way to be compatible with the surrounding use types. Therefore, the proposed project would not result in a substantial effect on the existing visual character or quality of the site and its surroundings.

The project would not result in cumulative impacts on visual character or quality because the entire existing viewshed and a list of past, present and future projects within that viewshed were evaluated. Section XXI. Mandatory Findings of Significance includes a comprehensive list of the projects considered. Those projects listed in Section XXI are located within the viewshed surrounding the project and would not contribute to a cumulative impact because the project would be visually integrated into the surroundings in an unobtrusive manner. Therefore, the project would not result in any adverse project or cumulative level effect on visual character or quality on-site or in the surrounding area.

d) Create a new source of substantial light or glare, which would adversely affect day or nighttime views in the area?

- | | |
|---|--|
| <input type="checkbox"/> Potentially Significant Impact | <input checked="" type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input type="checkbox"/> No Impact |

Less Than Significant Impact

The proposed project would use outdoor lighting and is not located within Zone A as identified by the San Diego County Light Pollution Code. It would not adversely affect nighttime views or astronomical observations, because the project would conform to the Light Pollution Code (Section 51.201-51.209), including the lamp type and shielding requirements per fixture and hours of operation limitations for outdoor lighting and searchlights, as detailed in a Photometric Study prepared for the project, dated August 2017, and accepted by the County.

In addition, the proposed project would control outdoor lighting and sources of glare in the following ways:

1. The project would not install outdoor lighting that directly illuminates neighboring properties.
2. The project would not install outdoor lighting that would cast a direct beam angle towards a potential observer, such as a motorists, cyclist or pedestrian.

- 3. The project would not install outdoor lighting for vertical surfaces such as buildings, landscaping, or signs in a manner that would result in useful light or spill light being cast beyond the boundaries of intended area to be lit.
- 4. The project would not install any highly reflective surfaces such as glare-producing glass or high-gloss surface color that would be visible along roadways, pedestrian walkways, or in the line of sight of adjacent properties.

The project would not contribute to significant cumulative impacts on day or nighttime views because the project would conform to the Light Pollution Code. The Code was developed by the San Diego County Planning & Development Services and Department of Public Works in cooperation with lighting engineers, astronomers, land use planners from San Diego Gas and Electric, Palomar and Mount Laguna observatories, and local community planning and sponsor groups to effectively address and minimize the impact of new sources light pollution on nighttime views. The standards in the Code are the result of this collaborative effort and establish an acceptable level for new lighting. Compliance with the Code is required prior to issuance of any building permit for any project. Mandatory compliance for all new building permits ensures that this project in combination with all past, present and future projects would not contribute to a cumulatively considerable impact. Therefore, compliance with the Code ensures that the project would not create a significant new source of substantial light or glare, which would adversely affect daytime or nighttime views in the area, on a project or cumulative level.

In addition, the project’s outdoor lighting would be controlled through the Major Use Permit, which further limits outdoor lighting through strict controls. Therefore, compliance with the Code, in combination with the outdoor lighting and glare controls listed above ensures that the project would not create a significant new source of substantial light or glare.

II. AGRICULTURE AND FORESTRY RESOURCES -- Would the project:

- a) Convert Prime Farmland, Unique Farmland, or Farmland of Statewide or Local Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, or other agricultural resources, to non-agricultural use?

- | | |
|---|---|
| <input type="checkbox"/> Potentially Significant Impact | <input type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input checked="" type="checkbox"/> No Impact |

No Impact

The project site does not contain any agricultural resources, lands designated as Prime Farmland, Unique Farmland, or Farmland of Statewide or Local Importance as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency. Therefore, no agricultural resources including Prime Farmland, Unique Farmland, or Farmland of Statewide or Local Importance would be converted to a non-agricultural use.

- b) Conflict with existing zoning for agricultural use, or a Williamson Act contract?

- Potentially Significant Impact
- Less Than Significant With Mitigation Incorporated
- Less than Significant Impact
- No Impact

No Impact

The project site is zoned Rural Residential (RR), which is not considered to be an agricultural zone. In addition, the project site’s land is not under a Williamson Act Contract. Therefore, the project does not conflict with existing zoning for agricultural use, or a Williamson Act Contract.

c) Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code section 12220(g)), timberland (as defined by Public Resources Code section 4526), or timberland zoned Timberland Production (as defined by Government Code section 51104(g))?

- Potentially Significant Impact
- Less Than Significant With Mitigation Incorporated
- Less than Significant Impact
- No Impact

No Impact

The project site, including offsite improvements, does not contain forest lands or timberland. The County of San Diego does not have any existing Timberland Production Zones. In addition, the project is consistent with existing zoning and a rezone of the property is not proposed. Therefore, project implementation would not conflict with existing zoning for, or cause rezoning of, forest land, timberland or timberland production zones.

d) Result in the loss of forest land or conversion of forest land to non-forest use?

- Potentially Significant Impact
- Less Than Significant With Mitigation Incorporated
- Less than Significant Impact
- No Impact

No Impact

The project site including any offsite improvements do not contain any forest lands as defined in Public Resources Code section 12220(g), therefore project implementation would not result in the loss or conversion of forest land to a non-forest use. In addition, the project is not located in the vicinity of offsite forest resources.

e) Involve other changes in the existing environment, which, due to their location or nature, could result in conversion of Farmland to non-agricultural use, or conversion of forest land to non-forest use?

- Potentially Significant Impact
- Less Than Significant With Mitigation Incorporated
- Less than Significant Impact
- No Impact

No Impact

The project site and surrounding area within a radius of 1 mile does not contain any active agricultural operations or lands designated as Prime Farmland, Unique Farmland, or Farmland of Statewide or Local Importance as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency. Therefore, no Prime Farmland, Unique Farmland, Farmland of Statewide or Local Importance, or active agricultural operations would be converted to a non-agricultural use. In addition, and as stated above, the proposed project does not contain any forest lands as defined in Public Resources Code section 12220(g), therefore project implementation would not result in the loss or conversion of forest land to a non-forest use

III. AIR QUALITY -- Where available, the significance criteria established by the applicable air quality management or air pollution control district may be relied upon to make the following determinations. Would the project:

a) Conflict with or obstruct implementation of the San Diego Regional Air Quality Strategy (RAQS) or applicable portions of the State Implementation Plan (SIP)?

- Potentially Significant Impact
- Less than Significant Impact
- Less Than Significant With Mitigation Incorporated
- No Impact

Less Than Significant Impact

An Air Quality Assessment was prepared by Ldn Consulting, Inc in November 2019. The General Plan designates the Project site as semi-rural residential (SR-0.5) with a regional category "semi-rural." The site is zoned RR, Rural Residential. The Project, which would develop a new high school serving 450 students and 33 staff, would be consistent with the General Plan land use designation with a major use permit. Because the project is allowed under the General Plan land use designation, which used San Diego Association of Governments (SANDAG) growth projections, it is consistent with the regional air quality standards (RAQS) and State Implementation Plan (SIP). Thus, the project would not conflict with either the RAQS or the SIP. Further, as analyzed and discussed in the Air Quality Assessment, the construction and operational emissions from the project are anticipated to be below established County screening level thresholds (SLTs) and would not violate any ambient air quality standards. This impact would be less than significant.

b) Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is in non-attainment under an applicable federal or state ambient air quality standard?

- Potentially Significant Impact
- Less than Significant Impact
- Less Than Significant With Mitigation Incorporated
- No Impact

Discussion/Explanation:

San Diego County is presently in non-attainment for the 1-hour concentrations under the California Ambient Air Quality Standard (CAAQS) for Ozone (O₃). San Diego County is also presently in non-attainment for the annual geometric mean and for the 24-hour concentrations of Particulate Matter less than or equal to 10 microns (PM₁₀) under the CAAQS. O₃ is formed when volatile organic compounds (VOCs) and nitrogen oxides (NO_x) react in the presence of sunlight. VOC sources include any source that burns fuels (e.g., gasoline, natural gas, wood, oil); solvents; petroleum processing and storage; and pesticides. Sources of PM₁₀ in both urban and rural areas include motor vehicles, wood burning stoves and fireplaces, dust from construction, landfills, agriculture, wildfires, brush/waste burning, and industrial sources of windblown dust from open lands.

Less Than Significant Impact

The project proposes the construction of a new high school in a two-story, 48,000 square foot building with balanced grading onsite. For the purposes of the Air Quality Assessment, project construction was assumed to start in January 2020 and is project to end December 2020. Additional discussion of assumptions made for the project construction analysis within the Air Quality Assessment. Emissions from project construction activities would be temporary and localized and would be required to comply with San Diego Air Pollution Control District (SDAPCD) Rules 55 and 67, and the County of San Diego Grading Ordinance. Based on the analysis provided in the Air Quality Analysis, project construction activities would not result in emissions in excess of the County's SLTs.

During project operations, the proposed project would generate approximately 854 average daily trips. Daily emissions of criteria pollutants associated with these motor vehicles, as well as emissions from operational area and energy sources, were estimated in the Air Quality Assessment. The project would generate daily emissions at levels below the County SLTs during operations.

The project would contribute PM₁₀, PM_{2.5}, NO_x, and VOC emissions from construction activities; however, the incremental increase would not exceed the established SLTs, and would be subject to SDAPCD Rules 55 and 67, and the County Grading Ordinance. These regulations require the implementation of fugitive dust control measures and VOC limits for all architectural coatings. The project would generate emissions of PM₁₀, PM_{2.5}, and NO_x during operations, primarily from mobile sources, and VOCs from area and mobile sources. Operational emissions would not be anticipated to exceed the County's SLTs. Further, as discussed in (a), project operations would be consistent with the RAQs and SIP.

Emissions generated during project construction activities and operations would be less than significant and would not result in a cumulative considerable net increase in criteria pollutants for which the region is in non-attainment.

c) Expose sensitive receptors to substantial pollutant concentrations?

- | | |
|--|---|
| <input type="checkbox"/> Potentially Significant Impact | <input type="checkbox"/> Less than Significant Impact |
| <input checked="" type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input type="checkbox"/> No Impact |

Discussion/Explanation:

Air quality regulators typically define sensitive receptors as schools (Preschool-12th Grade), hospitals, resident care facilities, day-care centers, or other facilities that may house individuals with health conditions that would be adversely impacted by changes in air quality. The County of San Diego also considers residences as sensitive receptors since they house children and the elderly.

Less Than Significant With Mitigation Incorporated

As discussed in the project’s Air Quality Assessment, without the application of best available control technology for toxics (T-BACT), the project would exceed the health risk threshold of one in one million exposed during the most intense period of construction activities (grading). The project would require all construction equipment have California Air Resources Board (CARB) certified Tier 4 engines with diesel particulate filters (DPFs). The application of Tier 4 engines and DPF would meet the requirements of T-BACT and would reduce the project’s health risk to below 10 in one million, which would be below the County’s impact threshold. The project would have a less than significant impact to sensitive receptors during construction activities with mitigation (Tier 4 engines and DPF) incorporated.

The project includes the development of a high school which would be considered a sensitive receptor and is located adjacent to existing commercial and residential uses. None of the existing surrounding uses would generate operational emissions that would expose new sensitive receptors to potential air quality impacts. As discussed in the Air Quality Assessment and the project Traffic Study, the existing average daily traffic along adjacent roadways is below the CARB recommended advisory screening level for school siting and would not generate any potential impacts to the proposed project. Additionally, during operations, the project would not add traffic to intersections that would result in CO-hotspots. Impacts to sensitive receptors during operations would be less than significant.

d) Result in other emissions (such as those leading to odors) adversely affecting a substantial number of people?

- | | |
|---|--|
| <input type="checkbox"/> Potentially Significant Impact | <input checked="" type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input type="checkbox"/> No Impact |

Less than Significant Impact

Potential onsite odor generators would include short term construction odors from activities such as paving and possibly painting. Odors created during short term construction activities would most likely be from placing asphalt which has a slight odor from the bitumen and solvents used

within hot asphalt. Since odors generated during construction are short-term, they would not be considered a significant impact.

For operations, the County Guidelines for Determining Significance for Air Quality (County of San Diego, 2007) includes a list of odor producing uses that are typically recognized. School uses are not listed and would therefore not be a significant odor causing source. Based on this, the proposed project would not result in significant odors during operations, and impacts would be less than significant.

IV. BIOLOGICAL RESOURCES -- Would the project:

a) Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Wildlife or CDFWU.S. Fish and Wildlife Service?

- | | |
|---|--|
| <input type="checkbox"/> Potentially Significant Impact | <input checked="" type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input type="checkbox"/> No Impact |

Less Than Significant Impact

Based on an analysis of the County’s Geographic Information System (GIS) records, the County’s Comprehensive Matrix of Sensitive Species, site photos, and a Biological Resources Report prepared by Elyssa Robertson (February 25, 2016), County staff biologist, Ashley Smith, has determined that the site supports native vegetation, namely, Non-native grassland. It has been determined that removal of this habitat would not result in substantial adverse effects, either directly or through habitat modifications, to any candidate, sensitive, or special status species for the following reasons: the proposed project site does not contain or marginally contains habitat and/or soils suitable for candidate, sensitive, or special status species (see Attachment C and D of the Biological Technical Report). Therefore, the impact is less than significant.

b) Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, or regulations, or by the California Department of Fish and Wildlife or US Fish and Wildlife Service?

- | | |
|--|---|
| <input type="checkbox"/> Potentially Significant Impact | <input type="checkbox"/> Less than Significant Impact |
| <input checked="" type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input type="checkbox"/> No Impact |

Less Than Significant Impact with Mitigation Incorporated

Based on a site visit conducted by County staff on December 4, 2015, and as supported by the Biological Resources Report prepared by Elyssa Robertson (February 25, 2016), County staff biologist, Ashley Smith, has determined that the proposed project site contains Non-native grassland within the project boundaries; however, no riparian habitats are present. No off-site impacts have been identified within or immediately adjacent to the sensitive natural community. Impacts to 5.26 acres of Non-native grassland would be mitigated at a ratio of 0.5:1, requiring the offsite purchase of 2.62 acres of Tier III habitat or higher within the MSCP South County

Subarea. Therefore, project impacts to any riparian habitat or sensitive natural community identified in the County of San Diego Multiple Species Conservation Program, County of San Diego Resource Protection Ordinance, Natural Community Conservation Plan, Fish and Wildlife Code, Endangered Species Act, Clean Water Act, or any other local or regional plans, policies or regulations, are considered less than significant with mitigation incorporated.

c) Have a substantial adverse effect on state or federally protected wetlands (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?

- Potentially Significant Impact
- Less Than Significant With Mitigation Incorporated
- Less than Significant Impact
- No Impact

No Impact

Based on a site visit conducted by County staff, on December 4, 2015, and as supported by the Biological Resources Report prepared by Elyssa Robertson (February 25, 2016), it has been determined that the proposed project site does not contain any wetlands as defined by Section 404 of the Clean Water Act, including, but not limited to, marsh, vernal pool, stream, lake, river or water of the U.S., that could potentially be impacted through direct removal, filling, hydrological interruption, diversion or obstruction by the proposed development. Therefore, no impacts would occur to wetlands defined by Section 404 of the Clean Water Act and under the jurisdiction of the Army Corps of Engineers.

d) Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?

- Potentially Significant Impact
- Less Than Significant With Mitigation Incorporated
- Less than Significant Impact
- No Impact

Less than Significant Impact

Based on an analysis of the County’s Geographic Information System (GIS) records, the County’s Comprehensive Matrix of Sensitive Species, site photos, a site visit by County staff on December 4, 2015, and as supported by the Biological Resources Report prepared by Elyssa Robertson (February 25, 2016), it has been determined that the site has limited biological value and impedance of the movement of any native resident or migratory fish or wildlife species, the use of an established native resident or migratory wildlife corridors, and the use of native wildlife nursery sites would not be expected as a result of the proposed project for the following reasons: the proposed project site is small (7.7 acres) and surrounded by developed lands, and is located within the South County MSCP (outside PAMA).

e) Conflict with the provisions of any adopted Habitat Conservation Plan, Natural Communities Conservation Plan, other approved local, regional or state habitat conservation plan?

- Potentially Significant Impact
- Less Than Significant With Mitigation Incorporated
- Less than Significant Impact
- No Impact

No Impact

Refer to the attached Ordinance Compliance Checklist dated December 12, 2019 for further information on consistency with any adopted Habitat Conservation Plan, Natural Communities Conservation Plan, other approved local, regional or state habitat conservation plan, including, Habitat Management Plans (HMP), Special Area Management Plans (SAMP), or any other local policies or ordinances that protect biological resources including the Multiple Species Conservation Program (MSCP), Biological Mitigation Ordinance, Resource Protection Ordinance (RPO), Habitat Loss Permit (HLP).

V. CULTURAL RESOURCES -- Would the project:

a) Cause a substantial adverse change in the significance of a historical resource pursuant to §15064.5?

- Potentially Significant Impact
- Less Than Significant With Mitigation Incorporated
- Less than Significant Impact
- No Impact

Less Than Significant Impact

Based on an analysis of records and a survey of the property by County of San Diego approved archaeologist, Jerry Schaefer, Ph.D., it has been determined that there is one historical resource present within the proposed project site. This resource includes concrete foundations that likely post-date World War II. They are neither associated with any significant historical persons nor with a master architect or builder. A cultural resources report entitled, *Cultural Resource Technical Report for the Liberty High School Site Project, Unincorporated Area of El Cajon, San Diego County, California*, dated March 2016, prepared by Jerry Schaefer, Ph.D. and Tony Quach evaluated the significance of the historical resources based on a review of records including maps, studies, property title and archived public records. Based on the results of this study, it has been determined that the historic resource is not significant pursuant to the CEQA Guidelines, Section 15064.5. Moreover, if the resources are not considered significant historic resources pursuant to CEQA Section 15064.5 loss of these resources cannot contribute to a potentially significant cumulative impact. This resource (P-37-034788) would be directly impacted as a result of proposed project implementation.

b) Cause a substantial adverse change in the significance of an archaeological resource pursuant to §15064.5?

- Potentially Significant Impact
- Less Than Significant With Mitigation Incorporated
- Less than Significant Impact
- No Impact

Based on an analysis of records and a survey of the property by a County of San Diego approved archaeologist, Jerry Schaefer, Ph.D. it has been determined that the project site does not contain any archaeological resources. The results of the survey are provided in an archaeological survey report entitled, *Cultural Resource Technical Report for the Liberty High School Site Project, Unincorporated Area of El Cajon, San Diego County, California*, dated March 2016, prepared by Jerry Schaefer, Ph.D. and Tony Quach. Due to the limited visibility of the site and the sensitivity of the area, archaeological monitoring would be made a condition of approval. The monitoring program would include the following requirements:

- Pre-Construction
 - Pre-construction meeting to be attended by the Project Archaeologist and Kumeyaay Native American monitor to explain the monitoring requirements.
- Construction
 - Monitoring. Both the Project Archaeologist and Kumeyaay Native American monitor are to be onsite during earth disturbing activities. The frequency and location of monitoring of native soils will be determined by the Project Archaeologist in consultation with the Kumeyaay Native American monitor. Both the Project Archaeologist and Kumeyaay Native American monitor will evaluate fill soils to ensure that they are negative for cultural resources
 - If cultural resources are identified:
 - Both the Project Archaeologist and Kumeyaay Native American monitor have the authority to divert or temporarily halt ground disturbance operations in the area of the discovery.
 - The Project Archaeologist shall contact the County Archaeologist.
 - The Project Archaeologist in consultation with the County Archaeologist and Kumeyaay Native American shall determine the significance of discovered resources.
 - Construction activities will be allowed to resume after the County Archaeologist has concurred with the significance evaluation.
 - Isolates and non-significant deposits shall be minimally documented in the field. Should the isolates and non-significant deposits not be collected by the Project Archaeologist, the Kumeyaay Native American monitor may collect the cultural material for transfer to a Tribal curation facility or repatriation program.
 - If cultural resources are determined to be significant, a Research Design and Data Recovery Program shall be prepared by the Project Archaeologist in consultation with the Kumeyaay Native American monitor and approved by the County Archaeologist. The program shall include reasonable efforts to preserve (avoid) unique cultural resources of Sacred Sites; the capping of identified Sacred Sites or unique cultural resources and placement of development over the cap if avoidance is infeasible; and data recovery for non-unique cultural resources. The preferred option is preservation (avoidance).
 - Human Remains.
 - The Property Owner or their representative shall contact the County Coroner and the PDS Staff Archaeologist.

- Upon identification of human remains, no further disturbance shall occur in the area of the find until the County Coroner has made the necessary findings as to origin.
 - If the remains are determined to be of Native American origin, the Most Likely Descendant (MLD), as identified by the Native American Heritage Commission (NAHC), shall be contacted by the Property Owner or their representative in order to determine proper treatment and disposition of the remains.
 - The immediate vicinity where the Native American human remains are located is not to be damaged or disturbed by further development activity until consultation with the MLD regarding their recommendations as required by Public Resources Code Section 5097.98 has been conducted.
 - Public Resources Code §5097.98, CEQA §15064.5 and Health & Safety Code §7050.5 shall be followed in the event that human remains are discovered.
- Rough Grading
 - Upon completion of Rough Grading, a monitoring report shall be prepared identifying whether resources were encountered. A copy of the monitoring report shall be provided to the South coastal Information Center and any culturally affiliated tribe who requests a copy.
 - Final Grading
 - A final report shall be prepared substantiating that earth-disturbing activities are completed and whether cultural resources were encountered. A copy of the final report shall be submitted to the South Coastal Information Center and any culturally-affiliated tribe who requests a copy.
 - Disposition of Cultural Material.
 - The final report shall include evidence that all prehistoric materials have been curated at a San Diego curation facility or Tribal curation facility that meets federal standards per 36 CFR Part 79, or alternatively have been repatriated to a culturally affiliated tribe.
 - The final report shall include evidence that all historic materials have been curated at a San Diego curation facility that meets federal standards per 36 CFR Part 79.
- c) Disturb any human remains, including those interred outside of formal cemeteries?
- | | |
|---|---|
| <input type="checkbox"/> Potentially Significant Impact | <input type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input checked="" type="checkbox"/> No Impact |

No Impact

Based on an analysis of records and a survey of the property by a County of San Diego approved archaeologist, Jerry Schaefer, Ph.D., it has been determined that the proposed project would not disturb any human remains because the proposed project site does not include a formal cemetery or any archaeological resources that might contain interred human remains. The results of the survey are provided in an archaeological survey report entitled, *Cultural Resource Technical Report for the Liberty High School Site Project, Unincorporated Area of El Cajon, San*

Diego County, California, dated March 2016, prepared by Jerry Schaefer, Ph.D. and Tony Quach.

VI. ENERGY USE -- Would the project:

a) Result in a potentially significant environmental impact due to wasteful, inefficient, or unnecessary consumption of energy resources, during project construction or operation?

- | | |
|---|--|
| <input type="checkbox"/> Potentially Significant Impact | <input checked="" type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less than Significant with Mitigation Incorporated | <input type="checkbox"/> No Impact |

Less than Significant Impact

The proposed project would result in the use of electricity, natural gas, petroleum, and other consumption of energy resources during both the construction and operation phases of the project; however, the consumption is not expected to be wasteful, inefficient, or unnecessary for the following reasons.

During construction, Tier IV certified construction equipment would be utilized during all phases of construction. Tier IV diesel engine standards are the strictest EPA emissions requirement for off-highway diesel engines. This requirement regulates the amount of particulate matter (PM), or black soot, and nitrogen oxides (NOx) that can be emitted from an off-highway diesel engine. Tier IV equipment also runs more efficiently and thus uses less energy resources. In addition, the project would be required to comply with the Construction and Demolition Materials Diversion Ordinance (Sections 68.508 through 68.518 of the County Code of Regulatory Ordinances). The ordinance requires that 90% of inerts and 70% of all other materials must be recycled from the project. In order to comply with the ordinance, applicants must submit a Construction and Demolition Debris Management Plan and a fully refundable Performance Guarantee prior to building permit issuance. This ultimately would result in less energy use overall as the demolished materials would be reused after recycling.

The proposed project would be designed according to the most recent 2016 Title 24 or future, more stringent versions of Title 24 that are applicable as the project is built out. Part 6 of Title 24 specifically establishes energy efficiency standards for residential buildings constructed in the State of California to reduce energy demand and consumption.

Although the state court set aside the CAP as described in further detail in response VIII Greenhouse Gas Emissions, a), the proposed project was required to incorporate more sustainable design standards and implement applicable reduction measures which are still effective measures to reduce GHG emissions and energy consumption.

According to the State CEQA Guidelines, the means to achieve the goal of conserving energy include decreasing overall energy consumption, decreasing reliance on oil, and increasing reliance on renewable energy sources. Additionally, the proposed project is consistent with the County's Climate Action Plan (CAP) and General Plan through the implementation of the measures identified in the County's CAP Checklist. These sustainability measures that would be implemented on the project site consist of various energy efficiency and design features, water

efficient appliances and installation of rain barrels and trees per residence. Additional measures such as efficient outdoor water usage, solar panels, energy efficient outdoor lighting, electric vehicle charging stations, building efficiency standards, recycling areas, and bike parking racks would be employed by the proposed project. While the project would result in the consumption of energy, the implementation of building energy efficiency measures, on-site renewable installation, and the supporting of EV infrastructure would reduce unnecessary energy consumption and support State efforts to conserve energy. Therefore, the construction and operation of the proposed project is not expected to result in the wasteful or inefficient use of energy.

b) Conflict with or obstruct a state or local plan for renewable energy or energy efficiency?

- | | |
|---|--|
| <input type="checkbox"/> Potentially Significant Impact | <input checked="" type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less than Significant with Mitigation Incorporated | <input type="checkbox"/> No Impact |

Less than Significant Impact

Many of the regulations regarding energy efficiency are focused on increasing the energy efficiency of buildings and renewable energy generation, as well as reducing water consumption and VMT. As stated in response VI. (a), the proposed project is employing the use of various energy efficient and savings features, as well as roof top solar photovoltaics that meet the regulatory requirements. The proposed project would be consistent with several energy reduction policies of the County General Plan, including policies COS-14.1, COS-14.3, and COS-16.2. Additionally, the proposed project would be consistent with sustainable development and energy reduction policies such as policies COS-14.3 and COS-15.4, through compliance with the most recent Title 24 standards at the time of project construction. The proposed project would implement energy reduction design features and comply with the most recent energy building standards consistent with applicable plans and policies. Therefore, impacts would be less than significant.

VII. GEOLOGY AND SOILS -- Would the project:

a) Directly or indirectly cause potential substantial adverse effects, including the risk of loss, injury, or death involving:

- i. Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42.

- | | |
|---|---|
| <input type="checkbox"/> Potentially Significant Impact | <input type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input checked="" type="checkbox"/> No Impact |

No Impact

The proposed project is not located in a fault rupture hazard zone identified by the Alquist-Priolo Earthquake Fault Zoning Act, Special Publication 42, Revised 1997, Fault-Rupture Hazards Zones in California, or located within any other area with substantial evidence of a known fault.

Therefore, there would be no impact from the exposure of people or structures to adverse effects from a known fault-rupture hazard zone.

ii. Strong seismic ground shaking?

- | | |
|---|--|
| <input type="checkbox"/> Potentially Significant Impact | <input checked="" type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input type="checkbox"/> No Impact |

Less Than Significant Impact

To ensure the structural integrity of all buildings and structures, the proposed project must conform to the Seismic Requirements as outlined within the California Building Code. The County Code requires a soils compaction report with proposed foundation recommendations to be approved before the issuance of a building permit. Therefore, compliance with the California Building Code and the County Code ensures the project would not result in a potentially significant impact from the exposure of people or structures to potential adverse effects from strong seismic ground shaking.

iii. Seismic-related ground failure, including liquefaction?

- | | |
|---|--|
| <input type="checkbox"/> Potentially Significant Impact | <input checked="" type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input type="checkbox"/> No Impact |

Less Than Significant Impact

The proposed project site is not within a “Potential Liquefaction Area” as identified in the County Guidelines for Determining Significance for Geologic Hazards. This indicates that the liquefaction potential at the site is low. In addition, the site is not underlain by poor artificial fill or located within a floodplain. Therefore, there would be a less than significant impact from the exposure of people or structures to adverse effects from a known area susceptible to ground failure, including liquefaction. In addition, since liquefaction potential at the site is low, earthquake-induced lateral spreading is not considered to be a seismic hazard at the site and impacts would be less than significant.

iv. Landslides?

- | | |
|---|--|
| <input type="checkbox"/> Potentially Significant Impact | <input checked="" type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input type="checkbox"/> No Impact |

Less Than Significant Impact

The site is not located within a “Landslide Susceptibility Area” as identified in the County Guidelines for Determining Significance for Geologic Hazards. Landslide Susceptibility Areas were developed based on landslide risk profiles included in the *Multi-Jurisdictional Hazard Mitigation Plan, San Diego, CA* (URS, 2004). Landslide risk areas from this plan were based on data including steep slopes (greater than 25%); soil series data (SANDAG based on USGS 1970s series); soil-slip susceptibility from USGS; and Landslide Hazard Zone Maps (limited to

western portion of the County) developed by the California Department of Conservation, Division of Mines and Geology (DMG). Also included within Landslide Susceptibility Areas are gabbroic soils on slopes steeper than 15% in grade because these soils are slide prone. A slope analysis was prepared by James Roberts, RCE (March 15, 2016) and it was determined that the average existing land slope is 10.7 percent. A Geotechnical Report is required prior to any construction activities. Because the geologic environment has a low probability to become unstable, the proposed project would have a less than significant impact from the exposure of people or structures to potential adverse effects from landslides. Therefore, there would be no potentially significant impact from the exposure of people or structures to adverse effects from adverse effects of landslides.

b) Result in substantial soil erosion or the loss of topsoil?

- | | |
|---|--|
| <input type="checkbox"/> Potentially Significant Impact | <input checked="" type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input type="checkbox"/> No Impact |

Less Than Significant Impact

According to the Soil Survey of San Diego County, the soils on-site are identified as Placentia sandy loam, 2 to 9 percent slopes, and Vista coarse sandy loam, 9 to 15 percent slopes that has a soil erodibility rating of “moderate” and/or “severe” as indicated by the Soil Survey for the San Diego Area, prepared by the US Department of Agriculture, Soil Conservation and Forest Service dated December 1973. However, the proposed project would not result in substantial soil erosion or the loss of topsoil for the following reasons:

- The proposed project would not result in unprotected erodible soils; would not alter existing drainage patterns; is not located in a floodplain, wetland, or significant drainage feature; and would not develop steep slopes.
- A Storm Water Management Plan has been prepared (Scott Harry, P.E., P.L.S. April 30, 2017). The plan includes the following Best Management Practices to ensure sediment does not erode from the proposed project site:
 - Temporary Construction BMPs
 - Silt Fence and Fiber Rolls
 - Street Sweeping/Vacuuming
 - Stockpile Management
 - Solid Waste Management
 - Stabilized Construction Entrance/Exit
 - Desilting Basin
 - Gravel Bag Berm
 - Material Delivery & Storage
 - Concrete Waste Management
 - Paving and Grinding Operations
 - Operational BMPs
 - Signage
 - Non-hazardous Soil Amendments
 - Smart Irrigation Systems
 - Grease Interceptor
 - Covered & Paved Trash Enclosures
 - Downspout to Swale or Landscaping
 - Direct Drainage to Infiltration Basin and/or Trench
- The proposed project involves grading. However, the proposed project is required to comply with the San Diego County Code of Regulations, Title 8, Zoning and Land Use

Regulations, Division 7, Sections 87.414 (DRAINAGE - EROSION PREVENTION) and 87.417 (PLANTING). Compliance with these regulations minimizes the potential for water and wind erosion.

Due to these factors, it has been found that the proposed project would not result in substantial soil erosion or the loss of topsoil on a project level.

In addition, the proposed project would not contribute to a cumulatively considerable impact because all the of past, present and future projects included on the list of projects that involve grading or land disturbance are required to follow the requirements of the San Diego County Code of Regulations, Title 8, Zoning and Land Use Regulations, Division 7, Sections 87.414 (DRAINAGE - EROSION PREVENTION) and 87.417 (PLANTING); Order 2001-01 (NPDES No. CAS 0108758), adopted by the San Diego Region RWQCB on February 21, 2001; County Watershed Protection, Storm Water Management, and Discharge Control Ordinance (WPO) (Ord. No. 9424); and County Storm water Standards Manual adopted on February 20, 2002, and amended January 10, 2003 (Ordinance No. 9426). Refer to XVIII. Mandatory Findings of Significance for a comprehensive list of the projects considered.

- c) Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in an on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?

Potentially Significant Impact Less than Significant Impact
 Less Than Significant With Mitigation No Impact
 Incorporated

Less Than Significant Impact

The proposed project involves 23,500 cubic yards of grading that would result in the creation of areas of cut and areas underlain by fill. In order to assure that any proposed buildings (including those proposed on the project site) are adequately supported (whether on native soils, cut or fill), a Soils Engineering Report is required as part of the Building Permit process. This Report would evaluate the strength of underlying soils and make recommendations on the design of building foundation systems. The Soils Engineering Report must demonstrate that a proposed building meets the structural stability standards required by the California Building Code. The report must be approved by the County prior to the issuance of a Building Permit. With this standard requirement, impacts would be less than significant. For further information regarding landslides, liquefaction, and lateral spreading, refer to VI Geology and Soils, Question a listed above.

- d) Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property?

Potentially Significant Impact Less than Significant Impact
 Less Than Significant With Mitigation No Impact
 Incorporated

Less Than Significant Impact

The proposed project is located on expansive soils as defined within Table 18-I-B of the Uniform Building Code (1994). This was confirmed by staff review of the Soil Survey for the San Diego Area, prepared by the US Department of Agriculture, Soil Conservation and Forest Service dated December 1973. The soils on-site are Placentia sandy loam, 2 to 9 percent slopes, and Vista coarse sandy loam, 9 to 15 percent slopes. However the project would not have any significant impacts because the project is required to comply the improvement requirements identified in the 1997 Uniform Building Code, Division III – Design Standard for Design of Slab-On-Ground Foundations to Resist the Effects of Expansive Soils and Compressible Soils, which ensure suitable structure safety in areas with expansive soils. Therefore, these soils would not create substantial risks to life or property.

e) Have soils incapable of adequately supporting the use of septic tanks or alternative wastewater disposal systems where sewers are not available for the disposal of wastewater?

- | | |
|---|---|
| <input type="checkbox"/> Potentially Significant Impact | <input type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input checked="" type="checkbox"/> No Impact |

No Impact

The proposed project would rely on public water and sewer for the disposal of wastewater. A service availability letter dated October 30, 2015 has been received from the Otay Water District indicating that the facility has adequate capacity for the proposed project’s wastewater disposal needs. No septic tanks or alternative wastewater disposal systems are proposed.

f) Directly or indirectly destroy a unique paleontological resource or site or unique geological feature?

- | | |
|---|---|
| <input type="checkbox"/> Potentially Significant Impact | <input type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input checked="" type="checkbox"/> No Impact |

San Diego County has a variety of geologic environments and geologic processes which generally occur in other parts of the state, country, and the world. However, some features stand out as being unique in one way or another within the boundaries of the County.

No Impact

A review of the County’s Paleontological Resources Maps indicates that the project is located entirely on plutonic igneous rock (cretaceous plutonic) and has no potential for producing fossil remains. In addition, the project site does not contain any unique geologic features that have been listed in the County’s Guidelines for Determining Significance for Unique Geology Resources nor does the site support any known geologic characteristics that have the potential to support unique geologic features.

VIII. GREENHOUSE GAS EMISSIONS – Would the project

a) Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?

- | | |
|---|--|
| <input type="checkbox"/> Potentially Significant Impact | <input checked="" type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input type="checkbox"/> No Impact |

Overview

The proposed project would incorporate sustainability features including the installation of a 90-kilowatt (kW) solar photovoltaic (PV) system, low flow water fixtures, and electric vehicle (EV) charging stations. Further details for the project description and design features are included in the project's Global Climate Change study, prepared by Ldn Consulting in November 2019.

The proposed project would generate greenhouse gas (GHG) emissions during construction activities through the operation of construction equipment, and during operations directly through mobile sources (i.e. vehicle trips) and area sources (i.e. consumer products, landscaping equipment), and indirectly through electricity consumption and solid waste decomposition.

Background on CAP and Litigation

The County of San Diego adopted a ~~Climate Action Plan~~ CAP on February 14, 2018 which outlines actions that the County will undertake to meet its greenhouse gas (GHG) emissions reductions targets. ~~Implementation of the CAP requires that new development projects incorporate more sustainable design standards and implement applicable reduction measures consistent with the CAP.~~ In March 2018, several petitioners filed a lawsuit against the County, alleging that the CAP and, in particular, M-GHG-1 were inconsistent with General Plan Goal COS-20 and Policy COS-20.1. In December 2018, the San Diego Superior Court ruled against the County. The Court issued a writ ordering the approval of the CAP and its EIR to be set aside, and enjoining reliance on the County CAP's mitigation measure M-GHG-1. In January 2019, the County appealed the San Diego Superior Court ruling which stayed the above described writ. Essentially, the CAP and its EIR were still in place during the appeal. However, in June 2020, the state appeals court upheld the lower court's ruling, rejecting the County's CAP.

Given the current legal instability concerning the County's CAP during review of the proposed project, the analysis prepared for the proposed project did not rely on the CAP to streamline the Project's environmental analysis under CEQA Guidelines Section 15183.5. ~~Rather, the proposed project's significance determination used the criteria detailed above, (informed by CEQA Guidelines Section 15064.4) and mitigation strategies (informed by CEQA Guidelines Section 15126.4(c)) that are independent of the CAP.~~ As such, In the event that the CAP does not withstand judicial scrutiny, the project has undergone a separate, stand-alone analysis for determining whether the project's GHG emissions would significantly impact the environment.

Amendments to Section 15064.4 of the CEQA Guidelines were adopted to assist lead agencies in determining the significance of impacts of GHG emissions. Section 15064.4 specifies that a lead agency "shall make a good-faith effort, based on the extent possible on scientific and factual data, to describe, calculate or estimate the amount of GHG emissions resulting from a project."

Section 15064.4 also provides lead agencies with the discretion to determine whether to assess those emissions quantitatively or to rely on a qualitative analysis or performance-based standard. In addition, the CEQA Guidelines specific that “[w]hen adopting or using thresholds of significance, a lead agency may consider thresholds of significance previously adopted or recommended by other public agencies, or recommended by experts, provided the decision of the lead agency to adopt such thresholds is support by substantial evidence” (14 CCR 15064.7(c)).

In the absence of a locally adopted threshold by the County of San Diego, projects can be evaluated according to CEQA Guidelines Section 15064.7(c) by comparing the project’s GHG emissions to an efficiency threshold prepared through the County’s Local Government Partnership with San Diego Gas and Electric (SDG&E). This efficiency threshold is based on a countywide inventory of GHG emissions, forecasted consistent with meeting the State reduction target in 2030 and reduction goal in 2050.

~~Therefore, for purposes of this analysis, the two Appendix G checklist questions set forth above are utilized as the thresholds of significance when evaluating the environmental effects of the project’s GHG emissions. In applying these thresholds, reference is made to CEQA Guidelines Section 15064.4(b)(1)-(3), which states that, “a lead agency should consider the following factors, among others, when assessing the significance of impacts from greenhouse gas emissions on the environment: (1) the extent to which a project may increase or reduce greenhouse gas emissions as compared to the existing environmental setting; (2) whether project emissions exceed a threshold of significance that the lead agency determines applies to the project; and, (3) the extent to which the project complies with regulations or requirements adopted to implement a statewide, regional, or local plan for the reduction or mitigation of greenhouse gas emissions.”~~

Recognizing that GHG emissions contribute to the cumulative impact condition of global climate change, Section 15064(h)(1) of the CEQA Guidelines is also applicable. Section 15064(h)(1) states that “the lead agency shall consider whether the cumulative impact is significant and whether the effects of the project are cumulatively considerable.” A cumulative impact may be significant when the project’s incremental effect, though individually limited, is cumulatively considerable. “Cumulatively considerable” means that the incremental effects of an individual project are significant when viewed in connection with the effects of other past, current, and reasonably foreseeable probable future projects.

Less Than Significant Impact

As discussed in further detail in the project’s Global Climate Change study, the County has developed a baseline emissions inventory with emissions ~~projections~~ forecasts through 2030 and 2050, developed and funded through the County’s Local Government Partnership with SDG&E. Target emissions reductions based on these projections were set by the County, consistent with State requirements (i.e. Senate Bill 32, CARB Scoping Plan) to achieve a 40 percent reduction from baseline emissions projections in 2030.

Efficiency metrics, which describe emissions based on a per capita basis, per service population basis, or some other rate-oriented descriptor, are commonly used and recommended throughout the state to determine potential impacts related to GHGs (e.g. CARB Scoping Plan

recommendations, Bay Area Air Quality Management District [BAAQMD] efficiency thresholds). The per service population metric, which refers to the sum of the number of jobs and residents (or for the proposed project, students) generated by the project, identifies a GHG threshold which, if below, the project would be determined to generate GHG emissions that would not conflict with State requirements and would assist the County in reducing community-wide GHG emissions to meet 2030 targets.

Based on the County's baseline GHG inventory and forecasts projections, projects in the County need to demonstrate they would generate emissions below the 2.94 metric tons of carbon dioxide equivalent (MTCO_{2e}) per service population. This service population threshold was determined using local data to be demonstrate consistency with the communitywide GHG reduction targets for 2030 based on State reduction targets.

Further detail for the calculations and assumptions applied to estimate project generated GHG emissions during construction and operations are provided in the project Global Climate Change study. The project would have a total service population of 488, including 450 students and 33 staff. Based on the project analysis, the project construction and operations would result in the generation of approximately 830 MTCO_{2e} annually, or 1.7 MTCO_{2e} per service population. Thus, the project's estimated GHG emissions would be below the County's service population threshold for 2030.

Although the state court set aside the CAP, the proposed project was required to incorporate sustainable design standards and implement applicable reduction measures which are still effective measures to reduce GHG emissions. These measures include carpool services and carpool/vanpool-only parking spaces, bicycle parking facilities, electric vehicle charging stations and electric vehicle-only parking spaces, and landscaping compliant with the County's Water Conservation in Landscaping Ordinance which requires a 40% reduction in current maximum applied water allowance for outdoor use. As stated above, the proposed project would also install a 90kW solar PV system. These measures would assist in reducing reliance on fossil fuels by providing incentives to participate in carpool/vanpools or alternative transportation methods such as biking or electric vehicles, provide a clean electricity source, and reduce water consumption, which in turn reduces the amount of energy needed to pump and treat water.

The State of California also requires additional project design requirements that would reduce GHG emissions from project operations including Title 24 of the California Code of Regulations. Part 6 of Title 24 established Building Energy Efficiency Standards that are designed to ensure buildings achieve energy efficiency and preserve outdoor and indoor environmental quality. The California Energy Commission (CEC) adopts updated standards every 3 years to incorporate new energy efficient technologies and construction methods. As a result, these standards save energy, increase electricity supply reliability, and avoid the need to construct new power plants. Part 11 of Title 24 established the nation's first green building standards that requires minimum mandatory standards, as well as voluntary standards, pertaining to the planning and design of sustainable site development, energy efficiency (in excess of Part 6), water conservation, material conservation, and interior air quality. The Project would be required to comply with the latest version of Part 6 and Part 11 of Title 24 standards at the building permit and construction phases.

It should also be noted that the Liberty Charter High School is the 9-12th grade program that serves the families that have enrolled their students in Literacy First Charter Schools. Most of those students live in communities near the proposed school site including the cities of El Cajon and La Mesa, and unincorporated communities of Spring Valley and Valle de Oro. The proposed school campus near the corner of Chase Avenue and Jamacha Boulevard would replace the current campus located in Lemon Grove. The relocated High School would be closer to the Elementary and Middle Schools (which represent the High School’s anticipated future student base) and would result in a reduction in vehicle miles traveled (which account for most of the emissions generated by the project) compared to the current location.

As previously stated, the project’s estimated GHG emissions would be below the service population threshold for 2030 utilized for this project. In addition, numerous design measures have been incorporated into the proposed project, reducing GHG emissions. Therefore, impacts related to the generation of GHG emissions would be less than significant.

b) Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases?

- | | |
|---|--|
| <input type="checkbox"/> Potentially Significant Impact | <input checked="" type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input type="checkbox"/> No Impact |

Less Than Significant Impact

The California Governor’s Office of Planning and Research (OPR) has advised that “a project that falls below an efficiency threshold that is aligned with long-term goals and relevant plans has no cumulative impact distinct from the project impact.” As described in the discussion for (a), the project would not result in a cumulatively considerable contribution to global climate change. The project land use and estimate emissions would be consistent with County goals and policies included in the County General Plan that address GHG reductions. Thus, the project would also be consistent with emissions reduction targets of AB 32 and the Global Warming Solutions Act. The project would not conflict with any applicable plan, policy, or regulation adopted for the purpose of reduction emissions of GHG emissions. Further discussion on project consistency with applicable plans and GHG reducing design features are included in the project’s Global Climate Change study.

IX. HAZARDS AND HAZARDOUS MATERIALS -- Would the project:

a) Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?

- | | |
|---|---|
| <input type="checkbox"/> Potentially Significant Impact | <input type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input checked="" type="checkbox"/> No Impact |

No Impact

The proposed project would not create a significant hazard to the public or the environment because it does not propose the storage, use, transport, emission, or disposal of Hazardous

Substances, nor are Hazardous Substances proposed or currently in use in the immediate vicinity. In addition, the project does not propose to demolish any existing structures onsite and therefore would not create a hazard related to the release of asbestos, lead based paint or other hazardous materials from demolition activities.

b) Create a significant hazard to the public or the environment through reasonable foreseeable upset and accident conditions involving the release of hazardous materials into the environment?

- | | |
|---|---|
| <input type="checkbox"/> Potentially Significant Impact | <input type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input checked="" type="checkbox"/> No Impact |

No Impact

As stated above under response IX(a), the proposed project would not create a significant hazard to the public or the environment because it does not propose the storage, use, transport, emission, or disposal of Hazardous Substances, and therefore could not result in the upset and accident conditions which would release hazardous materials. In addition, the project does not propose to demolish any existing structures onsite and therefore would not create a hazard related to the release of asbestos, lead based paint or other hazardous materials from demolition activities.

c) Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?

- | | |
|---|---|
| <input type="checkbox"/> Potentially Significant Impact | <input type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input checked="" type="checkbox"/> No Impact |

No Impact

Although the project is a proposed school, it is not located within one-quarter mile of an existing or other proposed school. The proposed project does not propose the handling, storage, or transport of hazardous materials. Therefore, the proposed project would not have any effect on an existing or proposed school.

d) Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5, or is otherwise known to have been subject to a release of hazardous substances and, as a result, would it create a significant hazard to the public or the environment?

- | | |
|---|--|
| <input type="checkbox"/> Potentially Significant Impact | <input checked="" type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input type="checkbox"/> No Impact |

Less than Significant Impact

Based on a site visit and regulatory database search, the proposed project site has not been subject to a release of hazardous substances that would create a significant hazard to the public

or environment. A Phase I Environmental Assessment was prepared for the proposed project (Marc Boogay, March 15, 2016, accepted April 2017) that did not identify any recognized environmental conditions for the subject site. The proposed project site is not included in any of the following lists or databases: the State of California Hazardous Waste and Substances sites list compiled pursuant to Government Code Section 65962.5., the San Diego County Hazardous Materials Establishment database, the San Diego County DEH Site Assessment and Mitigation (SAM) Case Listing, the Department of Toxic Substances Control (DTSC) Site Mitigation and Brownfields Reuse Program Database (“CalSites” Envirostor Database), the Resource Conservation and Recovery Information System (RCRIS) listing, the EPA’s Superfund CERCLIS database or the EPA’s National Priorities List (NPL). Additionally, the proposed project does not propose structures for human occupancy or significant linear excavation within 1,000 feet of an open, abandoned, or closed landfill, is not located on or within 250 feet of the boundary of a parcel identified as containing burn ash (from the historic burning of trash), is not on or within 1,000 feet of a Formerly Used Defense Site (FUDS), does not contain a leaking Underground Storage Tank (UST) and is not located on a site with the potential for contamination from historic uses such as intensive agriculture, industrial uses, a gas station or vehicle repair shop. Therefore, the proposed project would not create a significant hazard to the public or environment.

e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard, or excessive noise, for people residing or working in the project area?

- | | |
|---|---|
| <input type="checkbox"/> Potentially Significant Impact | <input type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input checked="" type="checkbox"/> No Impact |

No Impact

The proposed project is not located within an Airport Land Use Compatibility Plan (ALUCP), an Airport Influence Area, or a Federal Aviation Administration Height Notification Surface. Also, the proposed project does not propose construction of any structure equal to or greater than 150 feet in height, constituting a safety hazard to aircraft and/or operations from an airport or heliport. Therefore, the proposed project would not constitute a safety hazard for people residing or working in the proposed project area.

f) Impair implementation of, or physically interfere with an adopted emergency response plan or emergency evacuation plan?

- | | |
|---|--|
| <input type="checkbox"/> Potentially Significant Impact | <input checked="" type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input type="checkbox"/> No Impact |

The following sections summarize the proposed project’s consistency with applicable emergency response plans or emergency evacuation plans.

i. OPERATIONAL AREA EMERGENCY PLAN AND MULTI-JURISDICTIONAL HAZARD MITIGATION PLAN:

Less Than Significant Impact

The Operational Area Emergency Plan is a comprehensive emergency plan that defines responsibilities, establishes an emergency organization, defines lines of communications, and is designed to be part of the statewide Standardized Emergency Management System. The Operational Area Emergency Plan provides guidance for emergency planning and requires subsequent plans to be established by each jurisdiction that has responsibilities in a disaster situation. The Multi-Jurisdictional Hazard Mitigation Plan includes an overview of the risk assessment process, identifies hazards present in the jurisdiction, hazard profiles, and vulnerability assessments. The plan also identifies goals, objectives and actions for each jurisdiction in the County of San Diego, including all cities and the County unincorporated areas. The proposed project would not interfere with this plan because it would not prohibit subsequent plans from being established or prevent the goals and objectives of existing plans from being carried out.

ii. SAN DIEGO COUNTY NUCLEAR POWER STATION EMERGENCY RESPONSE PLAN

No Impact

The San Diego County Nuclear Power Station Emergency Response Plan would not be interfered with by the proposed project due to the location of the proposed project, the plant and the specific requirements of the plan. The emergency plan for the San Onofre Nuclear Generating Station includes an emergency planning zone within a 10-mile radius. All land area within 10 miles of the plant is not within the jurisdiction of the unincorporated County and as such a proposed project in the unincorporated area is not expected to interfere with any response or evacuation.

iii. OIL SPILL CONTINGENCY ELEMENT

No Impact

The Oil Spill Contingency Element will not be interfered with because the proposed project is not located along the coastal zone or coastline.

iv. EMERGENCY WATER CONTINGENCIES ANNEX AND ENERGY SHORTAGE RESPONSE PLAN

No Impact

The Emergency Water Contingencies Annex and Energy Shortage Response Plan would not be interfered with because the proposed project does not propose altering major water or energy supply infrastructure, such as the California Aqueduct.

v. DAM EVACUATION PLAN

No Impact

The Dam Evacuation Plan would not be interfered with because the proposed project is not located within a dam inundation zone.

g) Expose people or structures, either directly or indirectly, to a significant risk of loss, injury or death involving wildland fires?

- | | |
|---|---|
| <input type="checkbox"/> Potentially Significant Impact | <input type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input checked="" type="checkbox"/> No Impact |

No Impact

The proposed project is surrounded by urbanized areas and/or irrigated lands and no wildlands are adjacent to the proposed project. Also, a Fire Service Availability Letter and conditions, dated October 27, 2015, have been received from the San Miguel Consolidated Fire Protection District. The conditions from the San Miguel Consolidated Fire Protection District include the following requirements:

- Fire sprinklers required in all structures in accordance with NFPA 13 Standards and San Miguel Consolidated Fire Protection District Standards.
- Fire alarm system to be installed in accordance with NFPA 72 Standards.
- Gates shall have a clear width of 24 feet, and a vertical clearance of 13 feet 6 inches.
- Provide a 24-foot-wide fire lane for access to all sides of the school.
- Provide access from structures to the athletic field by means of a ramp/stairs for emergency evacuation of students.
- Fire hydrants shall be provided on the public street or on the site of the premises or both to be protected as required and approved by the Chief.

The Fire Service Availability Letter indicates the expected emergency travel time to the proposed project site to be 4 minutes. The Maximum Travel Time allowed pursuant to the Safety Element is 5 minutes. Therefore, based on the location of the proposed project; review of the project by County staff; and through compliance with the San Miguel Consolidated Fire Protection District's conditions, the proposed project is not expected to expose people or structures to a significant risk of loss, injury or death involving hazardous wildland fires.

X. HYDROLOGY AND WATER QUALITY -- Would the project:

a) Violate any water quality standards or waste discharge requirements or otherwise substantially degrade surface or groundwater quality?

- | | |
|---|--|
| <input type="checkbox"/> Potentially Significant Impact | <input checked="" type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input type="checkbox"/> No Impact |

Less Than Significant Impact: The proposed project proposes the development and operation of public charter high school which requires biofiltration BMPs be implemented to address both pollutant and hydromodification management requirements. The project applicant has provided a copy of the Priority Development (PDP) Storm Water Quality Management Plan (SWQMP) prepared by KARN Engineering and Surveying, dated 4/30/17, which demonstrates that the project would comply with all requirements of the 2013 Municipal Separate Storm Sewer System (MS4) permit and County of San Diego BMP Design Manual 2016. The proposed project site proposes and would be required to implement the following site design measures and/or source

control BMPs and/or treatment control BMPs to reduce potential pollutants to the maximum extent practicable from entering storm water runoff: Storm Drain Stenciling or Signage, Trash Storage Enclosure, Permeable Pavement, and Biofiltration Basins. These measures would enable the proposed project to meet waste discharge requirements as required by the Land-Use Planning for New Development and Redevelopment Component of the San Diego Municipal Permit (SDRWQCB Order No. R9-2013-0001), as implemented by the San Diego County Jurisdictional Runoff Management Program (JRMP) and BMP Design Manual.

Finally, the proposed project's conformance to the waste discharge requirements listed above ensures the project would not create cumulatively considerable water quality impacts related to waste discharge because, through the permit, the proposed project would conform to Countywide watershed standards in the JRMP and BMP Design Manual, derived from State regulation to address human health and water quality concerns. Therefore, the proposed project would not contribute to a cumulatively considerable impact to water quality from waste discharges.

b) Substantially decrease groundwater supplies or interfere substantially with groundwater recharge such that the project may impede sustainable groundwater management of the basin?

- Potentially Significant Impact
- Less Than Significant With Mitigation Incorporated
- Less than Significant Impact
- No Impact

No Impact

The proposed project would obtain its water supply from the Otay Water District that obtains water from surface reservoirs or other imported water source. The project would not use any groundwater for any purpose, including irrigation, domestic or commercial demands. In addition, the proposed project does not involve operations that would interfere substantially with groundwater recharge including, but not limited to the following: the proposed project does not involve regional diversion of water to another groundwater basin; or diversion or channelization of a stream course or waterway with impervious layers, such as concrete lining or culverts, for substantial distances (e.g. ¼ mile). These activities and operations can substantially affect rates of groundwater recharge. Therefore, no impact to groundwater resources or groundwater management is anticipated.

c) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or through the addition of impervious surfaces, in a manner which would:

i. Result in substantial erosion or siltation on- or off-site?

- Potentially Significant Impact
- Less Than Significant With Mitigation Incorporated
- Less than Significant Impact
- No Impact

Less Than Significant Impact: The project proposes the development and operation of public charter high school. As outlined in the PDP SWQMP dated 4/30/2017 and prepared by KARN Engineering and Surveying, the proposed project would implement the following site design measures, source control, and/or treatment control BMPs to reduce potential pollutants, including sediment from erosion or siltation, to the maximum extent practicable from entering storm water runoff: temporary construction BMPs, stabilization planting/vegetation, self-retaining permeable pavement, and Biofiltration basins. These measures would control erosion and sedimentation and satisfy waste discharge requirements as required by the Land-Use Planning for New Development and Redevelopment Component of the San Diego Municipal Permit (SDRWQCB Order No. R9-2013-0001), as implemented by the San Diego County Jurisdictional Runoff Management Program (JRMP) and BMP Design Manual. The SWQMP specifies and describes the implementation process of all BMPs that would address equipment operation and materials management, prevent the erosion process from occurring, and prevent sedimentation in any onsite and downstream drainage swales. The Department of Public Works would ensure that the Plan is implemented as proposed. Due to these factors, it has been found that the proposed project would not result in significantly increased erosion or sedimentation potential and would not alter any drainage patterns of the site or area on- or off-site. In addition, because erosion and sedimentation would be controlled within the boundaries of the proposed project, the proposed project would not contribute to a cumulatively considerable impact. For further information on soil erosion refer to VI., Geology and Soils, Question b.

ii. Substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or offsite?

- | | |
|---|--|
| <input type="checkbox"/> Potentially Significant Impact | <input checked="" type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input type="checkbox"/> No Impact |

Less Than Significant Impact: The proposed project would not significantly alter established drainage patterns or significantly increase the amount of runoff for the following reasons, based on a Drainage Study prepared by KARN Engineering and Surveying on 4/30/2017:

- Drainage would be conveyed to either natural drainage channels or approved drainage facilities.
- The proposed project would not increase surface runoff exiting the proposed project site equal to or greater than one cubic foot/second.

Therefore, the proposed project would not substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or off-site. Moreover, the proposed project would not contribute to a cumulatively considerable alteration or a drainage pattern or increase in the rate or amount of runoff, because the project would not substantially increase water surface elevation or runoff exiting the site, as detailed above.

iii. Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff?

- | | |
|---|--|
| <input type="checkbox"/> Potentially Significant Impact | <input checked="" type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input type="checkbox"/> No Impact |

Less Than Significant Impact: The proposed project does not propose to create or contribute runoff water that would exceed the capacity of existing or planned storm water drainage systems. The project proposes to connect to an existing storm water drainage system and would discharge at a flow rate equal to the pre-development flow rate based on the Drainage Study prepared by KARN Engineering and Surveying on 4/30/2017.

The project proposes the following potential sources of polluted runoff: construction activities, parking lots, trash storage or refuse area, food service, and a fertilized sports field. However, the following site design measures and/or source control BMPs and/or treatment control BMPs would be employed such that potential pollutants would be reduced in runoff to the maximum extent practicable: temporary construction BMPs, storm drain stenciling or signage, trash storage enclosure, self-retaining permeable pavement, and Biofiltration basins. Refer to IX Hydrology and Water Quality Questions a, b, c, for further information.

d) In flood hazard, tsunami, or seiche zones, risk release of pollutants due to project inundation?

- | | |
|---|---|
| <input type="checkbox"/> Potentially Significant Impact | <input type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input checked="" type="checkbox"/> No Impact |

i. FLOOD HAZARD

No Impact

No FEMA mapped floodplains, County-mapped floodplains or drainages with a watershed greater than 25 acres were identified on the proposed project site; therefore, no impact would occur.

ii. TSUNAMI

No Impact

The proposed project site is located more than a mile from the coast; therefore, in the event of a tsunami, would not be inundated.

iii. SEICHE

No Impact

The proposed project site is not located along the shoreline of a lake or reservoir; therefore, could not be inundated by a seiche.

e) Conflict with or obstruct implementation of a water quality control plan or sustainable groundwater management plan?

- | | |
|---|---|
| <input type="checkbox"/> Potentially Significant Impact | <input type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input checked="" type="checkbox"/> No Impact |

No Impact

As described in response (a), the proposed project would implement a combination of site design, source control and structural BMPs to prevent potential pollutants from entering storm water runoff. This includes infiltration basin and trench located at the eastern portion of the site which would treat on-site runoff. In addition, the proposed BMPs are consistent with regional surface water, storm water and groundwater planning and permitting process that has been established to improve the overall water quality in County watersheds. Moreover, the proposed project would obtain its water supply from the Otay Water District that obtains water from surface reservoirs or other imported water source and would therefore not impact a sustainable groundwater management plan. As a result, the proposed project would not contribute to a cumulatively considerable impact to obstruction to implementation of a water quality control plan or sustainable groundwater management plan.

XI. LAND USE AND PLANNING -- Would the project:

a) Physically divide an established community?

- | | |
|---|---|
| <input type="checkbox"/> Potentially Significant Impact | <input type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input checked="" type="checkbox"/> No Impact |

No Impact

The proposed project does not propose the introduction of new infrastructure such major roadways or water supply systems, or utilities to the area. Therefore, the proposed project would not significantly disrupt or divide the established community.

b) Cause a significant environmental impact due to a conflict with any land use plan, policy, or regulation adopted for the purpose of avoiding or mitigating an environmental effect?

- | | |
|---|--|
| <input type="checkbox"/> Potentially Significant Impact | <input checked="" type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input type="checkbox"/> No Impact |

Less Than Significant Impact

The proposed project is a charter high school within the Valle de Oro Community Plan Area of the County of San Diego General Plan. The proposed project is subject to the General Plan Semi-Rural Regional Category and the Semi-Rural 0.5 (SR-0.5) Land Use Designation. The proposed project is also subject to the policies of the Valle De Oro Community Plan. The property is zoned Rural Residential (RR) which permits schools with a Major Use Permit pursuant to the Zoning Ordinance Section 2185.

The proposed project does not conflict with any adopted land use plan, policy or regulation. It complies with the MSCP, RPO, and CEQA. Furthermore, it is consistent with the County of San Diego Guidelines for Significance.

XII. MINERAL RESOURCES -- Would the project:

a) Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?

- | | |
|---|--|
| <input type="checkbox"/> Potentially Significant Impact | <input checked="" type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input type="checkbox"/> No Impact |

Less Than Significant Impact

The proposed project site has been classified by the California Department of Conservation – Division of Mines and Geology (Update of Mineral Land Classification: Aggregate Materials in the Western San Diego Production-Consumption Region, 1997) as an area of “Potential Mineral Resource Significance” (MRZ-3).

However, the proposed project site is surrounded by developed land uses including residential and commercial uses which are incompatible to future extraction of mineral resources on the project site. A future mining operation at the proposed project site would likely create a significant impact to neighboring properties for issues such as noise, air quality, traffic, and possibly other impacts. Therefore, implementation of the project would not result in the loss of availability of a known mineral resource that would be of value since the mineral resource has already been lost due to incompatible land uses.

b) Result in the loss of availability of a locally-important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?

- | | |
|---|---|
| <input type="checkbox"/> Potentially Significant Impact | <input type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input checked="" type="checkbox"/> No Impact |

No Impact

The proposed project site is not located in an area that has MRZ-2 designated lands or is located within 1,300 feet of such lands. Therefore, the proposed project would not result in the loss of availability of locally important mineral resource(s).

Therefore, no potentially significant loss of availability of a known mineral resource of locally important mineral resource recovery (extraction) site delineated on a local general plan, specific plan or other land use plan would occur as a result of the proposed project.

XIII. NOISE -- Would the project result in:

a) Generation of a substantial temporary or permanent increase in ambient noise levels in the vicinity of the project in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?

- | | |
|---|---|
| <input type="checkbox"/> Potentially Significant Impact | <input type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input type="checkbox"/> No Impact |

Less Than Significant Impact

The proposed project is a charter high school and would be occupied by students, faculty and staff. Based on a site visit completed by County staff on December 4, 2015 and as described in the Noise Analysis prepared by Jeremy Loudon (August 3, 2017), the surrounding area supports residential and commercial uses. The proposed project would not expose people to potentially significant noise levels that exceed the allowable limits of the County of San Diego General Plan, County of San Diego Noise Ordinance, and other applicable standards for the following reasons:

General Plan – Noise Element

The County of San Diego General Plan, Noise Element, Tables N-1 and N-2 addresses noise sensitive areas and requires an acoustical study to be prepared for any use that may expose noise sensitive areas to noise in excess of a Community Noise Equivalent Level (CNEL) of 60 decibels (dBA). Moreover, if the project is excess of 60 dBA CNEL or 65 dBA CNEL, modifications must be made to the project to reduce noise levels. Noise sensitive areas include residences, hospitals, schools, libraries or similar facilities as mentioned within Tables N-1 and N-2. The project is a Major Use Permit for Liberty Charter High School that is comprised of a 48,000-square foot, two-story building for classrooms, administrative offices and a gymnasium. A sports field would be located east of the proposed building, on the eastern portion of the project site. Based on the noise report prepared by Jeremy Loudon (August 3, 2017), future noise levels at the proposed exterior active sports field are anticipated to be as high as 70 dBA CNEL, which is in conformance with the Table N-1 noise exposure thresholds pursuant to the County Noise Element. The proposed project is also subject to interior sound levels for proposed schools, and noise exposure should be no greater than 50 dBA Leq pursuant to Tables N-1 and N-2, No. 3. This threshold is applicable to uses usually occupied part of the day such as a school classroom. Interior levels are subject to an interior noise level of 50 dBA Leq. The project would demonstrate conformance with the 50-dBA interior if windows were improved to a specific STC rating (e.g. STC 26 rated dual pane windows). This level of building plan detail would be addressed prior to building permits. This would ensure that building plans associated with the project demonstrate conformance with the interior sound level requirements pursuant to the County Noise Element.

Project implementation would not expose existing or planned noise sensitive areas to road, airport, heliport, railroad, industrial or other noise in excess of the 60 dBA CNEL or 65 dBA CNEL. Therefore, the project would not expose people to potentially significant noise levels that exceed the allowable limits of the County of San Diego General Plan, Noise Element.

Direct and cumulative noise impacts to existing nearby residences were also evaluated. Project related traffic on nearby roadways would not have a substantial noise contribution to these roads and would not result in an off-site direct/cumulative noise impact at existing residences. Therefore, the project would not expose people to potentially significant noise levels that exceed the allowable limits of the County of San Diego General Plan, Noise Element.

Noise Ordinance – Section 36.404

Based on a Noise Analysis prepared by Jeremy Loudon (August 3, 2017), non-transportation noise generated by the project is not expected to exceed the standards of the County of San Diego Noise Ordinance (Section 36.404) at or beyond the project's property line. The site is zoned Rural Residential (RR) that has a one-hour average sound daytime limit of 50 dBA and nighttime limit of 45 dBA. The adjacent properties are zoned Rural Residential (RR) and General Commercial (C36) and have one-hour average daytime sound limit of 50 dBA and 60 dBA and nighttime sound limit of 45 dBA and 55 dBA, respectively. The Noise Analysis state's the project's noise levels to the nearest property lines are not expected to exceed the standards of the County of San Diego Noise Ordinance (Section 36.404) with noise measures incorporated. With the noise measures, the cumulative noise levels from the non-transportation sources would not exceed 45 dBA, and therefore complies with the County Noise Standards.

The proposed project is zoned residential and is subject to the one-hour average daytime sound level limit of 50 dBA and nighttime 45 dBA at the project property lines. Noise sources associated with the project involve HVAC systems and an outdoor school bell system. The proposed rooftop HVAC units would be visually screened by a parapet barrier which would also function as a noise control feature. This parapet barrier design would reduce levels from the HVAC unit. The proposed HVAC units would comply with County noise standards with the implementation of the parapet noise measure design. The school bell system is anticipated to operate for 4-5 seconds in duration, with a maximum of two occurrences in any given hour during daytime hours from 7:00 a.m. to 10:00 p.m. The one-hour average sound level limit from both the bell system and mechanical HVAC unit would be a high as 48.3 dBA, in compliance with the daytime sound level requirement pursuant to the County Noise Ordinance, Section 36.404. With the incorporation of noise measures, the proposed project's noise levels at the adjoining properties would not exceed County Noise Standards.

Noise Ordinance – Section 36.409

Based on a Noise Analysis prepared by Jeremy Loudon (August 3, 2017), the proposed project would not generate construction noise that may exceed the standards of the County of San Diego Noise Ordinance (Section 36.409). Construction noise was assessed and is subject to the County's eight-hour average sound level limit of 75 dBA at any occupied boundaries. Based on the noise report and the noise model, a dozer, tractor/backhoe, loader, grader, and water truck were used to calculate a conservative noise source at one single location of 50 feet. Although not physically possible, this would introduce a conservative combined highest construction noise levels of 80.6 dBA at a reference distance of 50 feet. Based on noise attenuation by distance, at a distance of 100 feet would reduce levels to 74.6 dBA. Grading equipment operations would be spread out over the entire site as far as 400 feet away from the applicable property lines. Grading activities within 100 feet of the western and southern property lines occur intermittently and is limited to the slope preparation for the parking lot with a single piece of equipment in operation at one location. The majority of the grading operation would occur more than 100 feet

from the property lines. Based on the Noise Report’s quantitative assessment, it is not anticipated that temporary construction noise would exceed the 75 dBA eight-hour average requirement. In addition, construction operations would occur only during permitted hours of operation pursuant to Section 36.409. Also, it is not anticipated that the proposed project would operate construction equipment in excess of an average sound level of 75dB between the hours of 7 AM and 7 PM.

Finally, the proposed project’s conformance to the County of San Diego General Plan and County of San Diego Noise Ordinance (Section 36-404 and 36.410) ensures the project would not create cumulatively considerable noise impacts, because the project would not exceed the local noise standards for noise sensitive areas; and the proposed project would not exceed the applicable noise level limits at the property line or construction noise limits, derived from State regulation to address human health and quality of life concerns. Therefore, the proposed project would not contribute to a cumulatively considerable exposure of persons or generation of noise levels in excess of standards established in the local general plan, noise ordinance, and applicable standards of other agencies.

b) Generation of excessive groundborne vibration or groundborne noise levels?

- | | |
|---|--|
| <input type="checkbox"/> Potentially Significant Impact | <input checked="" type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input type="checkbox"/> No Impact |

Less Than Significant Impact

A charter high school is proposed where low ambient vibration and quiet during daytime use is important. The facilities include a setback of more than 50 feet from County Circulation Element (CE) roadways that use rubber-tired vehicles with projected groundborne noise or vibration contours of 43 VdB or less. A setback of 50 feet from the roadway centerline for heavy-duty truck activities would insure that these proposed uses or operations would not be impacted significantly by groundborne vibration or groundborne noise levels (Harris, Miller and Hanson Inc., *Transit Noise and Vibration Impact Assessment* 2006, Carl E. Hanson, David A. Towers, & Lance D. Meiser). This setback ensures that this proposed project site would not be affected by any future projects that may support sources of groundborne vibration or groundborne noise related to the adjacent roadways. In addition, the project site is not located near or adjacent to any property line for parcels that are zoned industrial or extractive uses or located near any permitted extractive uses.

Also, the proposed project does not propose any major, new or expanded infrastructure such as mass transit, highways or major roadways or intensive extractive industry that could generate excessive groundborne vibration or groundborne noise levels and impact vibration sensitive uses in the surrounding area. Therefore, the proposed project would not expose persons to or generate excessive groundborne vibration or groundborne noise levels on a project or cumulative level.

c) For a project located within the vicinity of a private airstrip or an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?

- | | |
|---|---|
| <input type="checkbox"/> Potentially Significant Impact | <input type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input checked="" type="checkbox"/> No Impact |

No Impact

The proposed project is not located within a one-mile vicinity of a private airstrip and is not located within an Airport Land Use Compatibility Plan (ALUCP) for airports or within 2 miles of a public airport or public use airport. Therefore, the project would not expose people residing or working in the project area to excessive airport-related noise levels.

In addition, based on the list of past, present and future projects there are no new or expanded public airports projects in the vicinity that may extend the boundaries of the CNEL 60 dB noise contour. Refer to XXI. Mandatory Findings of Significance for a comprehensive list of the projects considered. Therefore, the proposed project would not expose people residing or working in the project area to excessive airport-related noise on a project or cumulative level.

XIV. POPULATION AND HOUSING -- Would the project:

a) Induce substantial unplanned population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)?

- | | |
|---|---|
| <input type="checkbox"/> Potentially Significant Impact | <input type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input checked="" type="checkbox"/> No Impact |

No Impact

The proposed project would not induce substantial population growth in an area because the proposed project does not propose any physical or regulatory change that would remove a restriction to or encourage population growth in an area including, but limited to the following: new or extended infrastructure or public facilities; new commercial or industrial facilities; large-scale residential development; accelerated conversion of homes to commercial or multi-family use; or regulatory changes including General Plan amendments, specific plan amendments, zone reclassifications, sewer or water annexations; or LAFCO annexation actions.

b) Displace substantial numbers of existing people or housing, necessitating the construction of replacement housing elsewhere?

- | | |
|---|---|
| <input type="checkbox"/> Potentially Significant Impact | <input type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input checked="" type="checkbox"/> No Impact |

No Impact

The proposed project would not displace any people or existing housing since the site is currently vacant.

XV. PUBLIC SERVICES

a) Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance service ratios, response times or other performance objectives for any of the public services:

- i. Fire protection?
- ii. Police protection?
- iii. Schools?
- iv. Parks?
- v. Other public facilities?

- | | |
|---|---|
| <input type="checkbox"/> Potentially Significant Impact | <input type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input checked="" type="checkbox"/> No Impact |

No Impact

Based on the service availability forms received for the proposed project, there would be no anticipated need for significantly altered services or facilities. Service availability forms have been provided which indicate existing services are available to the proposed project from the following agencies/districts: Otay Water District and the San Miguel Consolidated Fire Protection District. The project does not involve the construction of new or physically altered governmental facilities including but not limited to fire protection facilities, sheriff facilities, schools, or parks in order to maintain acceptable service ratios, response times or other performance service ratios or objectives for any public services. Therefore, the proposed project would not have an adverse physical effect on the environment because the proposed project would not require new or significantly altered services or facilities to be constructed.

XVI. RECREATION

a) Would the project increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?

- | | |
|---|---|
| <input type="checkbox"/> Potentially Significant Impact | <input type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input checked="" type="checkbox"/> No Impact |

No Impact

The proposed project does not propose any residential use, including but not limited to a residential subdivision, mobile home park, or construction for a single-family residence that may increase the use of existing neighborhood and regional parks or other recreational facilities in the vicinity.

b) Does the project include recreational facilities or require the construction or expansion of recreational facilities, which might have an adverse physical effect on the environment?

- | | |
|---|---|
| <input type="checkbox"/> Potentially Significant Impact | <input type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input checked="" type="checkbox"/> No Impact |

No Impact

The proposed project does not include recreational facilities or require the construction or expansion of recreational facilities. Therefore, the construction or expansion of recreational facilities cannot have an adverse physical effect on the environment.

XVII. TRANSPORTATION AND TRAFFIC -- Would the project:

a) Conflict with a program, plan, ordinance or policy addressing the circulation system, including transit, roadway, bicycle and pedestrian facilities?

- | | |
|--|---|
| <input type="checkbox"/> Potentially Significant Impact | <input type="checkbox"/> Less than Significant Impact |
| <input checked="" type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input type="checkbox"/> No Impact |

The County of San Diego Guidelines for Determining Significance for Traffic and Transportation (Guidelines) establish measures of effectiveness for the performance of the circulation system. These Guidelines incorporate standards from the County of San Diego Public Road Standards, Mobility Element, and the Transportation Impact Fee Program.

Less than Significant Impact with Mitigation Incorporated: A Focused Traffic Impact Study, dated November 2018, was prepared by Kimley Horn that evaluated the potential traffic-related impacts associated with the construction of the new charter school.

The proposed project would generate a total of 585 new daily trips, including 117 (82 in, 35 out) morning peak-hour trips, 99 (33 in, 66 out) afternoon (school traffic) peak-hour trips, and 59 (23 in, 36 out) afternoon (commuter traffic) peak-hour trips.

Based on the County of San Diego criteria for determining traffic related impact, the proposed project would have a direct traffic related impact along Chase Avenue between the westernmost driveway and Jamacha Road. To mitigate this traffic direct impact, the proposed project would widen Chase Avenue to provide a second eastbound lane and would provide sufficient space to accommodate a westbound left-turn lane onto the site. In addition, a raised median will be added. The roadway widening is consistent with Chase Avenue ultimate classification per the Valle de Oro Mobility Element, which states that Chase Avenue is classified as a 4.1B Major Road with Bike Lanes.

The proposed project does not have a traffic related impact at intersections within the study area.

The County’s Transportation Impact Fee (TIF) Program/Ordinance provides a mechanism for projects to mitigate cumulative impacts with a “fair share” fee payment. The TIF Program

identifies transportation facilities needed to address cumulative impacts caused by future growth. TIF payments are divided into funds for the local Area, Regional, State Highway and Ramps and, if applicable, the Regional Transportation Congestion Improvement Program (RTCIP) to account for future transportation improvement projects. The Liberty Charter High School project is located within the Valle de Oro local fee area within the South region. Payment of TIF mitigates cumulative impacts to less than significant.

b) Would the project conflict or be inconsistent with CEQA Guidelines section 15064.3, subdivision (b)?

- | | |
|---|---|
| <input type="checkbox"/> Potentially Significant Impact | <input type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less than Significant with Mitigation Incorporated | <input checked="" type="checkbox"/> No Impact |

Discussion/Explanation:

Section 15064.3 of the CEQA Guidelines details new regulations, effective statewide July 1, 2020 that sets forth specific considerations for evaluating a project’s transportation impacts. Generally, vehicle miles traveled (VMT) is the most appropriate measure of transportation impacts. VMT refers to the amount and distance of automobile travel attributable to a project. Other relevant considerations may include the effects of the project on transit and non-motorized travel. Except as provided regarding roadway capacity, a project’s effect on automobile delay shall not constitute a significant environmental impact.

No Impact: The County of San Diego has not adopted a threshold for VMT and is not expected to until July 2020, when the provisions of the section apply statewide. Since the VMT threshold is yet to be adopted by the County, no impact would occur.

c) Substantially increase hazards due to a geometric design feature (e.g., sharp curves, or dangerous intersections) or incompatible uses (e.g., farm equipment)?

- | | |
|---|--|
| <input type="checkbox"/> Potentially Significant Impact | <input checked="" type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less than Significant with Mitigation Incorporated | <input type="checkbox"/> No Impact |

Less than Significant Impact: The proposed project includes widening Chase Avenue; however, the segment of roadway does not include any curves or dangerous intersections that would directly or cumulatively increase hazards due to a geometric design feature or incompatible uses.

d) Result in inadequate emergency access?

- | | |
|---|---|
| <input type="checkbox"/> Potentially Significant Impact | <input type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less than Significant with Mitigation Incorporated | <input checked="" type="checkbox"/> No Impact |

No Impact: The proposed project would not result in inadequate emergency access. The proposed project is not served by a dead-end road that exceeds the maximum cumulative length permitted by the San Diego County Consolidated Fire Code, therefore, the proposed project has adequate emergency access. Additionally, roads used to access the proposed project site meet all County standards.

XVII. TRIBAL CULTURAL RESOURCES -- Would the project:

a) Cause a substantial adverse change in the significance of a tribal cultural resource, as defined in Public Resources Code §21074 as either a site, feature, place, or cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native American tribe, and that is:

i. Listed or eligible for listing in the California Register of Historical Resources, or in a local register of Historical Resources as defined in Public Resources Code §5020.1(k), or

- | | |
|---|---|
| <input type="checkbox"/> Potentially Significant Impact | <input type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input checked="" type="checkbox"/> No Impact |

ii. A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Public Resources Code §5024.1. In applying the criteria set forth in subdivision (c) of Public Resources Code §5024.1, the Lead Agency shall consider the significance of the resource to a California Native American tribe.

- | | |
|---|---|
| <input type="checkbox"/> Potentially Significant Impact | <input type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input checked="" type="checkbox"/> No Impact |

No Impact

Pursuant to AB-52, consultation was initiated with culturally affiliated tribes. No tribal cultural resources were identified during consultation. As such, would be no impacts to tribal cultural resources.

XVIII. UTILITIES AND SERVICE SYSTEMS -- Would the project:

a) Require or result in the relocation or construction of new or expanded water, wastewater treatment or storm water drainage, electric power, natural gas, or telecommunications facilities, the construction or relocation of which would cause significant environmental effects?

- | | |
|---|--|
| <input type="checkbox"/> Potentially Significant Impact | <input checked="" type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input type="checkbox"/> No Impact |

Less Than Significant Impact

The proposed project does not include new or expanded water or wastewater treatment facilities or the construction of such facilities. The service availability forms received do not require the construction of new or expanded water or wastewater treatment facilities. Service availability forms have been provided which indicate adequate water and wastewater treatment facilities are available to serve the project from the following agencies/districts: Otay Water District.

The proposed project would construct new stormwater facilities (infiltration basin and trench). Impacts for the construction of these facilities were evaluated. See Section IV - Biological Resources for a discussion of required mitigation measures. Electric power, natural gas, and telecommunications would be provided by existing facilities near the proposed project site. Therefore, with the inclusion of mitigation (biological resources) the proposed project would have a less than significant impact associated with the construction of new or expanded facilities.

b) Have sufficient water supplies available to serve the project and reasonably foreseeable future development during normal, dry and multiple dry years?

- | | | | |
|--------------------------|--|-------------------------------------|------------------------------|
| <input type="checkbox"/> | Potentially Significant Impact | <input checked="" type="checkbox"/> | Less than Significant Impact |
| <input type="checkbox"/> | Less Than Significant With Mitigation Incorporated | <input type="checkbox"/> | No Impact |

Less Than Significant Impact

The proposed project requires water service from the Otay Water District. A Service Availability Letter from the Otay Water District has been provided, indicating adequate water resources and entitlements are available to serve the requested water resources. Therefore, the proposed project would have sufficient water supplies available to serve the project.

c) Result in a determination by the wastewater treatment provider, which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?

- | | | | |
|--------------------------|--|-------------------------------------|------------------------------|
| <input type="checkbox"/> | Potentially Significant Impact | <input checked="" type="checkbox"/> | Less than Significant Impact |
| <input type="checkbox"/> | Less Than Significant With Mitigation Incorporated | <input type="checkbox"/> | No Impact |

Less Than Significant Impact

The proposed project requires wastewater service from the Otay Water District. A Service Availability Letter from the Otay Water District has been provided, indicating adequate wastewater service capacity is available to serve the requested demand. Therefore, the proposed project would not interfere with any wastewater treatment provider's service capacity.

d) Generate solid waste in excess of State or Local standards, or in excess of the capacity of local infrastructure, or otherwise impair the attainment of solid waste reduction goals?

- | | | | |
|--------------------------|--|-------------------------------------|------------------------------|
| <input type="checkbox"/> | Potentially Significant Impact | <input checked="" type="checkbox"/> | Less than Significant Impact |
| <input type="checkbox"/> | Less than Significant with Mitigation Incorporated | <input type="checkbox"/> | No Impact |

Less Than Significant Impact: Implementation of the proposed project would generate solid waste. All solid waste facilities, including landfills require solid waste facility permits to operate. In San Diego County, the County Department of Environmental Health, Local Enforcement Agency issues solid waste facility permits with concurrence from the California Integrated Waste Management Board (CIWMB) under the authority of the Public Resources Code (Sections 44001-44018) and California Code of Regulations Title 27, Division 2, Subdivision 1, Chapter 4 (Section 21440 et seq.). There are five, permitted active landfills in San Diego County with remaining capacity. Therefore, there is sufficient existing permitted solid waste capacity to accommodate the proposed project's solid waste disposal needs.

e) Comply with federal, state, and local statutes and regulations related to solid waste?

- | | |
|---|--|
| <input type="checkbox"/> Potentially Significant Impact | <input checked="" type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input type="checkbox"/> No Impact |

Less than Significant Impact

Implementation of the proposed project would generate solid waste. All solid waste facilities, including landfills require solid waste facility permits to operate. In San Diego County, the County Department of Environmental Health, Local Enforcement Agency issues solid waste facility permits with concurrence from the California Integrated Waste Management Board (CIWMB) under the authority of the Public Resources Code (Sections 44001-44018) and California Code of Regulations Title 27, Division 2, Subdivision 1, Chapter 4 (Section 21440 et seq.). The proposed project would deposit all solid waste at a permitted solid waste facility and therefore, would comply with Federal, State, and local statutes and regulations related to solid waste.

f) Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board?

- | | |
|---|---|
| <input type="checkbox"/> Potentially Significant Impact | <input type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input checked="" type="checkbox"/> No Impact |

No Impact: The proposed project does not involve any uses that would discharge any wastewater to sanitary sewer or on-site wastewater systems (septic). Therefore, the proposed project would not exceed any wastewater treatment requirements.

g) Require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?

- | | |
|--|---|
| <input type="checkbox"/> Potentially Significant Impact | <input type="checkbox"/> Less than Significant Impact |
| <input checked="" type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input type="checkbox"/> No Impact |

Less Than Significant With Mitigation Incorporated

The proposed project involves new and/or expanded storm water drainage facilities. The new and/or expanded facilities include an infiltration basin and trench. However, as outlined in this Environmental Analysis Form, the new and/or expanded facilities would not result in adverse physical effect on the environment, because all related impacts from the proposed storm water facilities have been mitigated to a level below significance. Refer to Section IV - Biological Resources for more information.

XX. WILDFIRE: --If located in or near state responsibility areas or lands classified as very high fire hazard severity zones, would the project:

a) Substantially impair an adopted emergency response plan or emergency evacuation plan?

- | | |
|---|--|
| <input type="checkbox"/> Potentially Significant Impact | <input checked="" type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less than Significant with Mitigation Incorporated | <input type="checkbox"/> No Impact |

Less than Significant Impact

The proposed project is not located in a moderate, high or very high fire hazard severity zone. The project is located entirely within the "urban unzoned" fire hazard severity zone. Lands east and southeast of the proposed project site are within the moderate, high and very high fire hazard severity zones; however, these lands are over one mile from the project site. However, the proposed project does not substantially impair any adopted emergency response plan or emergency evacuation plan and would be serviced by the San Miguel Fire Protection District. Pursuant to the fire service availability form submitted for the project, San Miguel Fire Protection District has indicated the proposed project is eligible for service and nearest fire station is located 0.2 miles from the proposed project. Therefore, the proposed project would not substantially impair an adopted emergency response plan or emergency evacuation plan.

b) Due to slope, prevailing winds, and other factors, exacerbate wildfire risks, and thereby expose project occupants to, pollutant concentrations from a wildfire or the uncontrolled spread of a wildfire?

- | | |
|---|---|
| <input type="checkbox"/> Potentially Significant Impact | <input type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less than Significant with Mitigation Incorporated | <input checked="" type="checkbox"/> No Impact |

No Impact

As indicated above in response a), the proposed project is not located within a high or very high fire hazard severity zone, and thus a fire protection plan is not required. The proposed project would not exacerbate wildfire risk due to slope, prevailing winds or other factors because the project site is relatively flat and is an infill development located near existing commercial and residential use types with minimal vegetation. The project will however be required to meet applicable fire measures such as fire sprinklers, fire hydrants, fire alarm system, fire apparatus access, access road requirements, emergency access, and fire clearing around all structures. Additionally, the San Miguel Fire Protection District has indicated the availability to serve the site

in the case that a fire would occur. The nearest fire station is located 4 minutes from the project site.

- c) Require the installation or maintenance of associated infrastructure (such as roads, fuel breaks, emergency water sources, power lines or other utilities) that may exacerbate fire risk or that may result in temporary or ongoing impacts to the environment?

- | | |
|---|---|
| <input type="checkbox"/> Potentially Significant Impact | <input type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less than Significant with Mitigation Incorporated | <input checked="" type="checkbox"/> No Impact |

No Impact

The proposed project site is not located within a high or very high fire severity zone. The proposed project is an infill development and as identified in response b), generalized fire safety measures would be required. No installation or maintenance of associated infrastructure, such as roads, fuel breaks, or emergency water sources, power lines or other utilities would be required for the project. As such, impacts would be less than significant.

- d) Expose people or structures to significant risks, including downslope or downstream flooding or landslides, as a result of runoff, post-fire instability, or drainage changes?

- | | |
|---|--|
| <input type="checkbox"/> Potentially Significant Impact | <input checked="" type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less than Significant with Mitigation Incorporated | <input type="checkbox"/> No Impact |

Less than Significant Impact

The proposed project would not expose people or structures to significant risks, including downslope or downstream flooding or landslides because the project is located on land which is relatively flat and is an infill development. Additionally, the proposed project site is not located within a "Landslide Susceptibility Area" as identified in the County Guidelines for Determining Significance for Geologic Hazards. A slope analysis for the project was prepared by James Roberts, RCE (March 15, 2016) and it was determined that the average existing land slope is 10.7 percent indicating that the project site is generally flat. As such, the potential for landslides is considered low.

The proposed project site is also not located within a floodplain, wetland, or significant drainage feature. In order to assure that any proposed buildings are adequately supported, a Soils Engineering Report is required as part of the Building Permit process. This Report would evaluate the strength of underlying soils and make recommendations on the design of building foundation systems. The Soils Engineering Report must demonstrate that a proposed building meets the structural stability standards required by the California Building Code. Lastly, the Drainage Study by Scott Harry, P.E., P.L.S. (KARN Engineering and Surveying, Inc. [April 30, 2017]), determined that the proposed project would not alter the existing drainage pattern in a manner which would result in flooding on- or off-site. Therefore, due to the above stated reasons, the proposed project would not expose people or structures to significant risks, including downslope or downstream flooding or landslides, as a result of runoff, post-fire instability or drainage changes. Impacts would be less than significant.

XXI. MANDATORY FINDINGS OF SIGNIFICANCE:

a) Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, substantially reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?

- | | |
|--|---|
| <input type="checkbox"/> Potentially Significant Impact | <input type="checkbox"/> Less than Significant Impact |
| <input checked="" type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input type="checkbox"/> No Impact |

Less Than Significant With Mitigation Incorporated

Per the instructions for evaluating environmental impacts in this Initial Study, the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory were considered in the response to each question in sections IV and V of this form. In addition to project specific impacts, this evaluation considered the projects potential for significant cumulative effects. Resources that have been evaluated as significant would be potentially impacted by the project, particularly biological resources. However, mitigation has been included that clearly reduces these effects to a level below significance. This mitigation includes the offsite purchase of Non-native grassland habitat. As a result of this evaluation, there is no substantial evidence that, after mitigation, significant effects associated with this proposed project would result. Therefore, this project has been determined not to meet this Mandatory Finding of Significance.

b) Does the project have impacts that are individually limited, but cumulatively considerable? (“Cumulatively considerable” means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects)?

- | | |
|---|--|
| <input type="checkbox"/> Potentially Significant Impact | <input checked="" type="checkbox"/> Less than Significant Impact |
| <input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input type="checkbox"/> No Impact |

Less Than Significant Impact

The following list of past, present and future projects were considered and evaluated as a part of this Initial Study:

PROJECT NAME	PERMIT/MAP NUMBER
St. Gregory of Nyssa Major Use Permit	PDS2005-3300-05-010
Winchester Ranch Tentative Map	PDS2010-3100-4416
Brayton Way Tentative Parcel Map	PDS2005-3200-20918
Law Tentative Parcel Map	PDS2006-3200-20991
Drysdale’s Boulder and Landscape	PDS2003-3300-03-060

Avocado Ranch Road Tentative Parcel Map	PDS2017-TPM-21253
Fuerte Tentative Parcel Map	PDS2018-TPM-21261
Dawson Subdivision Tentative Map	PDS2000-3100-5157
Lynn Lot Split Tentative Parcel Map	PDS2016-TPM-21236

Per the instructions for evaluating environmental impacts in this Initial Study, the potential for adverse cumulative effects were considered in the response to each question in sections I through XX of this form. In addition to project specific impacts, this evaluation considered the projects potential for incremental effects that are cumulatively considerable. As a result of this evaluation, there is no substantial evidence that there are cumulative effects associated with this project. Therefore, this project has been determined not to meet this Mandatory Finding of Significance.

c) Does the project have environmental effects, which will cause substantial adverse effects on human beings, either directly or indirectly?

- | | |
|--|--|
| <input type="checkbox"/> Potentially Significant Impact
<input type="checkbox"/> Less Than Significant With Mitigation Incorporated | <input type="checkbox"/> Less than Significant Impact
<input checked="" type="checkbox"/> No Impact |
|--|--|

In the evaluation of environmental impacts in this Initial Study, the potential for adverse direct or indirect impacts to human beings were considered in the response to certain questions in sections I. Aesthetics, III. Air Quality, VI. Geology and Soils, VIII. Hazards and Hazardous Materials, IX Hydrology and Water Quality XII. Noise, XIII. Population and Housing, and XVI. Transportation and Traffic. As a result of this evaluation, there is no substantial evidence that there are adverse effects on human beings associated with this project. Therefore, this project has been determined not to meet this Mandatory Finding of Significance.

XXII. REFERENCES USED IN THE COMPLETION OF THE INITIAL STUDY CHECKLIST

All references to Federal, State and local regulation are available on the Internet. For Federal regulation refer to <http://www4.law.cornell.edu/uscode/>. For State regulation refer to www.leginfo.ca.gov. For County regulation refer to www.amlegal.com. All other references are available upon request or on the County CEQA Public Review website at https://www.sandiegocounty.gov/content/sdc/pds/ceqa_public_review.html.

AESTHETICS

- California Street and Highways Code [California Street and Highways Code, Section 260-283. (<http://www.leginfo.ca.gov/>)
- California Scenic Highway Program, California Streets and Highways Code, Section 260-283. (<http://www.dot.ca.gov/hq/LandArch/scenic/scpr.htm>)
- County of San Diego, Planning & Development Services. The Zoning Ordinance of San Diego County. Sections 5200-5299; 5700-5799; 5900-5910, 6322-6326. (www.co.san-diego.ca.us)
- County of San Diego, Board Policy I-73: Hillside Development Policy. (www.co.san-diego.ca.us)
- County of San Diego, Board Policy I-104: Policy and Procedures for Preparation of Community Design Guidelines, Section 396.10 of the County Administrative Code and Section 5750 et seq. of the County Zoning Ordinance. (www.co.san-diego.ca.us)
- County of San Diego Light Pollution Code, Title 5, Division 9 (Sections 59.101-59.115 of the County Code of Regulatory Ordinances) as added by Ordinance No 6900, effective January 18, 1985, and amended July 17, 1986 by Ordinance No. 7155. (www.amlegal.com)
- County of San Diego Wireless Communications Ordinance [San Diego County Code of Regulatory Ordinances. (www.amlegal.com)
- Design Review Guidelines for the Communities of San Diego County. (Alpine, Bonsall, Fallbrook, Julian, Lakeside, Ramona, Spring Valley, Sweetwater, Valley Center).
- Federal Communications Commission, Telecommunications Act of 1996 [Telecommunications Act of 1996, Pub. LA. No. 104-104, 110 Stat. 56 (1996). (<http://www.fcc.gov/Reports/tcom1996.txt>)
- Institution of Lighting Engineers, Guidance Notes for the Reduction of Light Pollution, Warwickshire, UK, 2000 (<http://www.dark-skies.org/ile-gd-e.htm>)
- International Light Inc., Light Measurement Handbook, 1997. (www.intl-light.com)
- Rensselaer Polytechnic Institute, Lighting Research Center, National Lighting Product Information Program (NLPPI), Lighting Answers, Volume 7, Issue 2, March 2003. (www.lrc.rpi.edu)
- US Census Bureau, Census 2000, Urbanized Area Outline Map, San Diego, CA. (<http://www.census.gov/geo/www/maps/ua2kmaps.htm>)
- US Department of the Interior, Bureau of Land Management (BLM) modified Visual Management System. (www.blm.gov)
- US Department of Transportation, Federal Highway Administration (FHWA) Visual Impact Assessment for Highway Projects.

US Department of Transportation, National Highway System Act of 1995 [Title III, Section 304. Design Criteria for the National Highway System. (<http://www.fhwa.dot.gov/legsregs/nhsdatoc.html>)

AGRICULTURE RESOURCES

- California Department of Conservation, Farmland Mapping and Monitoring Program, "A Guide to the Farmland Mapping and Monitoring Program," November 1994. (www.consrv.ca.gov)
- California Department of Conservation, Office of Land Conversion, "California Agricultural Land Evaluation and Site Assessment Model Instruction Manual," 1997. (www.consrv.ca.gov)
- California Farmland Conservancy Program, 1996. (www.consrv.ca.gov)
- California Land Conservation (Williamson) Act, 1965. (www.ceres.ca.gov, www.consrv.ca.gov)
- California Right to Farm Act, as amended 1996. (www.qp.gov.bc.ca)
- County of San Diego Agricultural Enterprises and Consumer Information Ordinance, 1994, Title 6, Division 3, Ch. 4. Sections 63.401-63.408. (www.amlegal.com)
- County of San Diego, Department of Agriculture, Weights and Measures, "2002 Crop Statistics and Annual Report," 2002. (www.sdcounty.ca.gov)
- United States Department of Agriculture, Natural Resource Conservation Service LESA System. (www.nrcs.usda.gov, www.swcs.org).
- United States Department of Agriculture, Soil Survey for the San Diego Area, California. 1973. (soils.usda.gov)

AIR QUALITY

- CEQA Air Quality Analysis Guidance Handbook, South Coast Air Quality Management District, Revised November 1993. (www.aqmd.gov)
- County of San Diego Air Pollution Control District's Rules and Regulations, updated August 2003. (www.co.san-diego.ca.us)
- Federal Clean Air Act US Code; Title 42; Chapter 85 Subchapter 1. (www4.law.cornell.edu)

BIOLOGY

- California Department of Fish and Wildlife (CDFW). Southern California Coastal Sage Scrub Natural Community Conservation Planning Process Guidelines. CDFW and California Resources Agency, Sacramento, California. 1993. (www.dfg.ca.gov)
- County of San Diego, An Ordinance Amending the San Diego County Code to Establish a Process for Issuance of the Coastal Sage Scrub Habitat Loss Permits and Declaring the Urgency Thereof to Take Effect Immediately, Ordinance No.

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8365. 1994, Title 8, Div 6, Ch. 1. Sections 86.101-86.105, 87.202.2. (www.amlegal.com)
- County of San Diego, Biological Mitigation Ordinance, Ord. Nos. 8845, 9246, 1998 (new series). (www.co.san-diego.ca.us)
- County of San Diego, Implementing Agreement by and between United States Fish and Wildlife Service, California Department of Fish and Wildlife and County of San Diego. County of San Diego, Multiple Species Conservation Program, 1998.
- County of San Diego, Multiple Species Conservation Program, County of San Diego Subarea Plan, 1997.
- Holland, R.R. Preliminary Descriptions of the Terrestrial Natural Communities of California. State of California, Resources Agency, Department of Fish and Wildlife, Sacramento, California, 1986.
- Memorandum of Understanding [Agreement Between United States Fish and Wildlife Service (USFWS), California Department of Fish and Wildlife (CDFW), California Department of Forestry and Fire Protection (CDF), San Diego County Fire Chief's Association and the Fire District's Association of San Diego County.
- Stanislaus Audubon Society, Inc. v County of Stanislaus (5th Dist. 1995) 33 Cal.App.4th 144, 155-159 [39 Cal. Rptr.2d 54]. (www.ceres.ca.gov)
- U.S. Army Corps of Engineers Environmental Laboratory. Corps of Engineers Wetlands Delineation Manual. U.S. Army Corps of Engineers, Wetlands Research Program Technical Report Y-87-1. 1987. (<http://www.wes.army.mil/>)
- U.S. Environmental Protection Agency. America's wetlands: our vital link between land and water. Office of Water, Office of Wetlands, Oceans and Watersheds. EPA843-K-95-001. 1995b. (www.epa.gov)
- U.S. Fish and Wildlife Service and National Marine Fisheries Service. Habitat Conservation Planning Handbook. Department of Interior, Washington, D.C. 1996. (endangered.fws.gov)
- U.S. Fish and Wildlife Service and National Marine Fisheries Service. Consultation Handbook: Procedures for Conducting Consultation and Conference Activities Under Section 7 of the Endangered Species Act. Department of Interior, Washington, D.C. 1998. (endangered.fws.gov)
- U.S. Fish and Wildlife Service. Environmental Assessment and Land Protection Plan for the Vernal Pools Stewardship Project. Portland, Oregon. 1997.
- U.S. Fish and Wildlife Service. Vernal Pools of Southern California Recovery Plan. U.S. Department of Interior, Fish and Wildlife Service, Region One, Portland, Oregon, 1998. (ecos.fws.gov)
- U.S. Fish and Wildlife Service. Birds of conservation concern 2002. Division of Migratory. 2002. (migratorybirds.fws.gov)
- CULTURAL RESOURCES**
- California Health & Safety Code. §18950-18961, State Historic Building Code. (www.leginfo.ca.gov)
- California Health & Safety Code. §5020-5029, Historical Resources. (www.leginfo.ca.gov)
- California Health & Safety Code. §7050.5, Human Remains. (www.leginfo.ca.gov)
- California Native American Graves Protection and Repatriation Act, (AB 978), 2001. (www.leginfo.ca.gov)
- California Public Resources Code §5024.1, Register of Historical Resources. (www.leginfo.ca.gov)
- California Public Resources Code. §5031-5033, State Landmarks. (www.leginfo.ca.gov)
- California Public Resources Code. §5097-5097.6, Archaeological, Paleontological, and Historic Sites. (www.leginfo.ca.gov)
- California Public Resources Code. §5097.9-5097.991, Native American Heritage. (www.leginfo.ca.gov)
- City of San Diego. Paleontological Guidelines. (revised) August 1998.
- County of San Diego, Local Register of Historical Resources (Ordinance 9493), 2002. (www.co.san-diego.ca.us)
- Demere, Thomas A., and Stephen L. Walsh. Paleontological Resources San Diego County. Department of Paleontology, San Diego Natural History Museum. 1994.
- Moore, Ellen J. Fossil Mollusks of San Diego County. San Diego Society of Natural history. Occasional; Paper 15. 1968.
- U.S. Code including: American Antiquities Act (16 USC §431-433) 1906. Historic Sites, Buildings, and Antiquities Act (16 USC §461-467), 1935. Reservoir Salvage Act (16 USC §469-469c) 1960. Department of Transportation Act (49 USC §303) 1966. National Historic Preservation Act (16 USC §470 et seq.) 1966. National Environmental Policy Act (42 USC §4321) 1969. Coastal Zone Management Act (16 USC §1451) 1972. National Marine Sanctuaries Act (16 USC §1431) 1972. Archaeological and Historical Preservation Act (16 USC §469-469c) 1974. Federal Land Policy and Management Act (43 USC §35) 1976. American Indian Religious Freedom Act (42 USC §1996 and 1996a) 1978. Archaeological Resources Protection Act (16 USC §470aa-mm) 1979. Native American Graves Protection and Repatriation Act (25 USC §3001-3013) 1990. Intermodal Surface Transportation Efficiency Act (23 USC §101, 109) 1991. American Battlefield Protection Act (16 USC 469k) 1996. (www4.law.cornell.edu)
- GEOLOGY & SOILS**
- California Department of Conservation, Division of Mines and Geology, California Alquist-Priolo Earthquake Fault Zoning Act, Special Publication 42, Revised 1997. (www.consrv.ca.gov)
- California Department of Conservation, Division of Mines and Geology, Fault-Rupture Hazard Zones in California, Special Publication 42, revised 1997. (www.consrv.ca.gov)
- California Department of Conservation, Division of Mines and Geology, Special Publication 117, Guidelines for Evaluating and Mitigating Seismic Hazards in California, 1997. (www.consrv.ca.gov)
- County of San Diego Code of Regulatory Ordinances Title 6, Division 8, Chapter 3, Septic Tanks and Seepage Pits. (www.amlegal.com)
- County of San Diego Department of Environmental Health, Land and Water Quality Division, February 2002. On-site Wastewater Systems (Septic Systems): Permitting Process and Design Criteria. (www.sdcounty.ca.gov)
- County of San Diego Natural Resource Inventory, Section 3, Geology.
- United States Department of Agriculture, Soil Survey for the San Diego Area, California. 1973. (soils.usda.gov)
- HAZARDS & HAZARDOUS MATERIALS**
- American Planning Association, Zoning News, "Saving Homes from Wildfires: Regulating the Home Ignition Zone," May 2001.
- California Building Code (CBC), Seismic Requirements, Chapter 16 Section 162. (www.buildersbook.com)

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- California Education Code, Section 17215 and 81033. (www.leginfo.ca.gov)
- California Government Code. § 8585-8589, Emergency Services Act. (www.leginfo.ca.gov)
- California Hazardous Waste and Substances Site List. April 1998. (www.dtsc.ca.gov)
- California Health & Safety Code Chapter 6.95 and §25117 and §25316. (www.leginfo.ca.gov)
- California Health & Safety Code § 2000-2067. (www.leginfo.ca.gov)
- California Health & Safety Code. §17922.2. Hazardous Buildings. (www.leginfo.ca.gov)
- California Public Utilities Code, SDCRAA. Public Utilities Code, Division 17, Sections 170000-170084. (www.leginfo.ca.gov)
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(Sections 86.604(c) and (d)) of the Resource Protection Ordinance?	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
The Steep Slope section (Section 86.604(e))?	YES <input checked="" type="checkbox"/>	NO <input type="checkbox"/>	NOT APPLICABLE/EXEMPT <input type="checkbox"/>
The Sensitive Habitat Lands section (Section 86.604(f)) of the Resource Protection Ordinance?	YES <input checked="" type="checkbox"/>	NO <input type="checkbox"/>	NOT APPLICABLE/EXEMPT <input type="checkbox"/>
The Significant Prehistoric and Historic Sites section (Section 86.604(g)) of the Resource Protection Ordinance?	YES <input checked="" type="checkbox"/>	NO <input type="checkbox"/>	NOT APPLICABLE/EXEMPT <input type="checkbox"/>

Wetland and Wetland Buffers

The site contains no wetland habitats as defined by the San Diego County Resource Protection Ordinance. The site does not have a substratum of predominately undrained hydric soils, the land does not support, even periodically, hydric plants, nor does the site have a substratum that is non-soil and is saturated with water or covered by water at some time during the growing season of each year. Therefore, it has been found that the proposed project complies with Sections 86.604(a) and (b) of the Resource Protection Ordinance.

Floodways and Floodplain Fringe

The project is not located near any floodway or floodplain fringe area as defined in the resource protection ordinance, nor is it near a watercourse plotted on any official County floodway or floodplain map. Therefore, it has been found that the proposed project complies with Sections 86.604(c) and (d) of the Resource Protection Ordinance.

Steep Slopes

The average slope for the property is 10.7 percent gradient. Slopes with a gradient of 25 percent or greater and 50 feet or higher in vertical height are required to be placed in open space easements by the San Diego County Resource Protection Ordinance (RPO). There are no steep slopes on the property. Therefore, it has been found that the proposed project complies with Sections 86.604(e) of the RPO.

Sensitive Habitats

Sensitive habitat lands were identified on the site as determined on a site visit conducted by Elyssa Robertson on May 18, 2015. The project is conditioned to purchase offsite mitigation credits for the loss of Non-native grassland habitat. Therefore, it has been found that the proposed project complies with Section 86.604(f) of the RPO.

Significant Prehistoric and Historic Sites

The property has been surveyed by a County of San Diego approved archaeologist/historian, Jerry Schaefer, Ph.D., and it has been determined there is one historical site present. Testing and other investigation has determined that the historical site does not meet the definition of significant site. It does not need to be preserved under the Resource Protection Ordinance.

V. STORMWATER ORDINANCE (WPO) - Does the project comply with the County of San Diego Watershed Protection, Stormwater Management and Discharge Control Ordinance (WPO)?

YES

NO

NOT APPLICABLE

The project Storm Water Quality Management Plan and Hydromodification Management Study have been reviewed and are found to be complete and in compliance with the WPO.

VI. NOISE ORDINANCE – Does the project comply with the County of San Diego Noise Element of the General Plan and the County of San Diego Noise Ordinance?

YES

NO

NOT APPLICABLE

The proposal would not expose people to nor generate potentially significant noise levels which exceed the allowable limits of the County of San Diego Noise Element of the General Plan, County of San Diego Noise Ordinance, and other applicable local, State, and Federal noise control regulations.

Transportation (traffic, railroad, aircraft) noise levels at the project site are not expected to exceed Community Noise Equivalent Level (CNEL)=60 decibels (dB) limit because review of the project indicates that the project is not in close proximity to a railroad and/or airport. Additionally, the County of San Diego GIS noise model does not indicate that the project would be subject to potential excessive noise levels from circulation element roads either now or at General Plan buildout.

Noise impacts to the proposed project from adjacent land uses are not expected to exceed the property line sound level limits of the County of San Diego Noise Ordinance.

**MULTIPLE SPECIES CONSERVATION PROGRAM CONFORMANCE STATEMENT
For Liberty First Charter School
PDS2015-MUP-15-027
APN 498-330-39**

July 22, 2019

I. Introduction

The proposed project is a Major Use Permit for a Charter School on a 7.66 acre site. The project site is located at 1530 Jamacha Road in the Valle De Oro Community Plan area (APN 498-330-39), within unincorporated San Diego County. This area is within the Metro-Lakeside-Jamul segment of the County’s Multiple Species Conservation Program (MSCP). The site is currently vacant and single-family residential and commercial uses surround this site.

Biological resources on site were evaluated in a Biological Resources Letter Report (REC Consultants, Inc., February 25, 2016). The project site contains four habitat types: 5.81 acres of non-native grassland, 0.81 acre of developed land, 0.93 acre of disturbed land and 0.11 acre of non-native vegetation. No sensitive plant species and one sensitive wildlife species, a red-tailed hawk was observed on the project site. The project will impact the entire project site. The impact site does not qualify as Biological Resource Core Area (BRCA) and is not located in proximity to any Pre-Approved Mitigation Areas or undeveloped land.

Impacts to non-native grassland will require mitigation in accordance with the Biological Mitigation Ordinance (BMO), as listed in the Mitigated Negative Declaration. The purchase of habitat credits within a County-approved mitigation bank in the MSCP will be required prior to the approval of any permit.

Table 1. Impacts to Habitat and Required Mitigation

Habitat Type	Tier Level	Existing On-site (ac.)	Impact Neutral (ac.)	Proposed Impacts (ac.)	Mitigation Ratio	Required Mitigation
Non-Native Grassland (42200)	III	5.81	0.58	5.23	0.5:1	2.62
Developed Land (12000)	IV	0.81	0.23	0.58	--	--
Disturbed Land (11300)	IV	0.93	0.22	0.71	--	--
Non-Native Vegetation (11000)	IV	0.11	0.00	0.11	--	--
Total:	--	7.66	1.03	6.63	--	2.62

The findings contained within this document are based on County records, staff field site visits and the Biological Resources Letter Report (REC Consultants, Inc., February 25, 2016). The information contained within these Findings is correct to the best of staff’s knowledge at the time the findings were completed. Any subsequent environmental review completed due to changes in the proposed project or changes in

circumstance shall need to have new findings completed based on the environmental conditions at that time.

The project has been found to conform to the County's Multiple Species Conservation Program (MSCP) Subarea Plan, the Biological Mitigation Ordinance (BMO) and the Implementation Agreement between the County of San Diego, the CA Department of Fish and Wildlife and the US Fish and Wildlife Service. Third Party Beneficiary Status and the associated take authorization for incidental impacts to sensitive species (pursuant to the County's Section 10 Permit under the Endangered Species Act) shall be conveyed only after the project has been approved by the County, these MSCP Findings are adopted by the hearing body and all MSCP-related conditions placed on the project have been satisfied.

II. Biological Resource Core Area Determination

The impact area and the mitigation site shall be evaluated to determine if either or both sites qualify as a Biological Resource Core Area (BRCA) pursuant to the BMO, Section 86.506(a)(1).

A. Report the factual determination as to whether the proposed Impact Area qualifies as a BRCA. The Impact Area shall refer only to that area within which project-related disturbance is proposed, including any on and/or off-site impacts.

The Impact Area does not qualify as a BRCA since it does not meet any of the following BRCA criteria:

i. The land is shown as Pre-Approved Mitigation Area on the wildlife agencies' Pre-Approved Mitigation Area map.

The project site is not shown as being a Pre-Approved Mitigation Area (PAMA) on the MSCP County Subarea Plan maps. The nearest PAMA area is located 0.63 mile south of the project site.

ii. The land is located within an area of habitat that contains biological resources that support or contribute to the long-term survival of sensitive species and is adjacent or contiguous to preserved habitat that is within the Pre-Approved Mitigation Area on the wildlife agencies' Pre-Approved Mitigation Area map.

The project site is surrounded by existing residential and commercial development. Non-native grassland occurs on site but is not contiguous with any areas of biological open space nor any areas of PAMA or MSCP preserve lands.

iii. The land is part of a regional linkage/corridor. A regional linkage/corridor is either:

- a. Land that contains topography that serves to allow for the movement of all sizes of wildlife, including large animals on a regional scale; and contains adequate vegetation cover providing visual continuity so as to encourage the use of the corridor by wildlife; or**
- b. Land that has been identified as the primary linkage/corridor between the northern and southern regional populations of the California gnatcatcher in the population viability analysis for the California gnatcatcher, MSCP Resource Document Volume II, Appendix A-7 (Attachment I of the BMO.)**

The site is not part of a regional linkage/corridor as identified on MSCP maps nor is it in an area considered regionally important for wildlife dispersal. Surrounding properties are developed with existing residential and commercial uses.

iv. The land is shown on the Habitat Evaluation Map (Attachment J to the BMO) as very high or high and links significant blocks of habitat, except that land which is isolated or links small, isolated patches of habitat and land that has been affected by existing development to create adverse edge effects shall not qualify as BRCA.

The MSCP Habitat Evaluation Map identifies the site as agriculture and developed. The habitat on site is isolated and located within a developed residential and commercial area. The site does not function as a connection between large areas of undisturbed habitat.

v. The land consists of or is within a block of habitat greater than 500 acres in area of diverse and undisturbed habitat that contributes to the conservation of sensitive species.

The site is 7.66 acres and is surrounded by existing residential and commercial development. It is not contiguous to any large blocks of habitat.

vi. The land contains a high number of sensitive species and is adjacent or contiguous to surrounding undisturbed habitats, or contains soil derived from the following geologic formations which are known to support sensitive species:

- a. Gabbroic rock;**
- b. Metavolcanic rock;**
- c. Clay;**
- d. Coastal sandstone**

The site is not contiguous with undisturbed habitats. No sensitive plant species and one sensitive plant species, red-tailed hawk was observed on the project

site. The property has been mapped as Vista coarse sandy loam and Placentia sandy loam, which are not known to support sensitive species.

B. Report the factual determination as to whether the Mitigation Site qualifies as a BRCA.

The Mitigation Site is considered a BRCA because the off-site mitigation purchase will occur within a County-approved mitigation bank or at a site meeting the BRCA criteria.

III. Biological Mitigation Ordinance Findings

A. Project Design Criteria (Section 86.505(a))

The following findings in support of Project Design Criteria, including Attachments G and H (if applicable), must be completed for all projects that propose impacts to Critical Populations of Sensitive Plant Species (Attachment C), Significant Populations of Narrow Endemic Animal Species (Attachment D), Narrow Endemic Plant Species (Attachment E) or Sensitive Plants (San Diego County Rare Plant List) or proposes impacts within a Biological Resource Core Area.

The project does not propose impacts to any of the above resources or resource areas. BMO findings, including design criteria and Attachments G and H, are not applicable and have been excluded from this document.

IV. Subarea Plan Findings

Conformance with the objectives of the County Subarea Plan is demonstrated by the following findings:

1. The project will not conflict with the no-net-loss-of-wetlands standard in satisfying State and Federal wetland goals and policies.

No wetlands or wetland habitats are located on the project site. The project will not conflict with state and federal wetland goals or policies.

2. The project includes measures to maximize the habitat structural diversity of conserved habitat areas including conservation of unique habitats and habitat features.

The site does not support any unique habitats or habitat features. The loss of 5.23 acres of onsite non-native grassland will be mitigated by the preservation of 2.62 acres of Tier III or higher tier habitat within a County-approved mitigation bank.

3. The project provides for conservation of spatially representative examples of extensive patches of Coastal sage scrub and other habitat types that were ranked as having high and very high biological values by the MSCP habitat evaluation model.

The project does not propose to impact or conserve areas of high value habitat. The proposed project will result in the loss of 5.23 acres of non-native grassland which was mapped as agriculture and developed by the habitat evaluation model. The purchase of habitat credits within a County-approved mitigation bank will contribute to the conservation of higher value habitats within the MSCP area.

4. The project provides for the creation of significant blocks of habitat to reduce edge effects and maximize the ratio of surface area to the perimeter of conserved habitats.

The non-native grassland on-site will continue to be heavily impacted by edge effects from surrounding residential and commercial development. On-site conservation of habitat as part of the proposed project would only create a small block of habitat lacking connectivity to other blocks of habitat. Off-site mitigation as proposed by the project will facilitate preservation of large blocks of high quality habitat where edge effects are minimal and land is protected in perpetuity.

5. The project provides for the development of the least sensitive habitat areas.

The project will impact the entire site, including 5.23 acres of non-native grassland. Non-native grassland is considered sensitive within the MSCP. The habitat is fragmented and has minimal value for long-term conservation of sensitive plants or wildlife. Therefore, the development of this site is not considered an impact to sensitive habitat.

6. The project provides for the conservation of key regional populations of covered species, and representations of sensitive habitats and their geographic sub-associations in biologically functioning units.

No threatened, endangered, narrow endemic species were detected on the project site. Developing the site will not eliminate highly sensitive habitat or impact key populations of covered species.

7. Conserves large interconnecting blocks of habitat that contribute to the preservation of wide-ranging species such as Mule deer, Golden eagle, and predators as appropriate. Special emphasis will be placed on conserving adequate foraging habitat near Golden eagle nest sites.

The non-native grassland on the project site is isolated from any other habitat. The surrounding residential and commercial development has eliminated connection to larger, undisturbed areas in all directions. The site itself is too small for larger mammals and raptors to reside permanently. Wildlife that would be expected to travel across the property may include smaller species such as coyotes and ground squirrels that are adapted to residential neighborhoods.

- 8. All projects within the San Diego County Subarea Plan shall conserve identified critical populations and narrow endemics to the levels specified in the Subarea Plan. These levels are generally no impact to the critical populations and no more than 20 percent loss of narrow endemics and specified rare and endangered plants.**

No critical or narrow endemic species were detected on the site. Most sensitive species have a low potential to be present due to the existence of surrounding development.

- 9. No project shall be approved which will jeopardize the possible or probable assembly of a preserve system within the Subarea Plan.**

The project site is not within an area of regional significance with regard to conservation of sensitive species and habitats. The surrounding residential and commercial setting does not aid in conservation or wildlife dispersal. Developing the site will not hinder possible preserve systems.

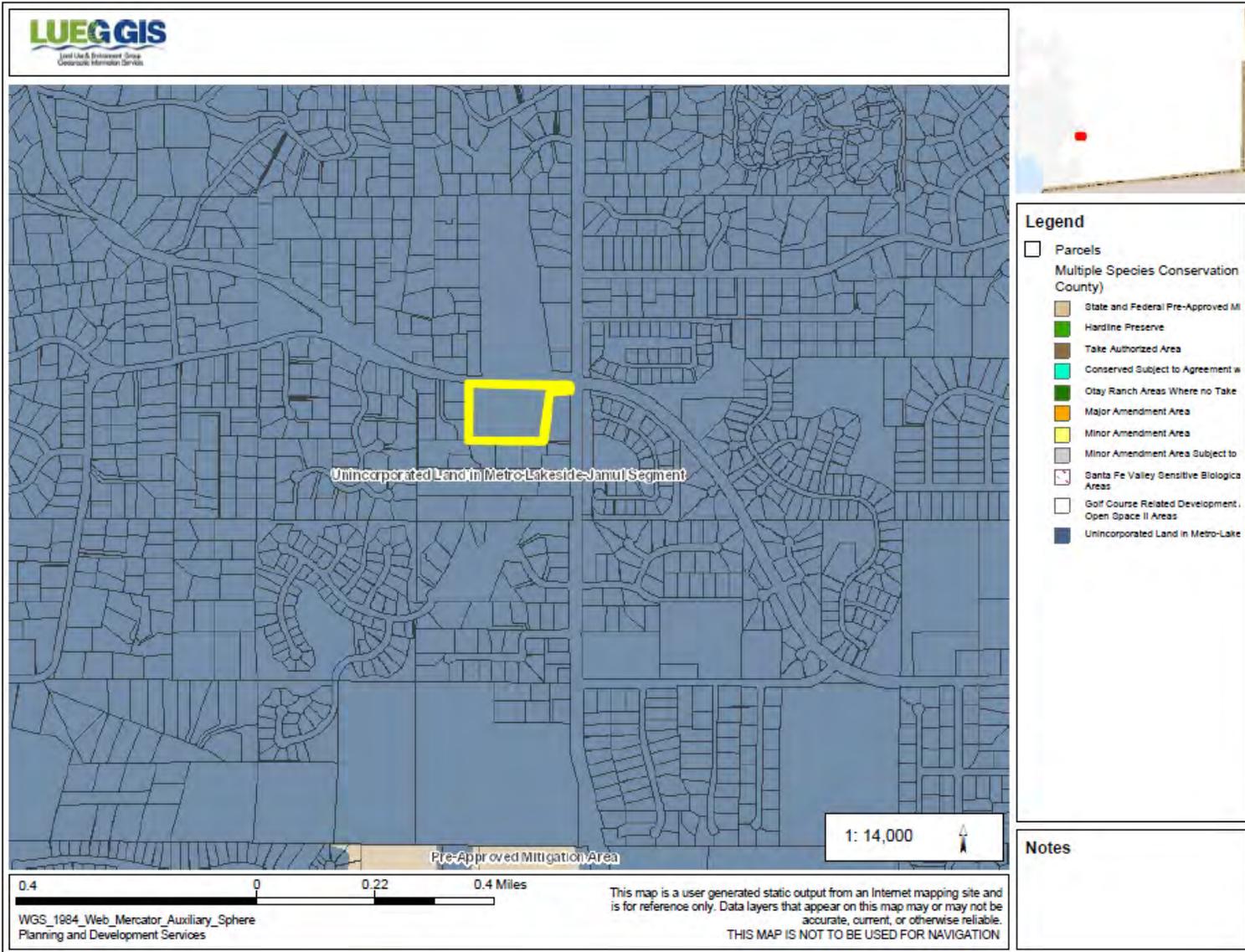
- 10. All projects that propose to count on-site preservation toward their mitigation responsibility must include provisions to reduce edge effects.**

The site does not propose on-site preservation of habitat. Proposed off-site mitigation will facilitate preservation of large blocks of high quality habitat where edge effects are minimal and land is protected in perpetuity.

- 11. Every effort has been made to avoid impacts to BRCAs, to sensitive resources, and to specific sensitive species as defined in the BMO.**

The project site is not within a Biological Resource Core Area. The site supports 5.81 acres of non-native grassland, 0.81 acre of developed land, 0.93 acre of disturbed land and 0.11 acre of non-native vegetation. Non-native grassland is a Tier III habitat and is considered sensitive within the MSCP. However, developing the entire site is not considered a significant impact to sensitive habitat because the small amount of habitat on site is surrounded by residential and commercial development in all directions. There were no threatened, endangered or narrow endemic species detected on-site. No significant populations of sensitive species are expected to reside on the property due to its disturbed condition and the surrounding land uses. The project site as a whole is not considered sensitive, nor are there any particularly sensitive resources present. Avoidance is therefore not necessary and project-related impacts are not considered significant once mitigation pursuant to BMO requirements is incorporated.

MSCP Designation For
Liberty First Charter School, PDS2015-MUP-15-027



**Attachment E –
PUBLIC DOCUMENTATION**



County of San Diego, Planning & Development Services
Project Planning Division

Memorandum

TO: File

FROM: Denise Russell, Project Manager

SUBJECT: Response to Comments; Liberty Charter High School; PDS2015-MUP-15-027; PDS2015-ER-15-14-010

DATE: September 18, 2020

The following are staff's responses to comments for the Liberty Charter High School (project) received during the public disclosure period for the Initial Study (IS) and Mitigation Negative Declaration (MND) prepared pursuant to the California Environmental Quality Act (CEQA) dated December 12, 2019. The document was circulated for public disclosure from December 12, 2019 through January 17, 2020, and 115 comment letters were received during that time. The following responses to comments are organized based on date of receipt.

Response to comments received from D&J Edgar, dated December 13, 2019:

1-A This comment states that the commenter does not agree with the siting of the project based on the proximity of the "liquor store/bar shops" and because of the additional traffic.

The following response is provided as a response to the above comment as well as a "Master Response" for the issue of school proximity to "liquor store/bar shops", bars, hookah lounges, smoke shop, vape shop, massage parlor, the 7/11 and their "easily accessible trash receptacles". Although this comment does not relate to an issue area under CEQA, the land use compatibility of the project is discussed below.

The project is located within the Rural Residential (RR) land use regulation of the County of San Diego (County) Zoning Ordinance. A charter school is an allowed use type within the RR zone with the granting of a Major Use Permit (MUP), for which the project is applying. In addition, the California Department of Alcoholic Beverage Control (ABC) is the government agency with regulation of alcoholic beverages in the state of California. ABC regulates the sale of alcoholic beverages through applications for new or renewed alcohol licenses by reporting on the moral character and fitness of applicants, and the suitability of premises where sales are to be conducted. ABC does not regulate the location of schools, such as charter schools. Additionally, the sports field and bioretention basins associated with the project are located on the eastern end of the parcel, acting as a physical buffer between the school buildings, entrance and parking lot, and the adjacent commercial shopping center. Therefore, because the project is consistent with the

use regulations of the zone, and the proximity of the school to the use types mentioned above does not raise any new CEQA-related issues, no changes were made to the IS and MND as a result of this comment. In addition, please see response 5-E for further information about traffic and safety.

- 1-B This comment states that at a minimum, access should be provided at both Chase and Jamacha to relieve congestion at “that very busy intersection”.

Please see response 5-E for further information about traffic. No changes were made to the IS and MND as a result of this comment.

- 1-C This comment suggests the commenter’s dissatisfaction with the public disclosure period of December 12th to January 17th during the busy holidays.

Pursuant to CEQA Guidelines Section §15105(b), when a proposed mitigated negative declaration is submitted to the State Clearinghouse for review by state agencies, the public review period shall not be less than 30 days. The project disclosure period was extended for an additional 5 days to account for the holidays. All requirements per CEQA Guidelines Section §15105(b) have been met. No changes were made to the IS and MND as a result of this comment.

- 1-D This comment states the commenter is opposed to the project. The commenter has stated concerns for ingress and egress on Chase Avenue and with parking.

Please see response 5-E for further information about traffic and parking. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Lindsey Emerson, dated December 14, 2019:

- 2-A This comment states the current traffic at the intersection of Chase and Jamacha is a “nightmare” and adding another school would be detrimental to the community.

Please see response 5-E for further information about traffic. No changes were made to the IS and MND as a result of this comment.

- 2-B This comment questions how the school can be located in close proximity to a smoke shop and liquor store.

Please see response 1-A for further information about the school proximity to the liquor store. No changes were made to the IS and MND as a result of this comment.

Response to comments received from D&J Edgar, dated December 18, 2019:

- 3-A This comment states the concern for additional traffic in the community caused by the project. Specifically, the commenter states that the current left-hand turn lane located on eastbound Jamacha Road is frequently backed up into the painted island causing limited straight through access to east bound Chase avenue. The commenter states the addition of the school would block all straight through east bound access.

Please see response 5-E for further information about traffic. No changes were made to the IS and MND as a result of this comment.

- 3-B This comment states the concern for the school drop off on a two-lane road and the limited onsite parking. The commenter additionally states the bicycle space will be the most underutilized part of the school because this “is not a neighborhood school”.

Please see response 5-E for further information about traffic and bicycle spaces. No changes were made to the IS and MND as a result of this comment.

- 3-C This comment states the school should provide access from both Chase Avenue and from Jamacha Road. This remainder of this comment is conclusionary.

Please see response 5-E for further information about traffic and bicycle spaces. No changes were made to the IS and MND as a result of this comment.

Response to comments received from the California Department of Fish and Wildlife, dated December 27, 2019:

- 4-A This comment states the California Department of Fish and Wildlife (CDFW) has reviewed the MND and is thankful for the opportunity to provide comments. This comment further discusses CDFW’s role as an introduction.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

- 4-B This comment states the County of San Diego currently participates in the Natural Community Conservation Planning Program through the implementation of the Multiple Species Conservation Subarea Plan.

This comment is informational and does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

- 4-C This comment states the project description and location.

This comment is informational and does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

- 4-D This comment states that CDFW would like a submitted Bill of Sale or Credit Sales Receipt instead of a copy of the purchase contract if the project applicant intends to mitigate using Option 1.

The project Major Use Permit condition for the offsite mitigation of non-native grassland has been updated to also require a Bill of Sale or Credit Sales Receipt in addition to the copy of the purchase contract. No changes were made to the IS and MND as a result of this comment.

Response to comments received from the Best Best & Krieger Attorneys at Law on behalf of the Grossmont Union High School District, dated January 7, 2020:

- 5-A This comment states that the Grossmont Union High School District (District) is concerned that the CEQA analysis for the project “was limited to only a MND instead of the much more thorough analysis that would have been required as part of a full Environmental Impact Report (EIR).”

Please see response 5-G for further information about the MND and the CEQA requirement of preparing an EIR. No changes were made to the IS and MND as a result of this comment.

- 5-B This comment states that although the District believes there are several areas of the MND that demonstrate a fair argument that the project may have significant environmental impacts, the District's comments are primarily focused on the potential impacts to the District's students and facilities. In addition, the commenter states the MND makes only one mention of the Valhalla High School and contains no analysis of the project's impact on the District's students or facilities.

Please see response 5-G for further information about the MND and the CEQA requirement of preparing an EIR. No changes were made to the IS and MND as a result of this comment.

- 5-C This comment states that the IS did not address the potential construction emission impacts on the District's students. The District is particularly concerned given the admission in the IS that construction operations would result in potentially significant diesel emissions. Due to this, the District believes an EIR should be prepared with a more detailed analysis of potential air quality impacts on District students with corresponding enhanced mitigation measures.

Please see response 5-G for further information about air quality as well as the MND and the CEQA requirement of preparing an EIR. No changes were made to the IS and MND as a result of this comment.

- 5-D This comment states the Public Services section of the IS did not include an analysis of the project's potential impacts to the District's students or educational programs. The district believes an EIR with appropriate analysis of potential staggered start/end times, traffic coordination, safe routes to school, student transportation, special events, etc., should be prepared.

Please see responses 5-E and 5-G for further information about traffic as well as further information about the MND and the CEQA requirement of preparing an EIR. No changes were made to the IS and MND as a result of this comment.

- 5-E This comment states the MND and the “Focused Traffic Impact Study” contain no analysis of the project's potential impact on Valhalla High School and/or potential exacerbation of traffic impacts of the project when considered with Valhalla High

School's current operations. Specifically, there is no analysis of the additional 585 vehicle trips given its proximity to Valhalla High School and its potential impacts to the District's students or educational programs in terms of school start/end time, special traffic issues, student transportation issues, and/or safe routes to school for District students walking or driving to school. Further, the commenter states while Project vehicle trips are analyzed, there does not appear to be any analysis of the safety routes of ingress and egress for project students who may walk and/or bike to school. Due to this, the District believes that an EIR should be prepared.

The following response is provided as a response to the above comment as well as a "Master Response" for the issue area of transportation and traffic.

Background

The County of San Diego establishes guidelines of significance to analyze CEQA impacts through the Guidelines for Determining Significance. The Guideline that was in effect during the public disclosure of the subject project for Transportation and Traffic, was the Guidelines for Determining Significance for Transportation and Traffic, dated August 24, 2011. Since the project went out for public disclosure in December 2019, a new set of guidelines, the Transportation Study Guidelines, have been adopted by the County Board of Supervisors on June 24, 2020, to address Senate Bill 743 (SB 743). SB 743 changed the way that public agencies evaluate transportation impacts under CEQA. A key element of this law is the elimination of using auto delay, Level of Service (LOS), and other similar measures of vehicular capacity or traffic congestion as a basis for determining significant transportation impacts under CEQA. The new established criteria for determining the significance of transportation impacts is Vehicle Miles Traveled (VMT) and is further addressed below in this response. Although no longer utilized as the standard for evaluating transportation impacts under CEQA, the County's General Plan identifies LOS as being a required analysis per Policy M-2.1 and is therefore also addressed in these responses.

LOS

Many of the comments received in relation to transportation and traffic were concerned with intersection queuing and local road congestion, or otherwise known as LOS. LOS is used to analyze roadways and intersections by categorizing traffic flow and assigning quality levels of traffic based on delay or density. Six LOS categories are defined for each type of transportation facility and letters designate each level, from A to F, with LOS A representing the best operating conditions, and LOS F, the worst. In the County of San Diego, LOS "D" is acceptable. If any intersection that operates at an acceptable LOS under pre-project conditions causes the intersection to degrade to an LOS "E" or "F" per post-project modeling predictions, the project would be required to identify improvements to operate at an acceptable "D", or better. In general, LOS criteria are stated in terms of the "average control delay" per vehicle for the peak 15-minute period within the hour

analyzed. The average control delay includes deceleration delay, queue, move-up time, and final acceleration time in addition to the stop delay. The 15-minute period analyzed is basically the segment of time within the hour at which traffic is at its worst.

A focused Traffic Impact Study (TIS) was prepared by Kimley Horn, dated November 2018, which provided trip generation data, analyzed direct impacts to traffic, and identified appropriate mitigation for direct and cumulative impacts from the project. Some comments raised concerns that because the traffic data used within the Focused Traffic Impact Study is from October 2014 (Chase Avenue and Jamacha Road) and February 2015 (Hillsdale Road and Jamacha Road), the analysis does not take into consideration the most recent, or existing conditions. The most recent Caltrans published counts for Jamacha Road, north and south of Chase Avenue, is from the year 2016 and is less than what was analyzed in the TIS. In addition, updated data has been received from the County Department of Public Works for Chase Avenue from 2016, which shows minimal increase from the data in the TIS (18,463 compared to 18,434). Because of this, traffic has not changed enough to warrant a new analysis.

Trip generation for the project was estimated by using the San Diego Association of Government (SANDAG) Brief Guide of Vehicular Traffic Generation Rates for the San Diego Region, April 2002. Because these traffic generation rates do not provide rates for the afternoon school dismissal period, the Institute of Transportation Engineers (ITE) Trip Generation Manual, 9th Edition, was used to estimate the traffic generation for the project. The project was found to generate a total of 585 new daily trips, including 117 (82 in, 35 out) morning peak-hour trips, 99 (33 in, 66 out) afternoon (school traffic) peak-hour trips, and 59 (23 in, 36 out) afternoon (commuter traffic) peak-hour trips. To analyze the project's potential impacts, a study area was defined based on the estimated traffic generation, likely project traffic patterns, procedures summarized in the County of San Diego Guidelines for Determining Significance and Report Format and Content Requirements dated August 24, 2011, and discussions with Caltrans.

Four intersections were identified from the study area for evaluation: Chase Avenue/Jamacha Road (SR-54), Hillsdale Road/Jamacha Road (SR-54), project Entry Driveway/Chase Avenue, and project Exit Driveway/Chase Avenue. The four intersections were evaluating using a model, Synchro Version 10. Existing traffic signal timing parameters were provided by Caltrans for the intersections of Chase Avenue/Jamacha Road (SR-54). The parameters used for the other intersections was peak-hour factor, or a measure of traffic demand fluctuations within the peak hour. The model found that all intersections would operate at a County acceptable LOS "D" and no mitigation was required. However, because Jamacha Road is a Caltrans facility, the project would reconstruct the Chase Avenue/Jamacha Road intersection to integrate sidewalks and curb ramps, as well as motorized, transit, pedestrian, and bicycle travel per current standards Caltrans DIB 82-06 and Caltrans Complete Streets Deputy Directive-64-R1, as a condition of approval. In

addition, the project would provide the Synchro Version 10 files along with early coordination as requested by Caltrans.

Five roadway segments were identified from the study area for evaluation: Jamacha Road from Chase Avenue to Penasco Road, and Chase Avenue to Shadowside Lane; and Chase Avenue from Jamacha Road to Fair County Road, Jamacha Road to Driveway 1, and Driveway 1 to Brayton Way. The model found that two of the roadway segments would exceed County thresholds, resulting in an unacceptable LOS "F", and required mitigation: Jamacha Road to Driveway 1, and Driveway 1 to Brayton Way. To reduce impacts to less than significant, the project would restripe Chase Avenue to accommodate a 300 foot long westbound left-turn pocketed into the site, add a raised median, and would widen Chase Avenue between the west end of the project site to the intersection of Jamacha Road (SR-54) to accommodate an additional eastbound travel lane. With the inclusion of the above-mentioned project mitigation, the roadway segments were found to operate at an acceptable LOS "D" and no further mitigation was required.

Ingress/Egress & Parking

Many comments raised concerns that the project would not be able to adequately accommodate cars for parking, as well as ingress and egress during school drop off/pick-up hours. Additional analysis was conducted within the Focused Traffic Study to address project drop-off and pick-up operations, onsite parking evaluation, and available public transit. The Focused Traffic Study found the two access driveways along Chase Avenue, pavement markings, sign installations, and the approximate 700 linear feet drop-off/pick-up area provided within the site for 28 vehicles, would adequately accommodate the typical drop-off/pick-up operations of the school. Therefore, an additional access point from Jamacha road, as suggested by a few commenters, would not be warranted. In addition, alternative transportation methods are available for students as an option to travel to school since many of the current enrolled students live within the El Cajon area. The project site is currently served by the Metropolitan Transit System (MTS) Route 816 approximately 600 feet from the site entrance. This route runs from the El Cajon Transit Center to the Cuyamaca College via Marshall Avenue, Main Street, Washington Avenue and Jamacha Road (SR-54). In areas where this bus service is available, students will have the option to take the bus to school. The project would also upgrade the intersection of Jamacha Road and Chase Avenue to integrate pedestrian and bicycle travel per Caltrans Complete Street Deputy Directive-64-R1 as well as provide for 45 bicycle parking spaces onsite. For students who are old enough to drive a vehicle to school, a total of 161 parking spaces are available onsite for students and staff. The number of parking spaces has been calculated pursuant to the County's Zoning Ordinance Section 6764, requiring 1 parking space per employee, 15 parking spaces for visitors, and 0.25 parking spaces per student. Consideration for special events has also been included within the Focused Traffic Study as part of the project description, and the project has been found to be consistent with all County requirements.

Safety

Some comments raised concerns regarding safety of the project, specifically “safe routes to school” for pedestrian and bikers. The project would improve the safety for pedestrian and bikers from existing conditions. The existing infrastructure conditions along Chase Avenue are 46 feet of right-of-way, which includes two six-foot shoulders/bike lanes. As previously mentioned, the project would be required to widen Chase Avenue as a condition of approval. Chase Avenue would be widened from 46 feet to 70 feet of right-of-way, which includes a five-foot sidewalk and six-foot bike lane on eastbound Chase Avenue, and an eight-foot shoulder/bike lane on westbound Chase Avenue. These improvements are consistent with the Guidelines for Determining Significance for Traffic and the General Plan Mobility Element, have been approved by the Department of Public Works, and meet all requirements of the County Public Road Standards. In addition, the project would upgrade the intersection of Jamacha Road and Chase Avenue to integrate pedestrian and bicycle travel per Caltrans Complete Street Deputy Directive-64-R1. The current bus stop along Jamacha Road near Chase Avenue would also be upgraded per section 4.3.16 of Caltrans DIB 82-06. As such, the project has been found to be consistent with all County requirements and would provide safer pedestrian and biker infrastructure than existing conditions.

TIF

Some comments raised concerns that the project analysis does not address other County projects, also referred to as a cumulative analysis, within the TIS. However, all potential cumulative impacts have been addressed through the County Transportation Impact Fee (TIF) program. The county’s TIF program provides a programmatic mechanism for projects to mitigate cumulative impacts with a “fair share” fee payment. The primary purpose of the TIF is twofold: (1) to fund the construction of identified roadway facilities needed to reduce, or mitigate, projected cumulative traffic impacts resulting from future development within the County; and (2) to allocate the costs of these roadway facilities proportionally among future developing properties based upon their individual cumulative traffic impacts. Therefore, through the project’s payment in the TIF Program, cumulative impacts would not be significant. In addition, the other mentioned County projects by commenters, such as the Cottonwood Sand Mine and Fuerte Ranch Estates, are discretionary projects which are subject to CEQA. Prior to approval of these projects, stand-alone traffic analyses is/has been required to address any potential direct and cumulative impacts and identify mitigation (if warranted).

VMT

As previously stated, the Transportation Study Guidelines were adopted to address SB 743 and established the new criteria for determining the significance of transportation impacts to VMT within the County of San Diego. SB 743 became effective on July 1, 2020, after the project’s public disclosure date of December

2019. The MND found that because the County of San Diego had not adopted a threshold for VMT and was not expected to until July 2020 when the provisions of the section apply statewide, no impacts would occur. Pursuant to the new adopted Transportation Study Guidelines Section 2.2, Table 1, the project meets the CEQA VMT screening criteria for a Locally Serving Public Facility as well as the Project Located in VMT Efficient Areas per the location-based screening maps, and would not require a CEQA VMT analysis. In addition, a qualitative VMT analysis was provided in the Air Quality Assessment. The VMT analysis was based on student zip codes and compared the VMT to the existing Lemon Grove location to the proposed El Cajon location. The analysis concluded that the relocation of the High School site would result in a reduction to school related VMTs. This conclusion was based on the existing population of middle school and elementary school student's location near the El Cajon area that would likely attend the high school in the coming years.

- 5-F This comment states that an analysis of the potential conflicts associated with neighboring business and the project's students appears to be missing. The neighboring businesses of special concern listed in the comment are a liquor store, vape shop, hookah lounge, and a body and foot massage. The commenter states further analysis of potential conflicts, safety concerns, and/or student impacts should be incorporated in the CEQA review of the project.

Please see response 1-A for further information about the liquor store, vape shop, hookah lounge, and the body and foot massage. No changes were made to the IS and MND as a result of this comment.

- 5-G This comment states that if an appropriate EIR analysis is prepared with a corresponding alternatives section, then the impacts to biological resources, air quality, cultural resources and greenhouse gas emissions may be further and/or better mitigated. This comment concludes with the suggestion of requiring the project applicant to prepare a full EIR that adequately addresses the potential environmental impacts of the project on the District's students and educational programs.

The following response is provided as a response to the above comment as well as a "Master Response" for further information about the MND and the CEQA requirement of preparing an EIR.

Pursuant to CEQA Guidelines section §15365, an Initial Study is a preliminary analysis prepared by the Lead Agency to determine whether an EIR or a Negative Declaration (ND) must be prepared or to identify the significant environmental effects to be analyzed in the EIR. The IS prepared for the project fully analyzed all issue areas pursuant to Appendix G of the Guidelines. The project was determined to result in potentially significant effects to the following issues areas: Air Quality, Biological Resources, Transportation, Utilities and Service Systems, and Mandatory Findings of Significance. With the incorporation of mitigation measures, all aforementioned issue areas were found to result in a less than significant

impact. All other issue areas per Appendix G of the CEQA Guidelines were determined to result in less than significant impacts, with no mitigation required. Pursuant to CEQA Guidelines section §15162.4(3), no mitigation measures are required for effects which are not found to be significant.

Commenters have stated the belief that a fair argument has been raised against the project, or additional analysis should be required through the use of an EIR, for the following issue areas: Air Quality, Biological Resources, Cultural Resources, Greenhouse Gas Emissions, Land Use and Planning, Noise, Public Services, Recreation, Traffic/Transportation, Utilities and Service Systems, Visual Impacts, and Wildfire. Other areas of concern raised included parking, public safety, and “other”, which is represented by proximity of the school to the shopping center with a liquor store, vape shop, and other unfavored use types. Please see response 1-A for further information on proximity of the school to unfavored use types, and 5-E for further information about traffic, parking and public safety.

IS and Technical Studies Findings

Air Quality

An Air Quality Assessment was prepared by Ldn Consulting, Inc and found the project would have a less than significant impact to the following subject areas: conflict or obstruct implementation with the Regional Air Quality Standards or the State Implementation Plan, cumulatively considerable net increase of any criteria pollutant for which the project region is in non-attainment under an applicable federal or state standard, and other emissions (such as those leading to odors) adversely affecting a number of people. The Air Quality Assessment circulated for public disclosure stated the Project could result in a potentially significant impact to exposing sensitive receptors to substantial pollutant concentrations during grading. As a condition of approval, the project is required to use construction equipment with California Air Resources Board Certified Tier 4 engines with diesel particulate filters (DPF). With the incorporation of the DPF, the Air Quality Assessment determined the project would fully mitigate potential impacts to sensitive receptors below a level of significance. Therefore, no further mitigation would be required.

Biological Resources

A Biological Resources Report was prepared by Elyssa Robertson, accepted by County staff biologist, and found the project would have a less than significant impact to the following subject areas: candidate, sensitive, or special status species; state or federally protected wetlands; wildlife movement, wildlife corridors or native wildlife nursery sites; and conflict with an adopted habitat conservation plan. The Biological Resources Report circulated for public disclosure stated the project could result in a potentially significant impact to riparian habitat or other sensitive natural community. While no riparian habitats were identified on the

project site, a County-identified sensitive community, non-native grassland, was identified onsite to be impacted by the project. As a condition of approval, the project is required to mitigate impacts to non-native grassland through the purchase of 2.62 acres of Tier III habitat, or higher, within the Multiple Species Conservation Plan South County Subarea. With the incorporation of the above mitigation measure, the IS determined the project would fully mitigate potential impacts to a sensitive natural community below a level of significance. Therefore, no further mitigation would be required.

Cultural Resources

A Cultural Resources Report was prepared by Jerry Schaefer and found the project would have a less than significant impact to the following subject areas: historical resource pursuant to §15064.5; archaeological resource pursuant to §15064.5; and human remains, including those interred outside of formal cemeteries. No potentially significant impacts were identified for cultural resources. Therefore, no mitigation would be required.

Greenhouse Gas Emissions

The IS analyzed the potential impacts of the project to Greenhouse Gas Emissions and found the project would have a less than significant impact to the following subject areas: generate greenhouse gas emissions, either directly or indirectly that may have a significant impact on the environment; and conflict with an applicable plan, policy or regulation adopted for the purposes of reducing the emissions of greenhouse gases.

The County of San Diego adopted a Climate Action Plan (CAP) on February 14, 2018 which outlines actions that the County will undertake to meet its Greenhouse Gas Emissions reduction targets. The project was determined to be consistent with the CAP. Due to legal challenges with the County's CAP further described in the updated and redlined IS, the state appeals court upheld the lower court's ruling, rejecting the County's CAP in June 2020, after the project was circulated for public disclosure. Given the legal instability concerning the County's CAP during review of the project, the analysis prepared for the project did not rely on the CAP to streamline the project's environmental analysis under CEQA Guidelines Section 15183.5. In the event that the CAP did not withstand judicial scrutiny, the project underwent a separate stand-alone analysis for determining whether the project's GHG emissions would significantly impact the environment.

Amendments to Section 15064.4 of the CEQA Guidelines were adopted to assist lead agencies in determining the significance of impacts of GHG emissions. Section 15064.4 specifies that a lead agency "shall make a good-faith effort, based on the extent possible on scientific and factual data, to describe, calculate or estimate the amount of GHG emissions resulting from a project." Section 15064.4 also provides lead agencies with the discretion to determine whether to assess

those emissions quantitatively or to rely on a qualitative analysis or performance-based standard. In addition, the CEQA Guidelines specific that “[w]hen adopting or using thresholds of significance, a lead agency may consider thresholds of significance previously adopted or recommended by other public agencies, or recommended by experts, provided the decision of the lead agency to adopt such thresholds is support by substantial evidence” (14 CCR 15064.7[c]).

In the absence of a locally adopted threshold by the County of San Diego, projects can be evaluated according to CEQA Guidelines Section 15064.7(c) by comparing the project’s GHG emissions to an efficiency threshold prepared through the County’s Local Government Partnership with San Diego Gas and Electric (SDG&E). This efficiency threshold is based on a countywide inventory of GHG emissions, forecasted consistent with meeting the State reduction target in 2030 and reduction goal in 2050.

Based on the County’s baseline GHG inventory and forecasts, projects in the County need to demonstrate they would generate emissions below the 2.94 metric tons of carbon dioxide equivalent (MTCO_{2e}) per service population. This service population threshold was determined using local data to demonstrate consistency with the communitywide GHG reduction targets for 2030 based on State reduction targets. Further detail for the calculations and assumptions applied to estimate project generated GHG emissions during construction and operations are provided in the Global Climate Change study prepared for the project. The project would have a total service population of 488, including 450 students and 33 staff. Based on the project analysis, the project construction and operations would result in the generation of approximately 830 MTCO_{2e} annually, or 1.7 MTCO_{2e} per service population. Thus, the project’s estimated GHG emissions would be below the service population threshold for 2030.

Although the State court set aside the CAP, the project was required to incorporate more sustainable design standards and implement applicable reduction measures which are still effective measures to reduce GHG emissions. Additional red-lined information has been provided in the IS to further disclose how the project would result in a less-than-significant impact to GHG considering the recent legal change concerning the CAP. The updates made in the IS include project design measures, consistency to County ordinances, and compliance with California regulations, which would not warrant a recirculation of the IS pursuant to CEQA Guidelines Section 15073.5. Therefore, because no potentially significant impacts were identified for Greenhouse Gas Emissions, no mitigation would be required.

Land Use and Planning

The IS analyzed the potential impacts of the project to Land Use and Planning and found the project would have a less than significant impact to the following subject areas: physically divide an established community; and conflict with any land use plan, policy, or regulation adopted for the purposes of avoiding or mitigating an

environmental effect. No potentially significant environment effects were identified for Land Use and Planning. The project does not propose the introduction of new infrastructure such as major water supply systems or utilities to the area. In addition, the project is consistent with the Semi-Rural 0.5 General Plan Land Use Designation and Rural Residential Zoning of the site which permits schools with a Major User Permit. The project is applying for a Major Use Permit; thus, no mitigation would be required.

Noise

A Noise Analysis was prepared by Jeremy Loudon and found the project would have a less than significant impact to the following subject areas: generation of substantial temporary or permanent increase in ambient noise levels in the vicinity of the project in excess of standards established; generation of excessive groundborne vibration or groundborne noise levels; and for a project located within the vicinity of a private airstrip, an airport land use plan, or within two miles of a public airport/public use airport, expose people residing or working in the project area to excessive noise levels. Commenters raised concerns over the “bells and chatter” or events of the school. The Noise Analysis determined no noise levels of the proposed noise sources of the school would directly or cumulatively exceed the property line standards at the shared commercial and residential property lines. Additionally, Section 36.417 of the County of San Diego Noise Ordinance provides exemptions to specific sources such as school activities. Therefore, noise reasonably related to authorized school entertainment events would be in compliance with the County’s Noise Ordinance. No potentially significant environmental effects were identified for Noise. Therefore, no mitigation would be required.

Public Services

The IS analyzed the potential impacts of the project to Public Services and found the project would have a less than significant or no impact to the following subject area: substantial adverse physical impacts associated with the provision of new or physically altered government facilities, the need for new or physically altered government facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance ratios, response time or other performance objectives for fire protection, police protection, schools, parks, or other public facilities. This determination was made based on the service availability forms received for the project. No potentially significant environmental effects were identified for Public Services. Therefore, no mitigation would be required.

Recreation

The IS analyzed the potential impacts of the project to Recreation and found the project would have no impacts to the following subject areas: existing

neighborhood and regional parks or other recreational facilities; and construction of new or expanded recreational facilities as part of the project that may have an adverse physical effect on the environment. Because the project does not propose a residential use or propose new or expanded recreational facilities, the IS determined no impacts would occur. No potentially significant environmental effects were identified for Recreation. Therefore, no mitigation would be required.

Utilities and Service Systems

The IS analyzed the potential impacts of the project to Utilities and Service Systems and found the project would have a less than significant or no impact to the following subject areas: relocation or construction of new or expanded facilities; sufficient water supplies available to serve the project and reasonably foreseeable future development during normal, dry and multiple dry years; determination of the wastewater treatment provider that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments; generate solid waste in excess of standards, the capacity of local infrastructure, or otherwise impair the attainment of solid waste reduction goals; comply with federal, state, and local statutes and regulations related to solid waste; and exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board. The IS circulated for public disclosure stated the project could result in a potentially significant impact to the environment from the construction of new or expanded storm water drainage facilities. However, the facilities have been analyzed within the IS as part of the project description and mitigated below a level of significance (i.e., biological resources mitigation). Therefore, no further mitigation would be required.

Visual Impacts

The IS analyzed the potential impacts of the project to Visual Resources (Aesthetics) and found the project would have a less than significant or no impact to the following subject areas: adverse effect on a scenic vista; damage scenic resources within a scenic highway; substantially degrade the existing visual character or quality of public views of the site and its surroundings; and substantial light or glare which would adversely affect day or nighttime views in the area. Concerns were raised by commenters regarding the school's visual impact on the community character as well as the facilities parking lot lighting on the surrounding neighbors during night and sporting events. The IS determined that because the project site is within an urbanized area of the Valle de Oro community (El Cajon) and is surrounded by highly developed land use types such as single-family residential and commercial uses, consistent with the Zoning Ordinance setbacks and height designator, incorporated landscaping for screening purposes, and would experience limited viewer exposure, impacts to visual character or quality would be less than significant. In addition, the IS determined substantial light or glare would not occur due to the project because the project would conform to the Zone B regulations of the San Diego County Light Pollution Code. These

regulations include the lamp type and shielding requirements per fixture and hours of operation limitation for outdoor lighting. For additional information on the project requirements for lighting and glare, please see IS section I. Aesthetics, d) as well as the project photometric study circulated for public disclosure. No potentially significant environmental effects were identified for Visual Resources (Aesthetics). Therefore, no mitigation would be required.

Wildfire

The IS analyzed the potential impacts of the project to Wildfire and found the project would have a less than significant impact or no impact to the following subject areas: substantially impair an adopted emergency response plan or emergency evacuation plan; due to slope prevailing winds, and other factors, exacerbate wildfire risks, and thereby expose project occupant to pollutant concentrations from a wildfire or the uncontrolled spread of a wildfire; require installation or maintenance of associated infrastructure that may exacerbate fire risk or that may result in temporary or ongoing impacts to the environment; and expose people or structures to significant risks, including downslope or downstream flooding or landslides, as a result of runoff, post-fire instability, or drainage changes. Concerns raised by commenters for Wildfire included the project's increase in traffic and the ability of Fire Authority to respond to emergencies, such as wildfires, in the area, as well as respond to emergencies on the school campus. The Focused Traffic Study determined the project would result in 585 ADT, requiring the project to mitigate impacts with the widening of Chase Avenue. With the addition of this mitigation, the impacted segment on Chase Avenue was determined to operate at an acceptable LOS "D". In addition, the Focused Traffic Study found the two access driveways along Chase Avenue, pavement markings, sign installations, and the approximate 700 linear feet drop-off/pick-up area provided within the site for 28 vehicles, would adequately accommodate the typical drop-off/pick-up operations of the school. Further, and as stated above, the IS found the project would not result in significant impact to substantially impairing an adopted emergency response plan or emergency evacuation plan. In the event of an emergency, such as a wildfire in the area during school pick-up and drop-off hours, the project would not impair Fire Authority from responding due to traffic. In the event that an emergency was to occur on the project site, the SMFPD has indicated the project is eligible for service with the nearest fire station located 0.2 miles from the project. No potentially significant environmental effects were identified for Wildfire. Therefore, no mitigation would be required.

Conclusion

The IS and MND prepared for the project adequately analyzed all project impacts, and required mitigation measures as project conditions of approval, when impacts were identified as potentially significant. In addition, the County does not find that a "fair argument" has been raised against the project because no fair argument

has been made supported by substantial evidence that the project may have a significant impact on the environment. Therefore, no changes were made to the IS and MND as a result of this comment and the preparation of an EIR would not be warranted.

Response to comments received from Ferri and Eric Horn, dated January 7, 2020:

- 6-A This comment states the location of the proposed school should be denied due to the current volume of both pedestrian and automobile traffic. The commenter continues by stating the additional increase in traffic would negatively impact the residential and suburban ambience of the area because there are students and resident walking, running and/or riding bikes on Chase avenue and Jamacha Road on any given day.

Because the Liberty Charter elementary and middle schools are located in the El Cajon area, the majority of those students live in the El Cajon area. The project would provide those students who currently commute to the Lemon Grove location, the opportunity to walk or bike to school. In addition, the project would upgrade the intersection of Jamacha Road and Chase Avenue to integrate pedestrian and bicycle travel per Caltrans Complete Street Deputy Directive-64-R1 as well as provide for 45 bicycle parking spaces onsite. The project would allow for additional pedestrian and bicycle infrastructure and thus would not negatively impact the residential and suburban ambience. For further information about traffic, please see response 5-E. No changes were made to the IS and MND as a result of this comment.

- 6-B This comment appears to have been cut off stating “The noise level of bells ringing every hour and the constant chatter of e In addition, please consider the impact of existing wildlife that inhabits that field.”

Please see response 5-G for further information about noise. No changes were made to the IS and MND as a result of this comment.

- 6-C This comment states the difficulty of turning right out of the existing shopping plaza because of the high volume of traffic congestion and suggests that an hourly traffic assessment on Chase Avenue be conducted prior to granting approval for this project.

Please see response 5-E for further information about traffic. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Lisa Amorteguy, dated January 8, 2020:

- 7-A This comment is written by one of the founders of the school who has been with Literacy First Charter Schools for the past 19 years and is a teacher in the school system. The commenter states how disheartened she is by the outcome of the meeting held Tuesday night, January 7, 2020, and believes the school system has

great things to offer students. The commenter asks to consider the positive impact the school will have on so many, future leaders and businesspeople who will reside in El Cajon.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Justin Allen, dated January 9, 2020:

8-A The commenter states the positive impact that the Literacy First Charter Schools has made on his family. The commenter believes the proposed location would ensure a more central location for students coming up from the K8 program, including his own children. The commenter further acknowledges how “NIMBY” is strong in most of the County but cannot think of a better neighbor than Liberty Charter High School.

NIMBY is an acronym meaning “not in my backyard”. This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Lori Benitez, dated January 9, 2020:

9-A The commenter states how proud she is that her son goes to Liberty Charter High School. Further, the commenter believes the students are some of the most quality students she has ever met and the community that the proposed school will be placed will be pleased with the students that join their community.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Liberty Bock-Heers, dated January 9, 2020:

10-A The commenter lives in El Cajon has one son currently attending the Liberty Charter High School and a younger son to soon be joining the Liberty First Charter School family. The commenter states how excited “we are” that a new Liberty Charter High School will be built in El Cajon.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Elizabeth Bottenberg, dated January 9, 2020:

11-A The commenter is a former student of the Liberty First Charter School and states the school has fundamentally formed her character, integrity, and grit as a student to be as successful as she is at “UCSD”. The commenter states how strongly she advocates for a Liberty Charter High School to “have a home” in El Cajon and to “urge you to think about the incredible impact Liberty Charter will have on the El

Cajon Community, and take a stand with me to fight for schools that truly invest in their students". Additional background is given of the Liberty Charter High School in regard to the most uplifting staff, hardworking students, welcoming community, and lack of "obnoxious sporting events" or "prison-sounding bells to guide students to class".

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Eileen Bottenberg, dated January 9, 2020:

- 12-A The commenter states her 3 children attend Liberty Charter High School for the past 13 years and urges the proposed move of the school to Chase/Jamacha as it would be a "tremendous asset" to the community with minimal traffic impact. The commenter states she attended the community meeting held on January 7 and heard the residents' concerns about traffic, but the residents compared "the small school to Valhalla and Steele Canyon". The commenter states the school does a "stellar job" of pick up and drop off to minimize traffic and that there is no heavy influx of traffic in the morning due to the varied time students arrive.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

- 12-B The commenter states the afternoon pick up process takes less than 15 minutes because all cars are pulled into the school driveway and move once the child is picked up. The commenter further states that several Liberty staff, including the vice president and security, are there every single day to ensure the process is seamless. The commenter states there is rarely a time when traffic backs up onto the street and urges the board and the residents to visit Liberty Charter in the morning and afternoon to view traffic and parking control.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

- 12-C The commenter states her "darlings" will be able to walk to the proposed Liberty location. The commenter recognizes how the mornings and afternoons are "complete chaos" near Valhalla but believes this to be due to little school intervention with traffic. The commenter further states that the Liberty Charter school does not use a bell system or has football games.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Tim Bottenberg, dated January 9, 2020:

- 13-A The commenter states he lives in Hillsdale Ranch, just down the street off Chase Avenue. The commenter believes comparing the traffic and noise impact of the

Liberty Charter High School to Valhalla Highschool is like “comparing apples to oranges”. The commenter states there is no traffic control and the drop off and pick up process lasts sometimes close to an hour. The commenter states he has never encounter traffic in the morning and afternoons last from 5 to 10 minutes without cars backing up into the street. In addition, the commenter says the proposed widening of Chase Avenue approaching Jamacha is needed.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Rana Dawood-Noaman, dated January 9, 2020:

14-A The commenter states she is the mother of three girls, two of which attend Liberty Charter High School, one which attend the elementary school Liberty Academy. The commenter states how she “automatically fell in love” with the Liberty First Charter School due to the education provided, teachers which take their time and work with every student passionately and respectfully, and the friendly environment of the school. The commenter states how she would appreciate the proposed school location and it would help out a lot of families.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Nancy Dorame, dated January 9, 2020:

15-A The commenter states how his son, who is on the autism spectrum, has never thrived so much in all his years in school, than he has since attending Liberty Charter High School. The comment continues to state “we are a East County Family, that would be every so grateful to have LCHS in El Cajon”.

LCHS was used as an acronym for Liberty Charter High School. This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Timothy Dresselhaus, dated January 9, 2020:

16-A The commenter states his strong support for the Liberty Charter High School as a value to our community and a resource to families. The commenter states the school is “outstanding in every regard – superb, caring, dedicated faculty; a wonderful and diverse student body; a track record of educational accomplishment. Our son has enjoyed the school immensely”.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Caroline Edkins, dated January 9, 2020:

17-A The commenter states the Liberty Charter High School, and other Literacy First Schools, are amazing schools and her husband and herself are thrilled their daughter attends the Literacy First Schools. The commenter discusses the higher-level education provided by the school and how the administration and teachers “practice what they teach”. In addition, the commenter points out how the school’s drop off/pick-up system is “the most efficient process we have ever experienced”.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Amy Elias, dated January 9, 2020:

18-A The commenter is the parent of a child attending the Liberty Charter High School and fully supports the proposed high school. The commenter further states her support exists because the Liberty Charter High School will provide another secondary educational option for families in East County.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Zeke Elias, dated January 9, 2020:

19-A The commenter is the parent of a child attending the Liberty Charter High School and fully supports the proposed high school. The commenter further states her support exists because the Liberty Charter High School will provide another secondary educational option for families in East County.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Veronica Espinoza, dated January 9, 2020:

20-A The commenter is a mother of four children and comments on how much Liberty Charter High School means to her family. Two of the children attend the elementary schools and one is a senior graduating from Liberty Charter High School. The commenter states that it would mean so much to her family and other families at liberty that they can have their very own school in El Cajon.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Jennifer Galvin, dated January 9, 2020:

21-A The commenter states she has been a resident of El Cajon for most of her life and has two teenage children who have attended Literacy First Charter School and

Liberty Charter High School. The commenter states at Literacy First Charter School, both of her children have been cared for and given an education that allows them to achieve academic success. The commenter states the main purpose of the comment is to express her profound love and gratitude for literacy First Charter School and Liberty Charter High School as options for east county families. The commenter urges the adoption of the proposed Liberty Charter High School plan within El Cajon for “the good of local families, the community, and the future of students who are counting on LCHS to continue to provide an excellent educational experience.”

LCHS was used as an acronym for Liberty Charter High School. This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Adela Gonzalez, dated January 9, 2020:

22-A The commenter states how she has noticed that the staff members at Liberty Charter High School care about each and every student and prepare them for their future and in their journey in becoming successful adults. The commenter continues by stating the school has great values and enforces the morals they have, like integrity and the importance of responsibility.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Bryan Heers, dated January 9, 2020:

23-A The commenter is writing in support of the new location for the Liberty Charter High School in El Cajon. The commenter further states his son attends the Liberty Charter High School location in Lemon Grove although they live in El Cajon. The commenter states that having the high school in closer proximity to the elementary and middle schools “creates a family environment where they can build relationships that last a lifetime.”

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Nadia Hermiz, dated January 9, 2020:

24-A The commenter states that she is a parent of a senior student at Liberty Charter High School and are “blessed to find a perfect school setting for our youngest child”. The commenter states the school has transformed her son into a leader, self-confident, and has excelled academically. In addition, the commenter states the school does an awesome job in traffic control. The commenter concludes by states “large school settings are not meant for every single child and every single child deserves an opportunity.”

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Bill Hilke, dated January 9, 2020:

25-A The commenter states how pleased he is by the education his son has received through the Liberty First Charter Schools and a permanent location for the high school is needed.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Randolph Houts, dated January 9, 2020:

26-A The commenter urges the approval of the site near the intersection of Chase and Jamacha Road in El Cajon. The commenter has children who have been “fortunate enough to receive the superior educational, civic, and social experience of the LFCS elementary and middle schools”. The commenter states the drive time will be reduced from 1 hour and 15 minutes to 10 minutes with no freeway driving, which is safer, less expensive, less burdensome on the environment, and would boost productivity.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

26-B This comment states the school is designed to not eliminate “any sort of green area” that is not already environmentally impacted. In addition, the comment states the project will enhance the area by providing additional commercial opportunities for local businesses. The commenter understands the project will bring traffic to the area but is located near one of the largest intersections, has proper signals, and the traffic will be limited to morning and afternoon pick-up. In addition, the commenter states Literacy/Liberty schools have always devoted a great deal of effort into their traffic control procedures and planning the parking areas.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

26-C This comment draws conclusions from the lack of traffic concern for the Greenfield development and the Shadow Mountain Church, stating the nearby intersections are much smaller and more congested near these developments, and the commenter would not expect that such concerns to be determinative for the project either.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

- 26-D This comment states the project will increase property and sales tax revenue and will permit growth in the school's size which promotes employment.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

- 26-E This comment discusses the difference between the Liberty/Literacy First Schools in comparison to traditional schools stating the Liberty/Literacy First Schools are more well-disciplined, teach history, civics and encourage a positive view of the country, more dedicated and attentive faculty and staff, and provide a unique education plan, amongst other differences. The commenter believes it is best for most children to progress with their social group through high school. The commenter believes the proposed facility would be a better location and neighborhood if developed in El Cajon.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Valda Houts, dated January 9, 2020:

- 27-A The commenter urges the County to approval the proposed Liberty Charter High School site in El Cajon at the intersection of Chase and Jamacha road. The commenter is a resident of El Cajon and currently drives an hour twice a day to take her high school student to school in order to maintain the friendships she has developed and receive a superior high school education. In addition, the commenter believes much effort has been put into the design of the new school to produce an attractive, functional space and requests an expedite approval of the project because it has been carefully considered for nearly 5 years.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Dana Ivey, dated January 9, 2020:

- 28-A The commenter is a parent of sons who have both attended the Liberty Charter Schools and is thankful for the Liberty Charter High School. The commenter believes the proposed school will serve many families if built in El Cajon.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Nopanoot Kempsey, dated January 9, 2020:

- 29-A The commenter is a parent of two children whom both attend the Liberty Charter Schools. The commenter states the administrators have been working hard for many years to move the high school from Lemon Grove to El Cajon and is essential to have all school campuses in the same local area. The commenter states the

move of the school location to El Cajon would also help decrease traffic on the I-8, I-125, and the I-94 and is a shorter commute for the children. The commenter supports the new high school location.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Victor Kempsey, dated January 9, 2020:

30-A The commenter states he is a parent of 2 students at Literacy First Charter Schools and is an ordeal to get his 9th grade son to the Lemon Grove campus. The commenter wishes to have both of his children attend the Literacy First Charter Schools because of the high-quality education they receive. The commenter also believes that traffic in the morning and afternoon is a little congested, but the streets around the new high school will be much better than they currently are.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Sergio Kid, dated January 9, 2020:

31-A This comment is from a parent of a special need student and is very pleased with the Literacy First Charter School. The commenter is “happy to give my opinion as a parent concern for my high schooler safety and wellbeing.”

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Chad Lansford, dated January 9, 2020:

32-A The commenter states both of his daughters have been students with the Literacy First Charter Schools since kindergarten and encourages the County to consider approving the project. The commenter also stated he understands the concerns with the new school but is clear to him that the administration has taken these concerns to heart and made sure to address each and every one of them.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Deirdre Lickona, dated January 9, 2020:

33-A The commenter states his child attend the Literacy First Charter High School and advocates for a permanent location for the school to offer more stability and programs to the kids.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Guadalupe Martinez, dated January 9, 2020:

34-A The commenter states she is a parent of a Liberty Charter High School Student and believes “their quality of service, educational and student behavior standards will remain as high as they are now, or even higher.”

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Debbie Medicus, dated January 9, 2020:

35-A This comment asks for consideration in pledging your support in favor of the Liberty Charter High School relocation to El Cajon. The commenter states she has two boys who have been attending the Literacy First Charter Schools and gives details regarding how each student has been set-up for success. For instance, one of her son has autism and ADHD who at one point “felt like a failure”, now has the desire to become a psychologist and writer. The commenter believes parents in El Cajon and across the county have limited choices for high school and concludes with the request to strongly consider pledging your support to Liberty Charter High School in El Cajon “for families like ours.”

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Paige O’Neal, dated January 9, 2020:

36-A The commenter asks for approval of the new location of Liberty Charter High School. The commenter has a daughter who loves going to the school and highly recommends “Liberty” as well. The commenter’s other daughter attends the Junior Academy in El Cajon. The commenter states she spends 1 hour getting both daughters to school and another hour picking them up. With so many concerns for traffic and the environment, the commenter believes the new school location where most families are located, would greatly reduce vehicle emissions. Further, the commenter addresses local traffic stating Liberty Charter High School is much smaller than Valhalla and Steele Canyon and teachers and staff work very hard to keep the drop-offs and pick-ups as smooth as possible. The commenter also believes more high school students will be able to ride bikes to the new location.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Martha Rodriguez, dated January 9, 2020:

37-A The commenter states she is a mom of six children who live in the city of El Cajon. The commenter believes having a high school in her community would be a very good thing for her family.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Tricia Rowland, dated January 9, 2020:

38-A The commenter states she has children which attended Literacy First Charter Schools since 2003 and “cannot stress the greatness of having the high school in El Cajon.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Jenny Spingelmire, dated January 9, 2020:

39-A The commenter is a parent with kids enrolled in Literacy First Charter Schools for 11 years. The commenter drives 2 hours a day to drop-off and pick-up kids from the school and concludes the new location would be a much better benefit and a great addition to El Cajon.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Chrystal Stump, dated January 9, 2020:

40-A The commenter is in support of the new campus proposal. The commenter understands the traffic concerns for the school but urges decision makers to observe the traffic patterns at one of the campuses. As a proud parent of a child attending a school of choice, parents make the choice to respect the flow and observe traffic plans.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Floriana Vogan, dated January 9, 2020:

41-A The commenter supports the plan for the new Liberty Charter High School in El Cajon. The commenter is a parent of kids who attend the Literary First Charter Schools and thrive and “will make the sacrifices to go to Lemon Grove, but it would be great to have a high school in the same community as our other three schools”.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Laura Zenteno, dated January 9, 2020:

42-A The commenter is a parent of a freshman student at Liberty Charter High School for the past few months but is “beyond delighted that my son was accepted to this

Charter School.” The commenter states she recommends Liberty Charter to anyone who has kids and would be such a wonderful service to kids and the community of El Cajon.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Haitham Afram, dated January 10, 2020:

43-A The commenter has two kids which attend “LFHC” and supports the effort of the new building. The commenter also states there is a large number of students who live in El Cajon.

“LFHC” is an acronym used which is believed to represent Literary First Charter Schools. This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Nicole Allen, dated January 10, 2020:

44-A The commenter has three sons who have attended Liberty Charter and stated she would “really love for our Charter to be able to have a home by the students it is serving so well.”

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Carolina Ballesteros, dated January 10, 2020:

45-A The commenter has two kids who attend Literacy First Charter Schools and strongly believes in the importance of locating Liberty Charter High School in El Cajon because it would change the lives of many families and students. The commenter states the teachers truly care for their students and the high school will be the perfect environment to guide a diversity of young adults into the real world.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Caralyn Blaker, dated January 10, 2020:

46-A The commenter has 2 kids who attend the Literacy First Charter Schools and is in support of the proposed site approval. The commenter further states how the school cares about kids, education, and the community and the need for more schools like the Literacy First Charter Schools in our neighborhoods.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Jennifer Burr, dated January 10, 2020:

47-A The commenter has two sons who attend the Literacy First Charter School. The commenter states the purpose of writing which is to inform how pleased she is with her children's experience at Literacy Firsts, and how hopeful she is that we are able to build the high school in El Cajon.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Adrian Caminos, dated January 10, 2020:

48-A The commenter is a parent of two children who have attended Literacy First Charter Schools and requests the approval of the permanent high school be built so more families can benefit from this school.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Melissa Eddery, dated January 10, 2020:

49-A The commenter states as a family they are so thankful for the opportunity to send their children to the Literacy First Charter School. The commenter further states the curriculum allows the staff to utilize the children's strengths to help them grow and succeed. The commenter is thrilled at the thought of what the new school site and buildings would enable the staff to offer their children and the children of so many families.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Marianne Forman, dated January 10, 2020:

50-A The commenter states she has been a "Literacy First Charter School Family" for eight years and her kids have "excelled and thrived" at Literacy First. The commenter believes the traffic report and environmental impacts have been carefully researched and presented and requests support for the new proposed high school location.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Jenny Gillespie, dated January 10, 2020:

51-A The commenter is a parent of 2 students of Liberty Charter High School. The commenter is passionate that all students should have access to continue their education with other students they have grown up with and requests the approval of the project due to proximity of the school to the other school locations.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Dina and Tom Godby, dated January 10, 2020:

52-A The commenter is a parent of two girls who have attended the Literacy First Charter School/Liberty Charter High School and makes a point about the difference of this school to other charter schools such as Steele Canyon or Helix, such as the Vice Principle knowing every students full name. The commenter understands the concerns from the community for traffic and parking but states this is also a concern of the Literacy staff. The commenter states that staff continue everyday to be onsite making sure that all traffic rules and agreed upon traffic flow are followed. The commenter asks for the decision made on this matter to consider the students that are lifelong learners at Literacy.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Stacey Harrell, dated January 10, 2020:

53-A The commenter finds it important for students be able to complete grades K-12 as a full program to allow students to continue their education path through Literacy first High School in El Cajon. The commenter states having a local high school is vital because the majority of students come from the east county area, specifically El Cajon. In addition, the commenter understands the concern with traffic but believes the traffic plan addresses all of these concerns including traffic paths, lights, parking, environmental impact, architecture and parking.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Manar Layth, dated January 10, 2020:

54-A The commenter states he has two kids in the school system and hopes the project will get built.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Leta Maclean, dated January 10, 2020:

55-A The commenter states both of her children came up through the Literacy First Charter School System in El Cajon. The commenter continues to state although the location in Lemon Grove is convenient for her family, the location was not convenient for a number of her son's friends which ended up going to other high schools closer to where they lived. The commenter believes having a high school in the same location as the other schools would be extremely beneficial. In addition, the commenter has not observed a significant impact to traffic at the

current location and does not believe there would be impacts to traffic at the proposed location.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Paul Miller, dated January 10, 2020:

56-A The commenter states he is the parent of a Liberty High School Sophomore and an 8th grader who will be attending next year. The commenter believes the new high school facility will be an overwhelmingly positive thing for the community and hope the project will be approved.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Steve Robinson, dated January 10, 2020:

57-A The commenter is the Vice Principal of the Liberty Charter High School in Lemon Grove and stated his attendance at the Valley De Oro Planning Commission meeting on Tuesday night. The commenter stated the main project concerns expressed were in relation to traffic, parking and safety but were “ill informed” based on the fact that Literacy First Charter School, a school of 450 students under close supervision, is being compared to Valhalla, “a school of thousands under loose supervision.” The commenter addresses parking issues by stating over the past nine years, 10% or less of the students drive a car to school. In addition, the commenter states the athletic games generate 30-100 spectators and the proposed parking is ample. The commenter states the school will have assigned parking for students and a plan for student parking and monitoring.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

57-B The commenter addresses traffic concerns of the project from residents. The commenter states residents assume the 450 students will arrive and be picked-up at the same time every day, but parents drop off students from 7:00 a.m. to 8:15 a.m. The commenter continues to say that within the last 8 years at the current site in Lemon Grove, cars have never backed-up on the street during drop-off time. In addition, the commenter adds that the new location on Chase would plan to have the seniors start later in the morning. For afternoon pick-up, the commenter states 100 – 150 students are involved in after school extra-curricular activities that stagger traffic. Further, drop-off and pick-up times start later than Valhalla.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

57-C The commenter address safety concerns of the project from residents. The commenter states the residents are concerned with parents dropping students off across the street where students will be running across Chase to get to school. The commenter also states concerns brought up by the residents included the

proximity of the school to a nearby 7/11, vape, liquor store, and massage parlor and the possibility of students rummaging through their trash cans. The commenter states a convenient store and gas station is currently located $\frac{1}{4}$ of a mile away from the current campus and a handful of student walk to the store before school starts. The commenter further states he has personally monitored drop off and pick up for 12 years, 8 at the current site, and students walk to the store without incident, use crosswalks every day, and the parents safety drop of kids at the designated locations. Lastly, the commenter states no complaint about students improperly crossing the street has occurred within the last 8 years.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

57-D The commenter concludes how serious the Literacy First Charter School take responsibility as neighbors and that the future neighbors will find the project a welcome addition to their community.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Sarah Rodriguez, dated January 10, 2020:

58-A The commenter is a parent of a student who attends a Literacy First Charter School and is a resident in El Cajon. The purpose of the comment is to urge the decision makers to allow the building of the Literacy First High School. The commenter states the current high school is far from the elementary and middle schools and may not be feasible to “continue through high school if the new campus is not allowed to open.”

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Rita Hanna, dated January 10, 2020:

59-A The commenter is asking for a status of the proposed Liberty Charter High School and has three kids enrolled in the Literacy First Charter School. The commenter supports the new location of the charter school at the corner of Chase Avenue and Jamacha Road because it will be in close proximity to the family. The commenter also asks for an estimate time for this process to be completed and when will construction take place? The commenter asked for an expedited review.

This commenter has been added to the notification list for the project. The project is moving forward and is scheduled for a hearing in September. The construction timeline will depend greatly on the decision made by the Planning Commission and the project applicant. If the project is approved or denied by the Planning Commission, the applicant or the public has an opportunity to appeal the decision to the Board of Supervisors.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Elesha Bacong, dated January 11, 2020:

60-A The commenter is a mother of a daughter in the second grade and believe the new location would be much better than the current Lemon Grove location. The commenter fully supports the building of the high School.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Debbie and Kevin Bailey, dated January 11, 2020:

61-A The commenter states the importance for their children to continue their current education at a high school that is aligned with the education and experience they are currently encompasses by. The commenter believes the new location in El Cajon is ideal for the High School because it is near all the other Literacy First Charter Schools and will have a positive impact on the community.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Noel Elliott, dated January 11, 2020:

62-A The commenter states how the Liberty Charter High School has been a huge positive for her family but would be a positive addition to the community.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Nancy Faulkner, dated January 11, 2020:

63-A The commenter states her son currently enrolls in a Literacy First Charter School and is autistic. The commenter believes it is vital to be able to continue his great experience after 8th grade and request to make the new high school a reality.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Jeni Fondaw, dated January 11, 2020:

64-A The commenter states her enthusiasm for the possibility of a Literacy First High School in their community within walking distance. The commenter lives nearby and feels assured that the traffic will be accommodated by the proposed development and expansion of Chase Avenue.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Lucy Garlow, dated January 11, 2020:

65-A The commenter expresses her support of Liberty Charter High School's new location. The commenter has three sons enrolled in the Literacy First Charter

School program and states the school creates a learning environment where every child can succeed. The commenter continues to say having an opportunity to have a new high school built in El Cajon in close proximity to the k-8 schools will allow greater continuity between the schools for the kids.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Tasha Smith Godinez, dated January 11, 2020:

66-A The commenter expresses her family's "love" for the Literacy First Charter School and why the high school is needed. The commenter states the teachers and leadership at the school are top rate and there is no better place for children to learn. The commenter continues by stating the leadership at Literacy First Charter School works hard to not only teach children to succeed academically, but also to instill strong character traits, patriotism, and grit.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Mary Hermosillo, dated January 11, 2020:

67-A The commenter states she has two daughters that attend Literacy First Charter Schools, are "extremely happy" with the program, and would like to continue it. The commenter further states the difference it would make to have the high school located in El Cajon to save time and stress in getting the kids to different campuses.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Natalie Howard, dated January 11, 2020:

68-A The commenter states her "love" for the school and that staff strive to give each child the attention and education they deserve.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Abel Juarez, dated January 11, 2020:

69-A The commenter states he is a parent of two Literacy First Charter School students and believes a permanent high school with this program would help his children as much as it would the youth in the community.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Alba Katz, dated January 11, 2020:

70-A The commenter lives in El Cajon and two of his children attend a Literacy First Charter School. The commenter continues to state the new high school is so important in El Cajon to continue the program from his kids k-8 education. The commenter asks to support the project.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Rihab Matti, dated January 11, 2020:

71-A The commenter states the need for a literacy first high school in El Cajon for the school to be nearby most of the students who live in El Cajon and his kids.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Carol Michael, dated January 11, 2020:

72-A The commenter asks to please consider the new high school for the kids and there is a great generation of young children being raised up from literacy first charter schools.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Gina Mikhail, dated January 11, 2020:

73-A The commenter states she has enrolled her kids in Literacy First Charter Schools where every student is known. The commenter concludes by asking to “help us build the new high school for better future of our kids”.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Reven Namo, dated January 11, 2020:

74-A The commenter states two of his kids currently go to the middle school and highly recommending building the high school in El Cajon.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Leslie Giampa, dated January 11, 2020:

75-A The commenter states she has children who attend a Literacy First Charter School. The commenter states how impressed she is with the quality of education her children are receiving and are excited for the high school to be located in El Cajon. Further, the commenter states the majority of the student live in east county and the other three campuses are also located in El Cajon. The commenter concludes by stating she hopes the County will approve the project.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Kyle Oliphant, dated January 11, 2020:

76-A The commenter states his position against the project because of the potential impact to traffic. The commenter compares the proposed school to the Valhalla and Hillsdale schools stated the traffic is already bad.

Please see response 5-E for further information about traffic. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Amir Oraha, dated January 11, 2020:

77-A The commenter that's it is important to have the Literacy First Charter School high school in El Cajon for his children to continue their high school years in a school where the programs are aligned with their K-8 experiences.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Elizabeth Perez, dated January 11, 2020:

78-A The commenter states how important she feels it is to have a new high school for her kids because the Literacy First Charter School has been an excellent school.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Angelica Retano, dated January 11, 2020:

79-A The commenter states she has two children currently enrolled in a Literacy First Charter School and her two oldest have attended the school. The commenter further expresses her excitement to have a permanent high school.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Carolyn Verhoye, dated January 11, 2020:

80-A The commenter states she is an educator at the Literacy First Charter School Junior Academy and voices her "enthusiastic support" for the proposed location of the future charter high school. The commenter urges the decision makers to support the planning and construction of the school.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Ledia Alnagar, dated January 12, 2020:

81-A The commenter states she has daughters which attend Literacy First Charter Schools and hopes to have the high school opened in El Cajon for convenience.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Timothy Bottenberg, dated January 12, 2020:

82-A The commenter states he is a Junior at Liberty High School and feels fortunate to go to a charter school. The commenter asks for the consideration of moving the Liberty Charter school.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Suolleen Mansor, dated January 12, 2020:

83-A The commenter states the need for a high school in El Cajon because it is very important for their kids to continue their education at a Literacy First Charter School.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Balsam Mikha, dated January 12, 2020:

84-A The commenter states their daughter started kindergarten at Liberty First Charter School and wishes to see their daughter continue her education with the same program.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Amal Shattah, dated January 12, 2020:

85-A The commenter recommends and prefers the proposed location of the school in El Cajon because of how much driving time would be reduced.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from the Valle de Oro Community Planning Group, dated January 12, 2020:

86-A The Valle de Oro Community Planning Group (VDO CPG) states the motion that was unanimously approved at the planning group meeting to “object to the Mitigated Negative Declaration and ask for a full Environmental Impact Report (EIR) based on concerns brought up [at the January 7, 2020 VDO CPG meeting].” These concerns included community character and benefit, safety, aesthetics, traffic by the public, the Grossmont Union High School District, and our Board”.

Please see responses 5-E and 5-G for further information about traffic as well as further information about the MND, including community character, safety, and

visual resources (aesthetics), and the CEQA requirement of preparing an EIR. No changes were made to the IS and MND as a result of this comment.

86-B The VDO CPG comments on the specifics of the concerns raised for the project including the following: only one mention of Valhalla school and not addressed in public services, air quality emissions during construction, traffic analysis not including Valhalla, and no analysis of potential conflicts with neighboring businesses.

Please see response 5-G for further information about public services and air quality. In addition, please see response 5-E for further information about traffic. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Brianne Walker, dated January 12, 2020:

87-A The commenter asks to allow Literacy First Charter School to build the new campus for more families to experience “the greatness of our school family”.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Blake Woods, dated January 12, 2020:

88-A The commenter states his excitement for “getting closer” to having a brand-new high school in El Cajon. The commenter believes the school has addressed all of the County’s concerns regarding traffic, parking, environmental impact and architecture that “planners” had concerns about. The commenter concludes by asking to make this opportunity available to more kids in East County, by opening the El Cajon location as soon as possible.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Zuhair, dated January 12, 2020:

89-A This comment states how “LFCS” is an amazing school and how the high school is needed in El Cajon.

LFCS is an acronym used for Literacy First Charter School. This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Merna Alkatib, dated January 13, 2020:

90-A The commenter states the concerns for the project including high volume of cars on “Chase rd”, parents using the shopping center before and after school, parking spaces available at the school, and walking safety.

Please see response 5-E for further information about traffic and safety. No changes were made to the IS and MND as a result of this comment.

Response to comments received from James Wells, dated January 13, 2020:

91-A The commenter expresses his support for the approval of the Liberty First Charter High School as a parent and an immediate neighboring homeowner.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Tina Wells, dated January 14, 2020:

92-A The commenter states how grateful he is for the education his children receive with Liberty First Charter School. The commenter continues by stating education is not a “one size fits all” and he is pleased to have an alternative, small high school.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Christie Haigh, dated January 15, 2020:

93-A The commenter expresses her “deep discomfort” for the Liberty High School project on the corner of Chase Avenue and Jamacha Boulevard. The commenter states the 2014 data is outdated and should not be used to support a project in 2020. The commenter further states the data is no longer accurate because it does not take into account community changes in the area using the examples of “almost 40 houses in the Fuerte Ranch Estates” that are scheduled to be built next to Fuerte Elementary on Fuerte Drive, and that Fuerte Elementary has grown by over 10%, or 60+ families.

Please see response 5-E for information about traffic. No changes were made to the IS and MND as a result of this comment.

93-B The commenter is concerned about students who would be biking to Valhalla and will be placed in danger by the “extreme” increase in traffic by the project. The commenter states because of the challenge for “Liberty drivers” to access the parking entrance due to traffic number and speeds, drivers would likely be in a hurry to access Liberty and not pay attention to students biking or walking down Chase.

Please see response 5-E for information about traffic and safety. No changes were made to the IS and MND as a result of this comment.

93-C The commenter is concerned about placing teenagers next to a strip mall that includes bars, a liquor store, a hookah lounge and a vaping shop. In addition, the

commenter states the 7-11 on the opposite corner will tempt teens to cross Chase for snacks, vaping, cigarettes or liquor.

Please see response 1-A for information about the school proximity to the liquor store, hookah lounge, vaping shop and 7-11. No changes were made to the IS and MND as a result of this comment.

- 93-D The commenter is concerned about the amount of parking availability for the project. The commenter further discusses the lack of parking on either Chase or Jamacha.

Please see response 5-E for further information about traffic and parking. No changes were made to the IS and MND as a result of this comment.

- 93-E The commenter is concerned with the back-up of cars on Chase and Jamacha because only 28 cars can fit in the drop off zone. Further, the commenter believes parents will be tempted to pull into the stores on the north side of Chase where students would need to cross Chase.

Please see response 5-E for further information about traffic. No changes were made to the IS and MND as a result of this comment.

- 93-F The commenter is concerned about parking for students and visitors to the school for sports or after school activities. The commenter further states this lack of parking may require parking in the neighborhoods around the school or in the shopping center parking lot.

Please see response 5-E for information about traffic and parking. No changes were made to the IS and MND as a result of this comment.

- 93-G The commenter is concerned about the possibility of the school exceeding their initial enrollment estimates and requiring additional parking space.

The project, if approved, would be subject to a Major Use Permit which governs the site, including the number of student enrollees. If the project wished to increase the enrollment estimates in the future, a modification to the Major User Permit would be required pursuant to Zoning Ordinance Section 7378. The modification would be subject to CEQA and would be required to comply with the County Zoning Ordinance Section 6750, Parking Regulations, for number of parking spaces required. In addition, please see response 5-E for further information about traffic. No changes were made to the IS and MND as a result of this comment.

- 93-H The commenter is concerned about potential road rage accidents. The commenter states there are currently multiple times a day where eastbound Chase backs up with cars and would get much worse. The commenter states cars turning from Jamacha to Chase, and trying to access the left turn lane into Liberty, will back up the huge amount of traffic which accesses westbound chase to get to the "8

Freeway". The commenter further states the drop zone will fill up and back up in both directions which would keep cars from progressing to Jamacha north, crossing Chase to Valhalla or the neighborhoods, or turning onto Jamacha to access "Hillsdale Middle".

Please see response 5-E for information about traffic and safety. No changes were made to the IS and MND as a result of this comment.

- 93-I The commenter is concerned about potential car accidents. The commenter states the eastbound turn from Chase to Liberty is before Chase splits to turn left onto Jamacha, straight across Chase (access Valhalla or neighborhoods), or right onto Jamacha (access Hillsdale Middle or Rancho/94W to 125). The commenter believes there is currently "a lot going on" and questions adding an additional 450 teenagers.

Please see response 5-E for information about traffic and safety. No changes were made to the IS and MND as a result of this comment.

- 93-J The commenter is concerned with the amount of time it will take residents to get out of their homes to go to the schools or work, with the addition of the school. The commenter is also concerned with the senior residents in the community. The commenter states under current conditions, it can take 5-10 minutes to turn onto Chase Blvd or turn from Chase Blvd to one of the feeder roads. The commenter further states cars travel at high speeds on Chase towards Jamacha and is concerned with the amount of time cars will have to slow down for the cars backed up onto Chase trying to access Liberty.

Please see response 5-E for information about traffic. No changes were made to the IS and MND as a result of this comment.

- 93-K The commenter does not believe there is need for an additional school, stating "GUHSD" is not at capacity and there are already good charter school options within the district. The commenter further states the support for the school is not from neighborhood residents and suggests Alpine as a possible location. The commenter concludes by stating the projecting has not been thoroughly vetted in the County and will regret adding such a high-density project to this area.

The project is proposed for the location within the El Cajon area where both the Liberty Charter Elementary School and Middle School are located. In addition, pursuant to the qualitative VMT analysis provided in the Air Quality Assessment, the large majority of students who attend Liberty Charter Elementary and Middle School that would likely attend the high school in the coming years, live within the El Cajon area. This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Abdul Mateen, dated January 15, 2020:

94-A The commenter is supportive of the literacy first charter high school in El Cajon. The commenter's children are currently studying at the Literacy First Charter School.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Lesli Navarro, dated January 15, 2020:

95-A The commenter states how important that the "LFHS" be carried out for the future of her children, good education and values. The commenter asks to "think about our children" and accept the project.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Karen Beck, dated January 16, 2020:

96-A The commenter states he is against the project.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from the Department of Transportation, dated January 16, 2020:

97-A This comment by the California Department of Transportation (Caltrans) is introductory. Caltrans states they are thankful for including them in the MND review for a project located near Jamacha Road, or State Route 54 (SR-54). Caltrans states how important the traffic impact study (TIS) is to determine the project's impacts to State facilities and to propose appropriate mitigation measures.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

97-B The commenter states the geographic area examined in the TIS should also include all regionally significant arterial system segments and intersections, including State highway facilities where the project will add over 100 peak hour trips. State highway facilities that are experiencing noticeable delays should be analyzed in the scope of the traffic study for projects that add 50 to 100 peak hour trips.

The Traffic Study prepared for the project did include all road segments and intersections where there were more than 25 peak hour trips per County Guidelines for Determining Significance for Traffic. In addition, please see

response 5-E for further information about traffic. No changes were made to the IS and MND as a result of this comment.

- 97-C The commenter states a focused analysis may be required for project trips assigned to a State highway facility that is experiencing significant delay, such as where traffic queues exceed ramp storage capacity.

Jamacha Road (SR 54) road segments and intersections were analyzed and are operation at an acceptable LOS in the existing condition. In addition, please see response 5-E for further information about traffic. No changes were made to the IS and MND as a result of this comment.

- 97-D The commenter states the TIS could also consider implementing vehicle miles traveled analysis into the modeling projections.

SB 743 and State Office of Planning and Research guidance determined that projects must use VMT to analyze transportation impacts after July 1, 2020 and projects that were circulated for public review prior to that date were not subject to the new regulation. In addition, please see response 5-E for further information about traffic and VMT. No changes were made to the IS and MND as a result of this comment.

- 97-E The commenter states any increase in goods movement operations and its impacts to State highway facilities should be addressed in the TIS.

The project does not significantly increase goods movement operations to the State Highway System. In addition, please see response 5-E for further information about traffic. No changes were made to the IS and MND as a result of this comment.

- 97-F The commenter states the data used in the TIS should not be more than 2 years old and asks to provide Synchro Version 10 files along with early coordination with Caltrans.

The County agrees that the data used in the TIS is older than 2 years old. However, the most recent published Caltrans count data for Jamacha Road in both directions from Chase Avenue are less than what was used in the synchro analysis to determine the projects impacts. Therefore, even though the data in the TIS is older than 2 years old, it can be considered conservative in comparison to the recent published counts. In addition, please see response 5-E for further information about traffic. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Shawn Dennison, dated January 16, 2020:

- 98-A The commenter states he is against the charter school location due to the existing traffic. The commenter additionally points to the proximity of Valhalla High School

and Vista Grande Elementary to the proposed location and states the school would contribute to an already dangerous situation because he sees “emergency vehicles heading there virtually every day”.

Please see response 5-E for information about traffic and safety. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Cathy Jezierski, dated January 16, 2020:

99-A The commenter does not believe the Liberty Charter site on the corner of Chase and Jamacha Road in El Cajon is the correct location for the school because of the traffic on Chase, speed cars travel at, and people running red lights at the intersection of Chase and Jamacha. The commenter believes the project will add to the congestion.

Please see response 5-E for information about traffic. No changes were made to the IS and MND as a result of this comment.

99-B The commenter states the proximity of Valhalla High School and Steele Canyon High School and raises the question of why a third school is needed. The commenter further states Alpine does not have a high school and suggests the proposed school be located there.

The project is proposed for the location within the El Cajon area where both the Liberty Charter Elementary School and Middle School are located. In addition, pursuant to the qualitative VMT analysis provided in the Air Quality Assessment, the large majority of students who attend Liberty Charter Elementary and Middle School that would likely attend the high school in the coming years, live within the El Cajon area. This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment

99-C The commenter does not believe the school should be located so close to a liquor store and vaping store.

Please see response 1-A for further information about the school proximity to the liquor store and vape shop. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Susan Kellstrom, dated January 16, 2020:

100-A The commenter states he lives a few blocks away from the Jamacha and Chase intersection and can attest to the increased activity every day that now occurs due to Valhalla High School traffic. The commenter believes any additional traffic to this area would be “catastrophic in the face of emergencies, such as wildfires”. The commenter states the local roads are inadequate to accommodate emergency traffic trying to evacuate fire areas, especially in the event of a fire occurring during school hours. The commenter does not believe widening the roads can alleviate the situation and further states the proposed sand mining project at the

Cottonwood Golf Course would further exacerbate the potential for “a traffic nightmare of disastrous proportions”.

Please see response 5-E for information about traffic. In addition, please see response 5-G for information about wildfire. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Donna McKenzie, dated January 16, 2020:

101-A The commenter states her opposition to the location of the Liberty Charter High School on Chase and Jamacha. The commenter is concerned for the safety of the students because of the potential traffic congestion and crossing Chase to get to the 7/11.

Please see response 1-A for further information about the school proximity to the 7/11. In addition, please see response 5-E for further information about traffic. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Linda Shoemaker, dated January 16, 2020:

102-A The commenter states her family has lived on Chase Avenue for 30 years and borders the proposed school on the South West corner of the lot. The commenter is concerned for the public safety problems the school would create and does not believe it can be mitigated. The commenter further states Chase Avenue is one lane in each direction west of the proposed development and is impacted by traffic volumes from students attending Valhalla High School, Hillsdale Middle School, and Fuerte Elementary School during the normal morning and afternoon pick-up times. The commenter continues by stating the bike lane/shoulder in front of her driveway is regularly used as a second lane to turn right on Jamacha when traffic backs up.

Please see response 5-E for information about traffic and safety. No changes were made to the IS and MND as a result of this comment.

102-B The commenter states the traffic conditions and issues she has witnessed as a current resident such as traffic on Chase Avenue to get to Hwy 94 and Interstate 8, as well as morning and afternoon traffic from “Jamacha to Chase lane”. The commenter believes the two unprotected turn lanes proposed for the project on Chase Avenue will significantly impact traffic on Chase Avenue in both directions. The commenter also believes that driveways along Chase would be used as turnouts and drop off points by cars hoping to avoid turning into the parking lot. The commenter states a current and comprehensive traffic study on Chase Avenue should be required.

Please see response 5-E for further information about traffic, parking and safety. No changes were made to the IS and MND as a result of this comment.

102-C The commenter states the proposed onsite parking appears to be inadequate for 400 students daily, especially for when special events at the school would be held. The commenter continues by stating no offsite parking exists in the vicinity of the school to accommodate overflow parking. The commenter is also concerned with the potential public safety impact of the school and believes students would not use crosswalks but would cross Chase Avenue to reach the 7 Eleven before and after school.

Please see response 1-A for information about the school proximity to the 7/11. In addition, please see response 5-E for further information about traffic. No changes were made to the IS and MND as a result of this comment.

102-D The commenter is concerned how the project may impact the nearby businesses with liquor licenses being placed within 300 feet of a school. The commenter further states she is concerned for the school to be located near a vape shop and hookah lounge.

Please see response 1-A for further information about the school proximity to the liquor store. No changes were made to the IS and MND as a result of this comment.

102-E The commenter is concerned with the level of the sound from the school and believes the 70db would be of greater magnitude than what the "rural community" can cope with.

Please see response 5-G for information about noise. No changes were made to the IS and MND as a result of this comment.

102-F The commenter states she believes the project is incompatible with the immediate and surrounding area and is not in the public interest.

Please see response 5-G for further information about Land Use and Planning and Visual Resources (Aesthetics). No changes were made to the IS and MND as a result of this comment.

102-G The commenter asks how the visual impacts of the facility's parking lot lighting on the surrounding neighbors would be addressed during night and sporting events.

Please see response 5-G for further information about Visual Resources (Aesthetics). No changes were made to the IS and MND as a result of this comment.

102-H The commenter believes the biological resources report "downplayed" the hawk population by only stating 1 hawk. The commenter states she regularly sees 4 adults plus juveniles annually that next in the palm trees on the West side of the subject property. The commenter also mentions seeing coyotes on the property at night and worries about their habitat being minimized by development.

The County agrees with the commenter that the Biological Resources Report prepared for the project disclosed that one red-tailed hawk was observed in the sole eucalyptus tree onsite. The red-tailed hawk is federally protected under the Migratory Bird Treaty Act (MBTA). The project is required to conform to the MBTA and as a condition of approval, no brushing, clearing and/or grading shall be allowed during the breeding season of avian species and raptors, defined as occurring between January 15th and September 15th. The condition may only be waived through written concurrence from the US Fish and Wildlife Service and the California Department of Fish and Wildlife, provided that no nesting avian species or raptors are present in the vicinity of the brushing, clearing and grading. In addition, coyotes are not considered special status species or County sensitive animals. No changes were made to the IS and MND as a result of this comment.

102-I The commenter asked for the project to be required to follow “the full Environmental Impact Review (EIR) process” to fully identify impacts to the environment and appropriate mitigation.

Please see response 5-G for further information about the MND and the CEQA requirement of preparing an EIR. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Claudia Ayala, dated January 17, 2020:

103-A The commenter states her opposition for the project as a resident at 2044 George Maria Way who travels on Chase Ave. every day. The commenter believes the congestion would only get worse with the building of the school.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Marisa Janine-Page of Caldarelli Hejmanowski Page Leer, dated January 17, 2020:

104-A The commenter states their opposition to the Liberty Charter High School due to the “many violations of the California Environmental Quality Act, the substantial hardship and impact to local businesses, and the extraordinary dangers and high likelihood of loss of life of children it will cause”. The commenter further states the decision should not be based on the likability of the school, but on whether the project complied with the environmentally important requirements of CEQA in providing the County and public with the required level of accurate and current information upon which to base a decision. The commenter does not believe the MND adequately addresses the potential impacts to the Rancho San Diego/El Cajon community or the children and family who attend Liberty Charter High School.

Please see response 5-G for further information about the MND and the CEQA requirement of preparing an EIR. No changes were made to the IS and MND as a result of this comment.

- 104-B The commenter states the project seeks to eradicate the natural sanctuary and the wildlife who grace the proposed lot currently. The commenter states the proposed lot currently offers natural open space to the neighbors, community, hawks who nest within the trees, red fox, and bunnies. The commenter also believes the proposed school would be a “gated, noisy, flood-lighted, traffic nuisance” that would be used by families who do not live in the neighborhood.

The project site is not designated as “Biological Open Space” nor is a Biological Open Space Easement recorded over the property. The project’s proposed land use type is consistent with the General Plan Land Use Designation and Zone with the application of a Major Use Permit. In addition, the project is required to mitigate impacts to the onsite non-native grassland community through the purchase of 2.62 acres of Tier III habitat, or higher, within the Multiple Species Conservation Plan South County Subarea. Further, please see response 5-E for information about traffic and response 5-G for information about Noise and Visual Resources (Aesthetics), which addresses light. No changes were made to the IS and MND as a result of this comment.

- 104-C The commenter does not believe there is any benefit to the community by building another high school just 400 feet from an underpopulated high school. The commenter states two teens have lost their lives in traffic accidents on their way to/from their high schools within 5 miles of the project. The commenter also states Chase avenue is a main thoroughfare for students attending Steele Canyon, Valhalla, or Christian and current traffic conditions cause drivers to block driveways and cross the center divider and narrow walkways to get around traffic congestion. The commenter continues by stating students run across the street through traffic hazards every morning at the project site. The commenter believes the proposed traffic mitigations are unreasonable and do not account for the students and parents who would cross the center divider above Fuerte heading to Valhalla, Steele Canyon, Hillsdale, and Cristian.

The project is proposed for the location within the El Cajon area where both the Liberty Charter Elementary School and Middle School are located. In addition, pursuant to the qualitative VMT analysis provided in the Air Quality Assessment, the large majority of students who attend Liberty Charter Elementary and Middle School that would likely attend the high school in the coming years, live within the El Cajon area. Further, please see response 5-E for further information about traffic and safety. No changes were made to the IS and MND as a result of this comment.

- 104-D The commenter is concerned for the proximity of bars, liquor stores, vape shops, and a hookah lounge “and their easily accessible trash receptacles” to the project.

The commenter does not believe the MND includes the analysis or mitigation necessary to keep the students and businesses safe.

Please see response 1-A for further information about the school proximity to the liquor store, vape shop, hookah lounge “and their easily accessible trash receptables”. No changes were made to the IS and MND as a result of this comment.

- 104-E The commenter quotes from the Code of California Regulations by stating the lead agency shall consider direct physical changes to the environment which may be caused by the project and reasonably foreseeable indirect physical changes in the environment. The commenter further states CEQA only excuses the preparation of an EIR and allows the preparation of a negative declaration when 1) the initial study identifies potentially significant effects on the environment but either revisions in the plans would avoid or mitigate the effects, and 2) the initial study shows there is no substantial evidence that the project may have a significant effect on the environment. The commenter believes the project violates CEQA because the project impacts to Biological Resources, Utilities & Service Systems and Air Quality, are substantially understated and are not mitigated to a level of insignificance, as mandated by law.

Please see response 5-G for information about the MND and the CEQA requirement of preparing an EIR. No changes were made to the IS and MND as a result of this comment.

- 104-F The commenter believes there is substantial evidence that the initial study fails to identify all environmental factors potentially affected by the project, including Aesthetics, Noise, Recreation, Greenhouse Gas Emissions, Land Use & Planning, Transportation, and Public Services. The commenter specifically points to traffic stating that no viable mitigation exists. The commenter quotes from court cases such as *Mejia v. City of Los Angeles*, *Architectural Heritage Ass’n v. County of Monterey*, and *No Oil, Inc. v. City of Los Angeles* to point to “a low threshold for initial preparation of an EIR” whenever it can be “fairly argued” on the basis of substantial evidence that the project may have a significant environmental impact. The commenter continues by stating that it was clear in the Valle de Oro Community Planning Group meeting held on January 7, 2020, that the residents of Rancho San Diego/El Cajon expresses “overwhelming and intense concerns” about the impacts from the project, especially traffic.

Please see response 5-G for further information about the MND, the CEQA requirement of preparing an EIR, and information on the issue areas of Visual Resources (Aesthetics), Noise, Recreation, Greenhouse Gas Emissions, Land Use and Planning, and Public Services. In addition, please see response 5-E for further information about traffic. No changes were made to the IS and MND as a result of this comment.

104-G The commenter states that there is a fair argument, based on substantial evidence, that traffic, parking, lighting, noise, wildfire, public resources, greenhouse gas emissions, and “other” have not been mitigated to a level of insignificance. The “other” states the proximity of the proposed school to two bars and one liquor store. The commenter concludes that without the above referenced issues being properly addressed and studied in an EIR, an informed decision on the project cannot legally be made.

Please see response 5-G for information about the MND, the CEQA requirement of preparing an EIR, and information in the issue areas of Visual Resources (Aesthetics) which addresses lighting, Noise, Wildfire, Public Resources, and Greenhouse Gas Emissions. In addition, please see response 5-E for further information about traffic and parking and response 1-A for information about the school proximity to the bars and liquor store. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Helen Dagnino, dated January 17, 2020:

105-A The commenter states her opposition to the new charter school on Chase and Jamacha and asks to “please find another location” due to traffic.

Please see response 5-E for information about traffic. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Leslie Didier-Paquin, dated January 17, 2020:

106-A The commenter states the opposition to the proposed location for the Liberty Charter School. The commenter lives on Penasco road and states she will be negatively impacted by the increased traffic.

Please see response 5-E for information about traffic. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Steven Dillingham, dated January 17, 2020:

107-A The commenter is opposed to the “negative declaration presented to the committee” by Liberty Charter High School and opposed to the building of the charter school on the Chase Avenue/ Jamacha Road intersection. The commenter believes the school will place an undue burden on the current residents while serving mostly students who do not live in the immediate area.

Please see response 5-G for information about the MND and the CEQA requirement of preparing an EIR. In addition, please see response 5-E for information about traffic. No changes were made to the IS and MND as a result of this comment.

107-B The commenter states most of the residents in the area had no idea Liberty charter was planning on building a high school and believes the community was not duly informed about the committee hearing until the night before. The commenter asks to please consider making any further meetings concerning Liberty well known throughout the community.

This commenter has been added to the list for project notifications. Pursuant to existing state regulations, public notices are required at project application, the onset of public disclosure for the preparation of the environmental documentation, and 10 days prior to the project hearing. The noticing must include a minimum of 20 surrounding property owners, as well as all property owners within a 300-foot radius of the project site, whichever is greater. Project Notices were mailed during project application, public disclosure period, and 10 days prior to hearing, to 27 nearby property owners, in compliance with the State regulations. In addition, the County notified other individuals independently who have reached out to obtain more information about the project. In addition, in the County of San Diego, the Community Planning Group (CPG) is the responsible party to inform the public about the upcoming CPG meetings. It is the County's understanding the CPG did inform the public about the upcoming meeting for the project. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Richard Fossett, dated January 17, 2020:

108-A The commenter states he lives a block away from the proposed high school and traffic is very bad, especially in the afternoon. The commenter states the only access to the proposed school would be from Chase Avenue where traffic already backs up in a long line up the hill on Chase Avenue to turn at the traffic signal located at the Chase Avenue and Jamacha Road junction.

Please see response 5-E for information about traffic. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Alyssa Burley, dated January 17, 2020:

109-A The commenter is concerned with the building of a Liberty Charter High School near the intersection of Chase & Jamacha in El Cajon. The commenter has lived in the community for 20 years and has had to travel to and from work on Chase because it is the only road which leads to highway 8. The commenter is concerned with the addition of 500 cars to the road and the potential for kids to get hit by cars trying to run to 7/11.

Please see response 5-E for further information about traffic and safety. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Terry Oneill, dated January 17, 2020:

110-A The commenter is a resident in the area and travel through Chase Road and Jamacha Boulevard every day. The commenter states the traffic already backs up on Chase under current conditions and is concerned for the children's safety.

Please see response 5-E for further information about traffic and safety. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Ardawna Starks, dated January 17, 2020:

111-A The commented states she wishes Liberty Charter High School would remain at the same place or in the same area because the relocation of the school would take double or triple the amount of time to travel to the school. She further states the bus or trolley is not an option for her child because of the "too many violent and sad stories" of people's actions on a bus or trolley.

Your comment has been added to the record. However, this comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Lajan Talkou, dated January 18, 2020:

112-A The commenter is an advocate for the proposed school location stating kids are provided education k through eight in El Cajon but are then required to move onto high school very far away. The commenter states Liberty Charter High School is a smaller and a respectable school and traffic would not range for too long because of the size of the school.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Lenny Copenrath, dated January 19, 2020:

113-A The commenter states he is a resident of the project area and a board member for the Hidden Mesa Estates HOA. The commenter's main concern with the project is traffic because of the various schools in the area and the project would only make it worse.

Please see response 5-E for information about traffic. No changes were made to the IS and MND as a result of this comment.

113-B The commenter states his understanding of the widening of Chase in an attempt to accommodate the additional traffic but views this as inadequate. The commenter believes the community truly understands the increasing traffic problems in the area and the dangerous high speeds of drivers on both Chase and Jamacha.

Please see response 5-E for further information about traffic and safety. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Danielle Nunez, dated January 19, 2020:

114-A The commenter believes it is imperative that the school be located on the property in El Cajon. The commenter understands traffic is a concern but believes the benefits to the community outweigh the costs and will be a great addition to the community.

This comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment.

Response to comments received from Chance Agnew, dated January 20, 2020:

115-A The commenter states his kids attend Valhalla “HS” and Fuerte Elementary and his biggest concern with the location of the school is the traffic. The commenter states there is a shortage of “CHP resources” that have jurisdiction over Fuerte Dr. and suggests purchasing the land for a “CHP/Sheriff Branch office” to help protect the community.

Thank you for your comment. However, this comment does not raise any new CEQA-related issues. No changes were made to the IS and MND as a result of this comment. In addition, please see response 5-E for further information about traffic.

From: D&J Edgar
To: [Russell, Denise](#)
Subject: Liberty High School PDS2015-MUP-15-027; PDS2015-ER-15-14-010
Date: Friday, December 13, 2019 1:28:25 PM

Ms Russell,

1-A

Is this high school planned for the land behind the liquor store at the S/E corner of Chase Av & Jamacha Rd?

If so, I think it is a very poorly considered idea. Aside from the proximity of the liquor store/bar shops, the additional traffic would be a congested nightmare for locals (like me living 1/2 mile away).

1-B

If this moves forward, at a minimum should be there be access off both Chase and Jamacha to relieve congestion at that very busy intersection.

1-C

Asking for comments (Dec. 12th letter for comment by Jan. 17th) over the busy holidays seems a way to evade community involvement and oversight.

1-D

Please accept this as a family very opposed to putting 483 people on a site with only ingress/egress at e/b Chase Ave. and only 161 parking spaces. There is limited to no street parking. 161 spaces are insufficient for even 25% of the students and staff, let alone visitors or for special events. This overflow will negatively impact the storefront businesses on the corner.

All in all, this proposal should be rejected and something with less density/traffic impact developed.

Sincerely,

Joyce Edgar
home owner
1454 Chase Ter
El Cajon, CA 92020

From: lindsey emerson
To: [Russell, Denise](#)
Subject: PDS2015-MUP-15-027; PDS2015-ER-15-14-010
Date: Saturday, December 14, 2019 2:33:30 PM

Hi,

2-A I wanted to reach out as to see if the County was accepting/ interested in public comment regarding **PDS2015-MUP-15-027; PDS2015-ER-15-14-010**. I drive my kids to Fuerte Elementary and live off Hidden Mesa. The intersection of Chase & Jamacha is a nightmare in the morning, adding a school right there would be detrimental to our community. The traffic report says 585 more daily trips through the area. That is unreasonable and unfair to community members trying to get to work and trying to get our kids to existing schools.

2-B I'm also curious to the shopping center with a smoke shop & liquor store. How can a school go next door?

Lindsey

Sent from my iPhone

From: D&J Edgar
To: [Russell, Denise](#)
Subject: Re: Liberty High School PDS2015-MUP-15-027; PDS2015-ER-15-14-010
Date: Wednesday, December 18, 2019 2:34:05 PM

Hi Ms Russell

Thank you for sending that link.

My concern is still that there is going to be a negative traffic impact in our community. Yesterday I tried to make a left turn onto Chase Ave from Chase Terrace at 3pm. The Eastbound traffic on Chase was backed up over one half a mile from the Jamacha Rd. intersection. While during the several minutes I sat evaluating the situation there were some breaks in the west lane, I could not cross to the east. I was heading to the Rancho San Diego Auto Club office and had to go via Avocado instead of Jamacha.

3-A

Vehicles trying to exit the school lot with existing 3pm traffic would have difficulty even though it appears all traffic would be required to exit eastbound.

Even when traffic is light enough to make the left turn, the left turn channel from e/b Chase to n/b Jamacha frequently backs up into the painted island, which allows the single e/b straight through lane to pass. The way I read the plan, the wide part of the painted island is to become a west bound turn lane to allow access into the school parking lot. Thus overflow from the left turn channel would block all straight through e/b access, unless vehicles traveled in the new right turn lane until passing the choke point, then crossed through the bicycle lane and back into the straight through traffic lane. Considering the current traffic, this would be difficult. With the addition of another 200+ cars, we are asking for gridlock.

3-B

The traffic pattern on the lot itself only provides about 100 linear feet for drop off. With cars in two lanes, this provides for 10 to 12 cars to load or unload at a time. Should only 113 students drive (parking spaces allocated for students) over 330 would probably need to be driven. Another part of the plan that will be very congested twice a day. The only part of the lot that will be underutilized is the bicycle spaces, as this is not a neighborhood school it is doubtful that 10% of the student body will ride bicycles to school.

3-C

Should this school be approved, it truly needs to provide vehicle access to and from Jamacha Rd. in addition to the Chase Ave access penciled in. Should there be an on campus incident during commute times, access for emergency vehicles would be extremely restricted.

While I have a personal bias against a high school in such close proximity to established businesses selling alcoholic beverages, my overwhelming objection to this plan is there is not enough parking and Chase Ave. as designed is not adequate to handle the increase in vehicle trips.

A concerned neighbor,

Joyce Edgar

> On Dec 18, 2019, at 12:07 PM, Russell, Denise <Denise.Russell@sdcounty.ca.gov> wrote:

>

> Hi Ms. Edgar,

>

> The proposed project does not include any additional traffic signals. The road improvements required are along the project's frontage and include widening of Chase Ave and adding a median for left turn into the project. The proposed improvements are reflected in the Tentative Map, located here:

>

> <https://www.sandiegocounty.gov/content/dam/sdc/pds/ceqa/LibertyCharter/Plot%20Plans.pdf>

>

> Please let me know if you have any other questions.

>

> Thank you,

>

> Denise Russell

> Land Use/Environmental Planner

> 858.694.2019

>

> -----Original Message-----

> From: D&J Edgar <djedgar1970@gmail.com>

> Sent: Monday, December 16, 2019 1:39 PM

> To: Russell, Denise <Denise.Russell@sdcounty.ca.gov>

> Subject: Re: Liberty High School PDS2015-MUP-15-027; PDS2015-ER-15-14-010

>

> Ms Russell,

>

> is there any plan to add a traffic signal or other access to help mitigate the traffic impact?

>

> Joyce Edgar

>

>> On Dec 16, 2019, at 1:26 PM, Russell, Denise <Denise.Russell@sdcounty.ca.gov> wrote:

>>

>> Ms. Edgar,

>>

>> You are correct, this is planned for the land behind the liquor store. I appreciate you taking the time to send in your comments. We will provide formal responses to your comments once the public review period is complete.

>>

>> Thank you,

>>

>> Denise Russell

>> Land Use/Environmental Planner

>> 858.694.2019

>>

>> -----Original Message-----

>> From: D&J Edgar <djedgar1970@gmail.com>

>> Sent: Friday, December 13, 2019 1:28 PM

>> To: Russell, Denise <Denise.Russell@sdcounty.ca.gov>

>> Subject: Liberty High School PDS2015-MUP-15-027; PDS2015-ER-15-14-010

>>

>> Ms Russell,

>>

>> Is this high school planned for the land behind the liquor store at the S/E corner of Chase Av & Jamacha Rd?

>>

>> If so, I think it is a very poorly considered idea. Aside from the proximity of the liquor store/bar shops, the additional traffic would be a congested nightmare for locals (like me living 1/2 mile away).

>>

>> If this moves forward, at a minimum should be there be access off both Chase and Jamacha to relieve congestion at that very busy intersection.

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>> Please accept this as a family very opposed to putting 483 people on a site with only ingress/egress at e/b Chase Ave. and only 161 parking spaces. There is limited to no street parking. 161 spaces are insufficient for even 25% of the students and staff, let alone visitors or for special events. This overflow will negatively impact the storefront businesses on the corner.

>>

>> All in all, this proposal should be rejected and something with less density/traffic impact developed.

>>

>> Sincerely,

>>

>> Joyce Edgar

>> home owner

>> 1454 Chase Ter

>> El Cajon, CA 92020

>



State of California – Natural Resources Agency
 DEPARTMENT OF FISH AND WILDLIFE
 South Coast Region
 3883 Ruffin Road
 San Diego, CA 92123
 (858) 467-4201
 www.wildlife.ca.gov

GAVIN NEWSOM, Governor
 CHARLTON H. BONHAM, Director



December 27, 2019

Denise Russell, Project Manager
 County of San Diego Planning & Development Services
 5510 Overland Ave., Suite 110
 San Diego, CA 92123E-mail: denise.russell@sdcounty.ca.gov

SUBJECT: Comments on the Notice of Intent (NOI) to adopt a Mitigated Negative Declaration for the Liberty Charter High School Project SCH# 2019129039

Dear Ms. Russell:

The California Department of Fish and Wildlife (Department) has reviewed the above-referenced Notice of Intent (NOI) to adopt a Mitigated Negative Declaration (MND) for the Liberty Charter High School Project.

Thank you for the opportunity to provide comments and recommendations regarding those activities involved in the Project that may affect California fish and wildlife. Likewise, we appreciate the opportunity to provide comments regarding those aspects of the Project that the Department, by law, may be required to carry out or approve through the exercise of its own regulatory authority under the Fish and Game Code.

4-A Department Role

The Department is California's Trustee Agency for fish and wildlife resources, and holds those resources in trust by statute for all the people of the State. (Fish & G. Code, §§ 711.7, subd. (a) & 1802; Pub. Resources Code, § 21070; California Environmental Quality Act [CEQA] Guidelines § 15386, subd. (a).) The Department, in its trustee capacity, has jurisdiction over the conservation, protection, and management of fish, wildlife, native plants, and habitat necessary for biologically sustainable populations of those species. (Id., § 1802.) Similarly for purposes of CEQA, the Department is charged by law to provide, as available, biological expertise during public agency environmental review efforts, focusing specifically on projects and related activities that have the potential to adversely affect fish and wildlife resources.

The Department is also a Responsible Agency under CEQA. (Pub. Resources Code, § 21069; CEQA Guidelines, § 15381.) The Department may need to exercise regulatory authority as provided by the Fish and Game Code. As proposed, for example, the Project may be subject to the Department's lake and streambed alteration regulatory authority. (Fish & G. Code, § 1600 et seq.) Likewise, to the extent implementation of the Project as proposed may result in "take" as defined by State law of any species protected under the California Endangered Species Act (CESA) (Fish & G. Code, § 2050 et seq.), related authorization as provided by the Fish and Game Code will be required.

Denise Russell, Project Manager
County of San Diego Planning & Development Services
December 27, 2019
Page 2 of 2

4-B

The Department also administers the Natural Community Conservation Planning (NCCP) program. The County of San Diego (County) currently participates in the NCCP program by implementing its approved MSCP Subarea Plan.

4-C

Project Location: The Liberty Charter High School is located at 1530 Jamacha Road, El Cajon, Valle de Oro Community Planning Area, Unincorporated San Diego County (APN# 498-330-39-00). The location is at the corner of Jamacha Road and Chase Avenue in El Cajon.

Project Description/Objective: The proposed project is construction of a charter high school (grades 9-12), operated by Literacy First Charter Schools (LFCS). Primary Project activities include construction of a 48,000 square-foot, two-story building, 161 parking spaces, an outdoor common area and patios, and a sports field. Access to the site would be provided by two driveways total connecting to Chase Avenue. Grading would consist of 23,500 cubic yards of balanced cut and fill. The existing Liberty Charter High School from its current location at 8425 Palm Street in Lemon Grove to the new location once construction is complete. The project would also require a Major Use Permit.

COMMENTS AND RECOMMENDATIONS

4-D

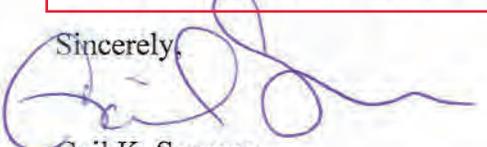
The Department offers the following comments and recommendations to assist San Diego County in adequately avoiding, minimizing, and/or mitigating the Project’s significant, or potentially significant, direct and indirect impacts on fish and wildlife (biological) resources.

If the proposer intends to mitigate using Option 1 then the Department requires the proposer to submit a Bill of Sale or Credit Sales Receipt instead of a copy of the purchase contract.

CONCLUSION

The Department appreciates the opportunity to comment on the NOI to assist San Diego County in avoiding, minimizing, and mitigating Project impacts on biological resources.

Questions regarding this letter or further coordination should be directed to Nasseer Idrisi, Senior Environmental Scientist (Specialist) at (858) 636-3159 or Nasseer.Idrisi@wildlife.ca.gov.

Sincerely,

Gail K. Sevens
Environmental Program Manager

cc: Office of Planning and Research, State Clearinghouse, Sacramento



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Tyree K. Dorward
(619) 525-1338
tyree.dorward@bbklaw.com
File No. 60315.00001

January 7, 2020

Attn: Denise Russell
County of San Diego, Planning & Development Services Project Processing Counter
5510 Overland Avenue, Suite 110
San Diego, CA 92123

Re: Grossmont Union High School District - Comments on Mitigated
Negative Declaration for Liberty Charter High School

Dear Ms. Russell:

On behalf of the Grossmont Union High School District (“District”), we provide the following comments on the Initial Study, Mitigated Negative Declaration, and supporting studies (collectively the “MND”) prepared pursuant to the California Environmental Quality Act (“CEQA”) for the Liberty Charter High School project (“Project”). The District understands the Project will be constructed on approximately 7.7 acres of vacant undeveloped land located at 1530 Jamacha Road, El Cajon (APN# 498-330-39-00) and will consist of a 48,000 square foot, two-story high school facility intended to serve up to 450 students.

5-A

As a preliminary note, the District is concerned that the CEQA analysis for the Project which consists of the proposed development of a brand new, substantially sized facility serving a large number of students was limited to only a MND instead of the much more thorough analysis that would have been required as part of a full Environmental Impact Report (“EIR”). As we know the County and the Project applicant are aware, CEQA creates a low threshold requirement for the preparation of an EIR. Generally, an EIR must be prepared when there is substantial evidence in the record to support a fair argument that a project may entail significant environmental effects; even if there is other substantial evidence there will not be such an impact. (*See San Bernardino Valley Audubon Soc. v. Metropolitan Water Dist.* (1999) 71 Cal.App.4th 382, 389.)

5-B

Here, while there are several areas of the MND that the District believes demonstrate a fair argument that the Project may have significant environmental impacts, the District’s comments on focus primarily on the potential impact on the Project to the District’s students and facilities. As noted in the MND, the Project will be constructed within approximately 3500 feet of the District’s Valhalla High School which serves approximately 2,230 students and staff. The District’s paramount concern is the safety and security of these students. To that end, the District notes that the MND makes only one mention of Valhalla High School and contains virtually no



BEST BEST & KRIEGER LLP
ATTORNEYS AT LAW

Attn: Denise Russell
County of San Diego
January 7, 2020
Page 2

analysis of the Project's impact on the District's students or facilities. With this overall comment in mind, the District has the following comments on the MND:

1. **Section III. Air Quality**

5-C While the Air Quality Assessment prepared by the Project applicant's consultant does note the location of Valhalla High School near the Project, the study contains no analysis of the potential impact of the Project's construction emissions on the District's students. Specifically, the potential exposure of students currently attending Valhalla High School who will be exposed to such emissions at least going to and from school near the construction site if they are traveling on Chase Avenue and/or Jamacha Road. This is particularly concerning given the MND's admission that construction operations would result in potentially significant diesel emissions. Accordingly, the District believes an EIR should be prepared with a more detailed analysis of the potential air quality impacts on District students with corresponding enhanced mitigation measures. In addition, an EIR would require an alternative analysis which would analyze other potential size, scope, and/or operational options for the Project which might lead to reduced impacts to Air Quality.

2. **Section XV. Public Services**

5-D Under CEQA, this section of a MND is required to address potential impacts on public services, including schools. However, here this section of the MND does not even acknowledge the Project's proximity to Valhalla High School nor any analysis of the Project's potential impacts to the District's students or educational programs. Such analysis should include, but not be limited to, any potential conflicts associated with the operation of another school facility in close proximity to Valhalla High School such as potential staggered start/end times, traffic coordination, safe routes to schools, student transportation, special events, etc. The District believes an EIR with appropriate analysis of these items, corresponding mitigation measures as well as Project alternatives is required.

3. **Section XVII. Transportation and Traffic**

5-E Here again, the MND and the "Focused Traffic Impact Study" contain no analysis whatsoever of the Project's potential impact on Valhalla High School and/or potential exacerbation of traffic impacts of the Project when considered with Valhalla High School's current operations. Specifically, there is no analysis of potential impacts associated with the Project's traffic and new additional 585 vehicle trips given its proximity to Valhalla High School. There is no analysis of the Project's potential impacts to the District's students or educational programs in terms of school start/end time and special event traffic issues, student transportation issues, and/or safe routes to school for District students walking or driving to



BEST BEST & KRIEGER
ATTORNEYS AT LAW

Attn: Denise Russell
County of San Diego
January 7, 2020
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school who may be impacted by Project traffic. Likewise, while Project vehicle trips are analyzed, there does not appear to be any analysis of the safety routes of ingress and egress the site for Project students who may walk and/or bike to school. The District believes that a full analysis of all of these traffic issues together with appropriate mitigation measures in an EIR is required.

4. General Comments

In addition to the above comments specific to the District's primary concerns associated with impacts to Valhalla High School's students and educational programs, the District also believes that there are several additional CEQA issues that are not adequately addressed in the MND:

- a) There does not appear to be any analysis of the potential conflicts associated with neighboring businesses and the Project's students. Specifically, we understand there is a Liquor Store, a Vape Shop, a Hookah Lounge, and a body & foot massage spa adjacent to the Project site. Even though a Major Use Permit may allow such a school use in a different zoning designation, a thorough analysis of potential conflicts, safety concerns, and/or student impacts should be incorporated in any CEQA review of the Project.

- b) Finally, if an appropriate EIR that adequately addresses the potential impacts to the District's students and facilities is prepared with a corresponding alternatives analysis, then impacts to biological resources, air quality, cultural resources, and greenhouse gas emissions might be further and/or better mitigated as well.

In conclusion, the District thanks the County of San Diego for the opportunity to review and comment on the proposed MND, and the District hopes the County will elect to require the Project applicant to comply with CEQA by going back and preparing a full EIR that adequately addresses the potential environmental impacts of the Project on the District's students and educational programs.



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ATTORNEYS AT LAW

Attn: Denise Russell
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If you have any questions or would like to discuss further, please feel free to contact the undersigned.

Sincerely,

A handwritten signature in blue ink, appearing to read 'Tyree K. Dorward'.

Tyree K. Dorward
of BEST BEST & KRIEGER LLP

cc: Alyssa Burley, Chair of the Valle De Oro Planning Group
Scott Patterson, Deputy Superintendent, Business Services GUHSD
Katy Wright, Executive Director Facilities Management GUHSD

January 7, 2020

Planning & Development Services
5510 Overland Avenue Suite 110
San Diego, CA 92123

RE: Liberty Charter High School; PDS2015-MUP-15-027; PDS2015-ER-15-14-010.

6-A

Please be advised that the location of proposed school should be denied due to current volume of both pedestrian and automobile traffic. There are currently plastic implanted cones to act as a median so traffic is not allowed to turn left onto Chase Ave for safety reasons.

On any given day, there are students and residents walking, running and/or riding bikes on Chase Ave and Jamacha Rd. While there are stores that abut Jamacha Rd, this is a residential area. Additional increase in traffic would negatively impact the residential and suburban ambience of the area.

6-B

The noise level of bells ringing every hour and the constant chatter of e In addition, please consider the impact of existing wildlife that inhabits that field.

6-C

It is hard enough to turn right currently out of the existing shopping plaza because of the already high volume of traffic congestion. It is highly recommended your department complete due diligence by conducting an hourly traffic assessment on Chase Ave prior to granting approval for this project. Unless this school is going to operate from 10am-2pm and/or 7pm-12am, this would not be a conducive location for additional traffic.

Please keep in mind both entrance/exits to the two existing shopping centers on the corner of Chase and Jamacha are right turn only for traffic safety.

Thank you.



Terri and Eric Horn
1942 Hidden Mesa Rd
El Cajon, Ca 92019
619-579-8779

Cordova, Lorena

From: lisa@amorteguy.com
Sent: Wednesday, January 8, 2020 9:04 PM
To: Russell, Denise
Subject: Liberty Charter High School

Denise Russell,

I am disheartened by the outcome of the meeting Tuesday night January 7th 2020 in regards to the building of Liberty Charter High School. I am one of the founders of the school and have been with Literacy First Charter Schools for the past 19 years. I am a teacher in our school system and believe we have great things to offer our students and having a High School for them to move to after being in our K-8 program is essential to the on going education of our students.

The students in our school are taught to love learning and as a result they love their school and their classmates. Because of this the presence of Liberty Charter School on Chase and Jamacha will not be a burden to those around but will be a positive thing. Our parents choose to send their students to our school and therefore are involved in their education and desired success for their child. We hold Parent/Student/Teacher Conferences with our students in High School each quarter helping build a positive relationship between Parents, Students and their Teachers. Not many High Schools can say they do quarterly conferences to involve the parents in their child's education. Because the students are involved in this conference process they take ownership to their learning. People who are vested in their own success are a positive influence. And because our teachers, parents and students desire to succeed the presence of a high school will not be a negative influence.

As an elementary teacher in our system it is important for my families to know they have a high school their child can go to when they finish their 8th grade year with us. Most of our families are from El Cajon so having the school located in El Cajon is key to their being able to attend and continue the stellar education they deserve. Our test scores show that our teachers, students and parents are committed to the success of students.

Please consider the positive impact this school will have on so many, future leaders, business people who will reside in El Cajon.

Lisa Amorteguy
Literacy First Charter Schools
Elementary Teacher

Cordova, Lorena

From: Justin Allen <justin.allen74@gmail.com>
Sent: Thursday, January 9, 2020 10:49 AM
To: Russell, Denise
Subject: Regarding new high school in El Cajon

Denise,

I am writing to let you know the positive impact Literacy First Charter Schools (LFCS) has made in my families life. As a resident of El Cajon, my wife and I were unsure where to send our children for elementary school. None of the local public schools were satisfactory when we reviewed their performances. With a single income at the time we could not afford a private school. We discovered LFCS and knew the educators running the school and knew our children would receive a high level education in a small classroom environment. That has been the case for the past 12 years. My oldest child is a junior at the high school now and the commute to Lemon Grove is tiring as is dealing with a temporary campus (and the Lemon Grove School District as they still own the campus). Liberty Charter High School (LCHS) has been looking for permanent home at least since my oldest was in Kindergarten. The location in El Cajon on Chase near Jamacha would ensure a more central location for the students coming up from the K8 program including my own children. One if the biggest reasons I have heard from 8th graders parents as to why they were not going to the LCHS was its location. I know NIMBY is strong in most the county, but I cannot think of a better neighbor than LCHS.

Thank you for your consideration,

Justin Allen

Cordova, Lorena

From: Lori Benitez <loribenitez311@gmail.com>
Sent: Thursday, January 9, 2020 9:15 PM
To: Russell, Denise
Subject: LCHS

Hello Mrs Russell!

I can't tell you how proud I am that my son goes to Liberty Charter High School. The school is not perfect, but it truly is amazing!!! The students are taught character! They are some of the most quality students I have ever met. They are getting an amazing education and if they desire to choose the correct school plan, they are very equipped to go to college.

I believe the community that our property will be built in will be more than pleased with the students that join their community. Most of them are so respectful and when the school is made aware that they were not, they make sure and deal with the student.

Thank you for your support!

Lori Benitez

9-A

Cordova, Lorena

From: Heers, Liberty <Liberty.Heers@cvesd.org>
Sent: Thursday, January 9, 2020 10:57 AM
To: Russell, Denise
Subject: LCHS

Good morning,

10-A I wanted to write you a quick note letting you know how excited we are that a new Liberty Charter High School will be built in El Cajon. Our son currently attends high school and when are excited for our younger son to be joining the LFCS family. We currently live in El Cajon and the thought that both our boys will be able attend a school in our neighbor is great news!! Thank you for your time.

Sincerely a very happy dad,
Liberty Bock-Heers

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Cordova, Lorena

From: Elizabeth Bottenberg <ebottenb@ucsd.edu>
Sent: Thursday, January 9, 2020 9:35 PM
To: Russell, Denise
Cc: steve.robinson@lfcsinc.org
Subject: Liberty Charter High School: The BEST kind of impact

I strongly advocate for Liberty Charter High School to have a home in El Cajon. I went to the LFCS program from K-12, and this school has fundamentally formed my character, integrity, and grit as a student to be as successful as I am at UCSD.

I live across the street from Valhalla high school, so of course I understand the concern of implementing another high school. Valhalla is a nightmare with loud football games, band practice, and commencement ceremonies. I could tell you the score of the soccer game from my own backyard. However, Liberty Charter is one of the few schools in the district comprised of the most uplifting staff, hardworking students, and welcoming community. The impact of Liberty Charter would be not only beneficial for El Cajon, but absolutely necessary for the rising generation. The next generation deserves to have the choice of attending a school that makes them feel known, cared for, and recognized. With Liberty Charter's small population of 400 students, students and alumni feel empowered to reach their potential.

Liberty Charter does not host obnoxious sporting events or even have prison-sounding bells to guide students to class. Liberty Charter advocates for taking responsibility of the impact that they make in their community, and they demonstrate this with the wonderful relationships they have with their other campuses on Mollison/Washington, Bradley/1st, and the Lemon Grove community. They are intentional with the work that they do, and they ensure that every aspect of their program is efficient and effective for students, families, and their neighbors.

I urge you to think about the incredible impact Liberty Charter will have for the El Cajon community, and take a stand with me to fight for schools that truly invest in their students.

--
Best,

Elizabeth

Cordova, Lorena

From: bottenbergs <bottenbergs@cox.net>
Sent: Thursday, January 9, 2020 12:21 PM
To: Russell, Denise
Subject: Liberty Charter High School

My 3 children attend Liberty Charter High School (Literacy First Charter) for the past 13 years (Daughter now at UCSD). I urge you to approve the move to the Chase/Jamacha location as it will be a tremendous asset to the community with minimal traffic impact.

12-A

I attended the community meeting on January 7 and heard the residents concerns about traffic. They compared our small school traffic to Valhalla and Steele Canyon.

We have 320 students, which is a fraction of the large schools. The traffic at that intersection is busy but the impact of our school traffic will be minimal. Our school does a stellar job of pick up and drop off to minimize traffic. The morning traffic is very light as students are dropped off anywhere from 7:30 a.m. to 8:10 a.m.. There is no heavy influx of traffic in the morning due to the varied time students arrive.

The afternoon school pick up is a process that takes less than 15 minutes. All cars are pulled into the school driveway and move once the child is picked up. Several Liberty staff, including Mr. Robinson, the VP, and security are there every single day to ensure the process is seamless.

12-B

There is rarely a time when traffic backs up onto the street. I understand the neighbors concern about students or parents parking in their driveway and neighboring retail. Our charter school has guidelines about parking which parents abide by. Should they fail to comply the issue is quickly addressed by Liberty staff.

I urge you, the board and the residents to visit Liberty Charter in the morning and afternoon to view our traffic and parking control. We have 4 schools and work hard to be good neighbors and would continue to do so in the new location.

I live in Hillsdale Ranch, right near Valhalla (my darlings will be able to walk to Liberty). The morning and afternoon is complete chaos due to the 3500+ students and what appears to be little school intervention with traffic. We can hear football games and graduation. Our school does not use a bell system or football. I also hear the bells from Vista Grande. The neighbors at Liberty Charter would not be subjected to many of the typical school noises.

12-C

Please feel free to give me a call if I can provide you with any additional information. Please support Liberty Charter.

Eileen Bottenberg
619 548 1311

1734 Lawndale Road
El Cajon, CA 92019

Cordova, Lorena

From: Tim Bottenberg <timbottenberg@cox.net>
Sent: Thursday, January 9, 2020 5:08 PM
To: Russell, Denise
Subject: Liberty Charter High School

Ms. Russell,

I would like to encourage you to vote in favor of the move of Liberty Charter High School to the Chase/ Jamacha location.

I too am concerned with traffic and noise at that location. I cross through that intersection multiple times a day, I live in Hillsdale Ranch just down the street off Chase Ave.

Comparing the traffic and noise impact Liberty HS would have with Valhalla HS is like comparing apples to oranges. Mornings and afternoons at Chase and Hillsdale are a disaster.

There is no traffic control and the drop off and pick up process lasts sometimes close to an hour. I personally learned to avoid that area.

I drop off occasionally and pick up every day from Liberty HS. I have never seen or encountered traffic in the morning. In the afternoons, the school controls traffic and the process lasts from 5 to 10 minutes without cars backing up into the street.

The proposed widening of Chase Avenue approaching Jamacha is needed. The volume of cars through that intersection is tremendous. The few cars going in and out of Liberty HS would have little or no impact on the already busy street.

Please support the move of Liberty Charter High School.

Thank you,

Tim Bottenberg
proud dad of 3 Literacy First and Liberty HS students

Cordova, Lorena

From: rana_oraha@yahoo.com
Sent: Thursday, January 9, 2020 9:45 PM
To: Russell, Denise
Subject: LCHS new high school

14-A Hello, I am Rana Dawood- Noaman, a mother's of three girls. Two of my girls, Lorieta and Leyia Habib attend the Liberty Charter High School as freshman. My other little girl, Loreen Habib, attends the elementary school Liberty Academy. Ever since my girls have started attending this school, I automatically fell in love with it for many reasons. The education the schools provide is over the top and has impressed me very well. The teachers take their time and work with every student passionately and are respectful to every student's ability to learn. The environment of this school is also something that has caught my attention as it is very friendly. Furthermore, when my high school girls were attending the schools in El Cajon, it was very easy to transport from home to school. However, the location of the High school now is putting difficulties. There is a lot of traffic in the mornings and in the afternoons, especially because me and my husband are full time workers. I would really appreciate if this new high school was approved. It would really help out a lot of families.

Best regarding,
Rana Dawood- Noaman

Cordova, Lorena

From: Nancy Dorame <jesushiked@gmail.com>
Sent: Thursday, January 9, 2020 9:53 PM
To: Russell, Denise
Subject: LFCS & LCHS

Dear Ms. Russell,

15-A Our 16 year old son, who is on the Autism Spectrum, has never thrived so much in all his years in school, than he has since attending LCHS. He loves his school! We are an East County family, that would be ever so grateful, to have LCHS in El Cajon.

Respectfully,

Nancy Dorame

Sent from my iPad

Cordova, Lorena

From: Dresselhaus, Timothy R. <Timothy.Dresselhaus@va.gov>
Sent: Thursday, January 9, 2020 9:44 AM
To: Russell, Denise
Subject: Liberty Charter High School

Dear Ms. Russell:

I wanted to indicate to you my strong support for Liberty Charter High School as a value to our community and a resource to its families.

16-A The school is outstanding in every regard – superb, caring, dedicated faculty; a wonderful and diverse student body; a track record of educational accomplishment. Our son has enjoyed the school immensely.

Supporting the school in its mission is of great value to the community – I heartily endorse their project proposal.

Regards,

Timothy Dresselhaus, MD MPH

Cordova, Lorena

From: Caroline Edkins <chiborn5@hotmail.com>
Sent: Thursday, January 9, 2020 11:43 AM
To: Russell, Denise
Subject: Liberty Charter High School Permanent El Cajon Location

Hi Denise,

I wanted to say a few words in regards to the new Liberty Charter High School Permanent El Cajon Location. Please finalize! Liberty Charter High School (and the other Literacy First schools) is an amazing school. My husband and I are thrilled that our daughters are attending the Literacy First Schools. Our girls are learning at a much higher level than what most public schools achieve and our daughters are growing into young responsible adults.

My older daughter has been at Liberty Charter High School for three years. Our younger daughter is presently at the Literacy First Junior Academy and been attending Literacy First for two years.

17-A

What a difference between the prior schools our girls attended and Literacy First. Liberty Charter High School (and the lower grades) is an amazing school system with top notch office administration and teachers. My husband and I are blown away of the high level of education our daughters are receiving. Focusing on Liberty Charter High School: The administration and teachers practice what they teach: While teaching, this team of educators encourage the students to be conscious thinkers, self-directed, responsible community leaders, effective communicators, and literate citizens.

We were so impressed by Liberty Charter High and Literacy First school model that we moved to the East County from North Park. We wanted our girls to be able to benefit from the school and the activities and not let distance discourage our involvement. The school's drop off/pick up system is the most efficient process we have ever experienced. Very streamlined!

I hope a positive decision will be made on behalf of Liberty Charter High School. My husband and I are so grateful that our girls are attending this amazing school system.

Sincerely,

Caroline Edkins-Mom of a Liberty Charter High School student and Literacy First Junior Academy student

Cordova, Lorena

From: Amy Elias <amyelias@hotmail.com>
Sent: Thursday, January 9, 2020 1:30 PM
To: Russell, Denise
Subject: Liberty Charter High School

18-A I am the parent of a school-age child at Liberty Charter High School and I am submitting this letter to express my interest in and my support for the construction of a new High School in our lovely city of El Cajon. My support exists because Liberty Charter High School will provide another secondary educational option for families in East County area, one which will focus on high standards of achievement, personal growth, problem-solving, social responsibility and global awareness.

Please consider my letter and pledge full support for our new High school, for our community, our children, their future and supporting our students to become career and college ready and be productive citizens in our wonderful East County Community.

Sincerely,

Amy Elias

Cordova, Lorena

From: Ezequiel Zeke Elias <ezequielelias@hotmail.com>
Sent: Thursday, January 9, 2020 1:00 PM
To: Russell, Denise
Cc: zekeelias@hotmail.com
Subject: Please support our new High School location for Liberty Charter School

19-A I am the parent of a school-age child at Liberty Charter High School and I am submitting this letter to express my interest in and my support for the construction of a new High School in our lovely city of El Cajon. My support exists because Liberty Charter High School will provide another secondary educational option for families in East County area, one which will focus on high standards of achievement, personal growth, problem-solving, social responsibility and global awareness.

Please consider my letter and pledge full support for our new High school, for our community, our children, their future and supporting our students to become career and college ready and be productive citizens in our wonderful East County Community.

Sincerely,

Ezequiel Elias

Thank you,

Zeke Elias.

"Our greatest fear in life should not be of failure, but succeeding in life at things that really don't matter." Francis Chan

Cordova, Lorena

From: Veronica Espinoza <veronika.espi@gmail.com>
Sent: Thursday, January 9, 2020 1:22 PM
To: Russell, Denise
Subject: Liberty charter high school

Hello Mr Russell,

20-A First and foremost I would like to thank you for taking the opportunity to read this email. I am a mother of four children. I would also like to let you know how much liberty charter high means to my family. I have a senior that is graduating this year in June from liberty charter high school, a freshman going into the school next year. I also have two little ones that go to the elementary schools. liberty Charter high school is an amazing part our family life. It would mean so much to my family and so many other families at liberty that we can have our very own school here in El Cajon let me tell you it is a challenge to drive from El Cajon to lemon Grove Monday through Friday driving my highschooler and taking 3 little ones back to El Cajon dropping them off at their schools Here in El Cajon. Our high school kids give so much back to our community Through community service and through other fun outgoing ways to our community. thank you so much for reading this email and we all will be very grateful to have our very own school here in El Cajon. charter school is a Choice and I love the fact that we may have that choice here in El Cajon. thanks again

Espinoza Prieto family
Sent from my iPhone

Cordova, Lorena

From: Jennifer Galvin <jgalvin01@gmail.com>
Sent: Thursday, January 9, 2020 12:24 PM
To: Russell, Denise
Subject: Liberty Charter High School Project

To Whom it May Concern,

My name is Jennifer Galvin and I have been a resident of El Cajon for most of my life. As a child, I attended Chase Avenue and Anza Elementary School as well as Emerald Junior High School. I have been a resident of Harbison Canyon for the past 16 years and have two teenage children who attend Literacy First Charter School and Liberty Charter High School. Like every other parent, I want my children to receive the best possible education in an environment where they are not only safe, but where they are known, respected and supported. I want them to be in a place where they have opportunities to grow and thrive, learning to leverage technology while also being given a traditional foundation which includes art, physical education, music classes, drama, sports and emphasizes patriotism and character. When I began looking for a school that met these requirements, I discovered LFCS, a school which at that time, had been in business for 10 years and was performing high above expectations not only on student test scores but also as a business who was receiving awards from the city of El Cajon for its fiscal responsibility. My husband and I enrolled our daughter for the lottery and were overjoyed when she was granted a space at the primary academy located on Mollison and Washington, just one street over from where I had attended elementary school. At LFCS, both my children have been cared for and given an education that allows them to achieve academic success. The staff, from teachers to playground staff, administration to office support and everyone in between, all demonstrated then, and continue to demonstrate, a passion for the students in their care. It would take pages and pages to describe every benefit our family has experienced from being part of this community, not the least of which is witnessing our children and their peers actively giving back to the community through participation in donations to families in need, veterans, and others. I am writing to you today, however, mainly to express my profound love and gratitude for Literacy First Charter School (LFCS) and Liberty Charter High School (LCHS) as options for east county families when other local schools weren't the best fit. As my daughter approached 9th grade last year, it became evident that the best choice for her and our family was to continue in the incredible educational model that she has been so successful under for the previous 9 years. Firm in our conviction, we decided that if we needed to drive her from Harbison Canyon to Lemon Grove to keep her with this community, we would do so. Although the other three campuses of LFCS are located within the city of El Cajon and primarily serve families from it and the surrounding areas, the value we place on the education our kids receive make the extra miles worth it. That being said, the Lemon Grove campus is not owned by LFCS and the lease could be up at any time. For the sake of stability for our families, staff and students, a more permanent location is necessary. LFCS is already a well-known and valued part of the city of El Cajon, so what better place for their permanent high school facility than a lot within the city's borders? While I realize that there are other high schools in the area, our family chose LFCS and LCHS specifically because they are doing things differently and, in my opinion, more effectively and efficiently, than the already overcrowded schools nearby. After viewing the plans, reports, and findings on the environmental impact of the school, as well as traffic and parking accommodations, it is apparent that all requirements have been addressed. Having another school in the area as an option for parents that is not only highly rated but efficiently operated, can only have a positive impact on the community. I urge you to adopt the proposed Liberty Charter High School plan for the good of local families, the community, and the future of students who are counting on LCHS to continue to provide an excellent educational experience.

Thank you for your time and consideration.

Sincerely,
Jennifer Galvin

Cordova, Lorena

From: Adela Gonzalez <adelag0121@gmail.com>
Sent: Thursday, January 9, 2020 7:38 PM
To: Russell, Denise
Subject: Liberty Charter High School

Hi!!

22-A I have noticed that the staff members care about each and every student and try and prepare them for their future and in their journey in becoming successful adults. I believe that Liberty Charter High school has great values and enforces the morals they have, like integrity and the importance of responsibilities.

We love LCHS

Adela Gonzalez

Cordova, Lorena

From: Bryan Heers <bryanheers@gmail.com>
Sent: Thursday, January 9, 2020 11:54 AM
To: Russell, Denise
Cc: Bock-Heers, Liberty
Subject: LCHS New location

Hi Denise,

23-A I am writing in support of the new location for the LCHS in El Cajon. Our son current attends LCHS in Lemon Grove, our home is located in El Cajon, and our younger son Marcos will be attending the sister middle school next year in El Cajon. Besides the advantage of the new location in proximity to our home, the site in general is in a much safer neighborhood than the current location. Also, having the High School in El Cajon will serve to keep more students moving from the middle school up to the high school. Its a challenge when your child moves to a new school in a different area where there are no children they know from the previous school. Having the high school in closer proximity to the elementary and middle schools creates a family environment where they can build relationships that last a lifetime.

Much Thanks,

Bryan Heers

Cordova, Lorena

From: Nadia Hermiz <nhermiz@cox.net>
Sent: Thursday, January 9, 2020 11:52 AM
To: Russell, Denise
Cc: Nadia Hermiz
Subject: LCHS

Hello,

I am a parent of a senior student at liberty charter high school. We were blessed to find a perfect school setting for our youngest child. Without the school I know my son would've been lost or maybe even a high school dropout in a large school setting. Small class sizes allowed my son to excel and now has a great opportunity to not only attend a two-year college but a very good chance to attend a four-year university. The school transform my son into a leader , self-confident and excel academically. Although I will no longer have any children that will be attending high school; I want to see the same opportunities for other children. This community needs a school for less advantage children.

24-A

I know there's a lot of community concerned about traffic and school competition. There is no school competition. Having two older children that graduated from a nearby large high school; These are completely two different school settings and I am sure you are aware of it.

Second is traffic, The school does an awesome job in traffic control especially with such few students. Furthermore the land most likely will be developed. Owning a property in the district I feel I would be more confident to have a school there than an additional hundreds of homes.

Ultimately we have to think of our future and our children ARE our future. Large school settings are not meant for every single child and every single child Deserves an opportunity. Please take this into consideration.

Best regards,
Nadia hermiz

Sent from my iPhone

Cordova, Lorena

From: Bill Hilke <bhilke@cox.net>
Sent: Thursday, January 9, 2020 1:25 PM
To: Russell, Denise
Subject: Liberty high school

25-A My son has been going to liberty charter since kindergarten. He is now in 10th grade. We have been so pleased by the education he has received But not only educationally but the dedication and caring the teachers have exhibited. There is definitely a need to get a permanent site for the high school. Thanks Bill Hilke.

Sent from my iPhone

Cordova, Lorena

From: Randolph Houts <rhouts@gmail.com>
Sent: Thursday, January 9, 2020 12:24 PM
To: Russell, Denise
Subject: Literacy First Charter School and Liberty Charter High School

Ms. Russell,

I'm writing to urge approval of the site for the LCHS campus near the intersection of Chase and Jamacha Road in El Cajon. My children have been fortunate enough to receive the superior educational, civic, and social experience of the LFCS elementary and middle schools. My oldest daughter diverted to what I now refer to as the "West Hills High experiment" for 2 years, partly for sports, but is now at Grossmont Middle College High School, a great opportunity for a dedicated, well-grounded learner like her. My younger daughter is currently a student at Liberty/Literacy school.

26-A

We chose the Charter program for reasons I'm sure folks have heard before, but also because the Liberty/Literacy schools were located conveniently and my wife and I could position my office, our home, and the school in close proximity and limit driving to quick trips on local roads. This new site will change the round trip to drop-off and to get home or to work from 1 hour and 15 minutes of driving to 10 minutes with no freeway driving. That's safer, less expensive, and less burdensome for the environment. It also boosts productivity, because less time on the road means more time at the office for those of us who have eschewed the paycheck and benefit world and are self-employed. Forget mystery bike lanes to nowhere for nobody; this will make a difference.

I have reviewed the elevations of the proposed school campus and it seems entirely in keeping with the area and will not eliminate any sort of green area that's not already environmentally impacted or important to the character of the selected intersection and its environs. It will, in point of fact, enhance the area and provide additional commercial opportunities for local businesses. It will bring traffic to the area, but the intersection is one of the larger ones in the area, has proper signals, and the traffic will be limited to morning and afternoon pick-up hours. The intersection is not one that is particularly involved in the morning work commute, so I do not see it as a significant issue. Also, Literacy/Liberty schools have always devoted a great deal of effort and concern into their traffic control procedures and they've taken that into account in planning the campus and parking area.

26-B

Notably, I haven't seen a great deal of concern about congestion in the government's approval of the large residential development at Greenfield off I-8, a much smaller, more congested intersection that will cause major issues on an already overburdened, narrowing section of I-8, or for the many safety and traffic issues the Shadow Mountain Church causes residents of Granite Hills, so I would not expect such concerns to be determinative here, either.

26-C

Approval of the project will also increase property and sales tax revenue, something that makes the government feel all warm and fuzzy about development normally. The new campus will permit growth in the school's size; and that promotes employment and probably world peace in some way as well.

26-D

Seriously, there is something different and better about the Liberty/Literacy First schools in my view, although coming from a family that's always supported public over private education at the pre-baccalaureate stage, it pains me to say so. Liberty/Literacy schools are more well-disciplined. They teach history, civics, and encourage a positive view of our country, focusing on the fundamentals understood to be the bedrock of education for generations, at least until recently in California. The faculty and staff are more dedicated and attentive. It's not a religious school, but the curriculum and staff are less hostile to my family's fundamental beliefs. Political agendas, bloated yet ineffective administration, and misguided law-making are hurting the non-charter "regular" public schools; I truly mourn it. But Debbie Beyer and her staff have created a unique educational plan that satisfies requirements, teaches my kids better, and is less offensive to our values. Importantly, though, the curriculum is also designed to be an integrated program that students will

26-E

benefit the most from if they can have continuity through the high school years. It's also best for most children if they can progress with their social group through high school. This isn't strip-mall no. 2,357; it's a good thing for the community!

We have been unable to return my oldest daughter to the Charter system because of CIF eligibility rules applicable to Varsity athletes, but fortunately, the GMCHS program challenges her academically and preserves her eligibility. For my younger girl, I'd like to keep her on track and having the campus at this new location, with all of its facilities, would really be a wonderful thing. It's a superior facility in a better location and a much better neighborhood. It's hard to imagine what the cogent argument would be against approving it and it will keep my big truck off the road in the a.m.; that's something we can all get behind, right?

There are plenty of political forces arrayed against Charter schools, notably among them the school districts and their patent self-interest and rank protectionism. I think the District is misguided and should embrace the Charters as agents of badly needed reform, but that's a discussion for another letter. This proposed campus is a commercial property development proposal and the county's decision cannot legally be made based on political loyalties, influence, who buys who a Latte, or some special interest's desire to prevent the growth of Charter schools. Every material and legitimate factor argues for the approval of this project and we'll all be watching to see how the people who serve as the servants of the public proceed here. I don't see why it's taken 5 years, but it's time to approve this.

Thanks for taking the time to consider my comments.

Best Regards,

Randolph C. Houts, Esq.

THE LAW OFFICES OF RANDOLPH C. HOUTS

Website: <http://www.rhoutslaw.com/>

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Cordova, Lorena

From: Valda Houts <rhouts.asst@gmail.com>
Sent: Thursday, January 9, 2020 12:29 PM
To: Russell, Denise
Subject: Liberty Charter High School

I am writing to you to urge the county's approval of the proposed Liberty Charter High School site in El Cajon at the intersection of Chase and Jamacha roads. I am a resident of El Cajon (granite hills area) and currently drive an hour twice a day to take my high school student to school. She attended the affiliated charter elementary and middle schools which are both located in El Cajon. Like many of her friends parents, we are spending a lot of time on the road and paying for lots of extra gas so she can maintain the friendships she's developed and continue to receive a superior high school education.

27-A I have been extremely impressed with the careful planning and attention to detail Liberty/Literacy First staff have put forth in mitigating traffic and parking issues surrounding their other campuses. Additionally, I am very familiar with the area surrounding the proposed campus as it is 10 minutes from my house. The plans which have been carefully drawn and reviewed over several years will beautify the area and provide another high school option for the growing population in El Cajon (I pass 4 new housing developments several miles from the site every day). Much effort has been put into the design to produce an attractive, functional space.

Finally, I know the school has the funds in place to execute this project in an efficient and timely manner once approval of the project is received. It will address many of the constraints the high school teachers and staff face at the current location and provide future students with an excellent education for many years to come.

Please expedite approval of the project which has been carefully considered for nearly 5 years. All of us in the Liberty/Literacy school community are behind the new high school location.

Sincerely,

--

Valda Houts
Liberty High School Mom and Taxi Service

Cordova, Lorena

From: Dana Ivey <dana.ivey@cox.net>
Sent: Thursday, January 9, 2020 5:21 PM
To: Russell, Denise
Cc: Steve Robinson; Tamara Contasti
Subject: Liberty Charter High School

Denise:

28-A We have been fortunate enough to be a part of the Literacy First Family from almost the very beginning. Our oldest son graduated from the program a year prior to high school being an option. Our youngest son started kindergarten by year later and is now a junior at Liberty Charter High School. As a result have only known quality education. Our eldest son went on to West Hills High School and took all honors or AP courses. He has since graduated from SDSU. Luke currently takes one AP course. He loves his school, is known individually by teachers and the administration. With all of their help he has been given the chance to succeed. We are thankful for Liberty Charter High School. We know it will serve many families if built in El Cajon.

Sincerely,

Bill & Dana Ivey

Sent from my iPhone

Cordova, Lorena

From: Nopanoot Kempsey <nkempsey@yahoo.com>
Sent: Thursday, January 9, 2020 10:38 AM
To: Russell, Denise
Subject: Liberty High School

Dear Ms. Russell,

My name is Nopanoot Kempsey, a parent of two. My both of children are attending at Liberty Charter School, my daughter is in 3rd grade and my son is in high school. I love this school. They offer a strong academic program. All teachers and administrators are resourceful and kind. They do their best work for the children without any discrimination.

29-A

The administrators have been working hard for many years planning to move the high school from Lemon Groove to El Cajon. In my opinion, it is essential to have all school campuses in the same local area so which helps decrease traffic at the I-8, I-125, and I-94, also means shorter commute for the children. I am one of many families support the new high school location. Please consider this plan for the children.

Thank you.

Warm regards,

Nopanoot Kempsey

Cordova, Lorena

From: Kempsey, Victor <victor.kempsey@lfcinc.org>
Sent: Thursday, January 9, 2020 11:01 AM
To: Russell, Denise
Subject: Liberty Charter High School PDS2015-MUP-15-027; PDS2015-ER-15-14-010

Ms. Russell,

30-A I am writing you this email as a parent of 2 students at Literacy First Charter Schools. I am an administrator at one of the campuses, but this email is as a parent. It is an ordeal to get my 9th grade son to the Lemon Grove campus. I live in Lakeside and having the campus at the Chase Avenue location would allow me to get him there quickly and hassle free using the city streets. I want both of my children to attend LFCS throughout their education because of the high quality of education they receive. This is true from kindergarten until 12th grade. Allowing them to stay with this school will benefit them greatly. Maybe you can come by one of our campuses and see for yourself what is being done here? I know there are concerns about traffic, but from my understanding the street around the new high school will be much better than they currently are. There may be about 10 minutes in the morning and afternoon when traffic is a little congested, but the would be off heavy traffic time when there probably isn't a lot of traffic in the location anyway and even if there is, it won't be that bad. In fact, for the rest of the day those streets will be better than ever.

Thank you for your time in reading this email.

--
Victor Kempsey
Assistant Principal
Junior Academy, LFCS

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Cordova, Lorena

From: Sergio kid? (1/2) <gutierre.anabel@hotmail.com>
Sent: Thursday, January 9, 2020 1:37 PM
To: Russell, Denise

To whom this may concern,

31-A I'm a parent of a special need student who has an IEP and I'm very pleased to have my new freshman at LCHS . I'm a new parent in a charter environment setting and very happy with the results of this charter experience, Hoping that you or the people who are involved in taking any decision making for the new LCHS to give the students and staff and parents to have a new school to treat the issues of the poor parking problem we have among other concern issues. I'm happy to give my opinion as a parent concern for my high schooler safety and wellbeing. Thanks to LCHS for making our kids your priority.

Sent via the Samsung Galaxy Note10+, an AT&T 5G Evolution capable smartphone
Get [Outlook for Android](#)

Cordova, Lorena

From: Chad Lansford <chad@leftend.com>
Sent: Thursday, January 9, 2020 5:10 PM
To: Russell, Denise
Subject: LFCS High School

Ms Russell,

I'm writing to you to voice my support for the Literacy First Charter School High School in El Cajon. Both of my daughters have been students with LFCS since Kindergarten, and I couldn't have asked for a better school to have them enrolled in. My oldest daughter is currently a freshman at the current LFCS high school in Lemon Grove and is really enjoying her time there. It was important to her, and to my wife and I, that she continue her high school education with LFCS because of the values they instill in our daughters, as well as the trust they have earned with our family.

32-A

I understand the county has concerns with the plans for the new high school, but after reading and reviewing all of the plans available for the new school - it's clear to me that the administration has taken these concerns to heart and made sure to address each and every one of them. If I lived in the area immediately surrounding the proposed location - I'd be hard pressed to come up with a more desirable use for that particular piece of land. Especially for a school that has performed as well as LFCS. In our previous house near Helix high school, I have fond memories of occasionally hearing the cheering during a football game - and it gave the neighborhood a better feel to see students on their way to and from school.

I encourage the county to consider approving use of the proposed site for the LFCS high school. We now live in the Mt Helix area of La Mesa, and we choose to drive further than many other schools to stay with LFCS. This new location is exciting to us, and I know it will benefit not only our family, but countless local families and the community for years and years to come.

Sincerely,
Chad Lansford
619-886-1088

Cordova, Lorena

From: Deirdre Lickona <dlickona@gmail.com>
Sent: Thursday, January 9, 2020 10:06 AM
To: Russell, Denise
Subject: LFCHS

Dear Ms.Russell,

I am the parent who's child attended Literacy First Charter High School.

The sports program for my son was very important in his character development and his physical health. It was a struggle to do this without a permanent gym location but due to the dedication of the staff and teachers at the school we managed to make it work .

I would like to advocate for a permanent location for the school as I hope to send my other children there in the future .

A permanent location enables the school to offer more stability and programs to the kids.

Sincerely,

Deirdre Lickona

33-A

Cordova, Lorena

From: Guadalupe Martinez <guadalupesanchez@yahoo.com>
Sent: Thursday, January 9, 2020 2:52 PM
To: Russell, Denise
Subject: New school campus for Liberty Charter High School

Greetings Ms. Russell,

I'm the parent of a Liberty Charter High School's student. I just want to share my thoughts about their new school campus project.

34-A

When I was looking for a high school for my daughter, I heard so many good comments about this school that I decided to enroll her there. This is her second year there and I'm still happy with the school in general. I'm sure their quality service, educational and student behavior standards will remain as high as they are now, or even higher.

Thank you for your time.

Sincerely,

Guadalupe Martinez
Parent of a 10th grade student at Liberty Charter High School

Cordova, Lorena

From: Deb Elizabeth <debbiemedicus@gmail.com>
Sent: Thursday, January 9, 2020 11:14 AM
To: Russell, Denise
Cc: Steve Robinson; cmmedicus@gmail.com
Subject: Support for Liberty Charter High School in El Cajon

Dear Ms. Russell,

On behalf of my family and others in our situation, please consider pledging your support in favor of Liberty Charter High School relocating to El Cajon.

I have two boys, ages 13 and 17. They have been attending Literacy First Charter Schools since they were in Kindergarten.

When my oldest was in Kindergarten, I worked in our local school district as a teacher. I was displeased with what I saw and knew that our local school wouldn't be a good fit for him. He has Autism and ADHD and needed to be known, protected from a culture of bullying, and pushed to his potential. Years later, he is a Sophomore at Liberty Charter High School. He has participated in Cross Country two years in a row. Now he is earning As and Bs with accommodations and very little homework support from me at home! His teachers understand his needs and value his quirky humor. They give grace when needed to help him overcome his organizational and social challenges. I am so grateful for the role that so many teachers have had in helping a struggling reader, a little boy that hated school and felt like a failure, desire to become a psychologist and writer one day!

35-A

My youngest is completely the opposite in skills and personality. Learning comes easily and he has been given opportunities to excel and develop leadership skills that will serve him. Next year will be his first year of high school and I am so grateful that he will continue to have these opportunities at Liberty Charter High School. With lots of collaboration with staff, he went from a sneaky little 1st grader to a strong mathematician, writer, reader, friend, and leader that I am very proud of. He has a bright future ahead of him as well.

Parents have limited choices in El Cajon and across the county for high school. If you reside in this area, you can choose a huge school, where students are often unknown and negative peer pressure is high or you can home school. Had my oldest not gotten in the LFCS lottery years ago, we would have gone the home school route, even though we needed my income as a teacher. At Liberty Charter High School, students are known and positive peer pressure is more the norm than at any other high school I have experienced. Leadership skills, athletics, and academics are rigorously encouraged as well. The smaller setting allows for more accountability and personal growth for students. The desire to have seniors leave as better people than they were as Freshman is not only a statement, it is lived out in the actions of administrators and staff members.

Thank you for reading the story of my boys. It's a story that would be very different if Literacy First had not been an option for us in El Cajon all those years ago.

Please strongly consider pledging your support to Liberty Charter High School in El Cajon for families like ours.

Sincerely,

Debbie Medicus
Parent and Educator

Cordova, Lorena

From: Paige O'Neal <paige@onealshome.com>
Sent: Thursday, January 9, 2020 12:54 PM
To: Russell, Denise
Subject: Liberty Charter High School

To Whom It May Concern,

I am writing to ask for your approval for the new location of Liberty Charter High School. My oldest daughter has attended LFCS since 1st grade, and is currently a freshman at the LCHS location in Lemon Grove. It has been a wonderful experience for her. She will tell all who ask that she loves going to her school and as a parent I highly recommend Liberty as well.

I understand that some people have concerns about the new location. As an El Cajon resident with another daughter at the Junior Academy in El Cajon, I spend 1 HOUR getting them both to school and another hour picking them up. If people are worried about environmental concerns, let them know that having the high school in El Cajon, where the rest of this charter system is located and most families are located, will greatly reduce the emissions that environmentalists are so concerned about.

For those concerned about traffic, LCHS is much smaller than Valhalla and Steele Canyon, and teachers and staff work very hard to keep the drop-offs and pick-ups as smooth as possible. A 15 minute period in the morning and afternoon where traffic might be heavier on Chase will be nothing like the Steele Canyon traffic. If anything, the traffic at the current high school location is much better than the K-8 locations, given that parents can drop high school students off earlier and pick them up from sports, etc later, so that the arrival and departure of students will be more dispersed time-wise. Furthermore, more high school students will be able to ride bikes to the new location.

I hope this encourages you to approve Liberty Charter High School's new location!

Sincerely,

Paige O'Neal

Cordova, Lorena

From: 2 amazing sisters Rodriguez <rodriguezmarthab7@gmail.com>
Sent: Thursday, January 9, 2020 10:34 PM
To: Russell, Denise

37-A In our family we are mom and dad and six children. Last year our first daughter graduated and this year 2020 our second son is about to graduate. We live in the city of El Cajon and the experience of having our children in LFCS has been highly positive for the children. The teachers and school staff are very good and professional. I can only say positive things about everything. Having a high school in our city would be a very good thing for our family, we still have 4 more girls and not having to move and drive to another city would be a great advantage for our children to make faternity ties with the same year classmates for years is something extremely positive for everyone. Healthy children, emotionally and mentally healthy people, equals a healthy community.

Cordova, Lorena

From: Tricia Rowland <tmrowlandjr88@gmail.com>
Sent: Thursday, January 9, 2020 11:17 PM
To: Russell, Denise
Subject: Liberty Charter

My children have attended LFCS since 2003. The locations in El Cajon have greatly helped me in getting my children to school

On time

And in a timely manner. I cannot stress the greatness of having the high school

In El Cajon, as well. Thank you for taking the time to read this email. The school and the teachers and staff have been a blessing to my family.

Thank you,

Tricia Rowland

Cordova, Lorena

From: Jenny Spigelmire <jenny@cropping.info>
Sent: Thursday, January 9, 2020 9:50 PM
To: Russell, Denise
Subject: LFCS/ LCHS

Hello,

39-A We have been party of the LFCS family for 11 years now. The teachers and staff are amazing and they genuinely care about our kids. They work with us parents to help them thrive. Not only to they educate these kids, but they build character qualities every parent wants for their children. My youngest is an 8th grader this year. It would be much to our benefit as well as many others to have the new high school located in El Cajon. As it is, I spend 2 hours a day driving to drop-off and pick-up up kids from school (1 in Santee at the Junior academy, the other at the high school in lemon Grove). But I do it because it's worth it to me and my kids. What a blessing this school and staff have been to us!!! I think they would be a great addition to El Cajon.

Thank you,
Jenny Spigelmire

Cordova, Lorena

From: Chrystal Stump <chrystalstump@gmail.com>
Sent: Thursday, January 9, 2020 10:16 AM
To: Russell, Denise
Subject: LFCS High School Support

Good morning!

Thank you for taking a moment to read my support on the new campus proposal.

My oldest son is a current freshman at LCHS and has attended Literacy First Charter School since the Liberty Campus opened. I have two sons behind him also at LFCS campuses.

I understand the traffic concerns with the school, which is why I encourage you to observe the traffic patterns at one or all campuses. I can proudly say being a school of choice, parents also make the choice to respect the flow and observe traffic plans. We value our campuses and know how important being a good neighbor is.

LFCS's have been an outstanding member of the El Cajon community and its continued growth will leave positive lasting impressions as the track record shows.

Good day!
Chrystal Stump

El Cajon resident
Mom of 3 boys at LFCS schools

40-A

Cordova, Lorena

From: Floriana Vogan <itsflor@hotmail.com>
Sent: Thursday, January 9, 2020 5:00 PM
To: Russell, Denise
Subject: Liberty Charter High School

Dear Ms. Russell:

41-A I am writing to you today to support the plan for the new Liberty Charter High School in El Cajon. I have two kids. My son is a Senior at Liberty Charter High School, and my daughter is a 7th grader at the Junior Academy. My son started in the public school system in Rancho San Diego, and although he had some great teachers, I started to see a change in the system at about 3rd grade. From hearing experiences of our neighbors who had kids in middle and high school, as well as stories from my brother and sister in law who work at a local high school, I knew I had to move my kids out of the public system. My daughter was lucky enough to get a spot at Literacy First in kindergarten, and after one year I decided to move my son there as well. Both kids have thrived at this school. The values and education are exceptional. When Kenny was selecting a high school I knew it would be a challenge to get him to Lemon Grove and my daughter to the Junior Academy near the Bradley airport, but this school is worth it.

We have been waiting for this new high school for several years. For parents, like me who choose to have their kids go to Literacy First for the quality of education, we will make the sacrifices to go to Lemon Grove, but it would be great to have a high school in the same community as our other three schools.

Please consider supporting the new location for the Liberty Charter High School.

Respectfully,

Floriana Vogan

Cordova, Lorena

From: L Zenteno <lolanette12@gmail.com>
Sent: Thursday, January 9, 2020 10:08 AM
To: Russell, Denise
Subject: Liberty Charter High School

Good Morning,

I am a parent of a freshman student at Liberty Charter High School. This is our first experience with Liberty Charter and it's only been a few months now but I am beyond delighted that my son was accepted at this Charter school.

He attended private school for all his school years and I did not want to send him to a public school with a very large student population, since he was not used to that environment.

42-A I love that Liberty Charter requires uniforms, that they are a small school compared to larger public schools, and that the staff is always present, available and knows my son by name, all of which my son is accustomed to.

They further impressed me with the counseling they offer, the opportunities for college and their sports programs.

I recommend Liberty Charter to anyone I know who has kids and it would be such a wonderful service to kids and to the community of El Cajon to have another high school.

Thank you for your time,

-Laura Zenteno

Cordova, Lorena

From: Haitham Afram <haithamafram@yahoo.com>
Sent: Friday, January 10, 2020 9:11 AM
To: Russell, Denise
Subject: New building for LFCS

Hi

I have two students in LFHC, my son and my daughter, I think it's time now for LFHC to have a new building in El Cajon, because there are a large number of students live in El Cajon in addition this school is so distinguished and the staff are very good, I support the effort for this new building and it will be important culture edifice, now it is very important to begin to establish this new school.

Thanks for listening

Haitham Afram

43-A

Cordova, Lorena

From: Allen, Nicole <nicole.allen@lfcinc.org>
Sent: Friday, January 10, 2020 9:05 PM
To: Russell, Denise
Subject: Liberty Charter High School - Changing lives for the better

Hello Ms Russell,

My three sons have been given an excellent education from Liberty Charter. We really would love for our Charter to be able to have a home by the students it is serving so well.

My sons have gone through their program in El Cajon though 8th grade, but we now have to travel to Lemon Grove. Make no mistake, it is worth it, but it would be so much better to have it closer to the other schools and our home.

My oldest is now a Junior and just scored a 1350 in his PSAT. The college board said that he is in the top 98th percentile. He would not have gotten this far without the quality teachers and education from Liberty High School.

Thank you for your time and consideration,

-Nicole Allen

--
Nicole Allen LCHS RtI Coordinator, English, Academy and Internship Teacher

Cordova, Lorena

From: Carolina Ballesteros <arjo2424@yahoo.com>
Sent: Friday, January 10, 2020 4:00 PM
To: Russell, Denise
Subject: Urgent: High School Location

Good afternoon,

45-A I am writing this email because I want to advocate for Literacy First Charter School. I am a parent of 2 wonderful and brilliant kids. My daughter attended LFCS from K-8th grade, and I currently have a son who attends the Junior Academy and he is in 6th grade. I strongly believe that it is important to have an LFCS high school in El Cajon, due to the fact that it will change the lives of many families and students, just how LFCS impacted our family. LFCS is not just a regular school, it is a FAMILY. The teachers truly care for their students and that is something that has been lost throughout the years. I think many can agree that high school is difficult for the parent and student, because it is a stage in life in which the student question and find themselves. If we are able to keep an LFCS high school, we can provide a safe, caring, and welcoming environment for this new generation, including my son. The LFCS high school will be the perfect environment to guide a diversity of young adults into the real world. Thank you.

Best Regards,
Carolina Ballesteros

Cordova, Lorena

From: Blaker, Caralyn <caralyn.blaker@lfcinc.org>
Sent: Friday, January 10, 2020 3:42 PM
To: Russell, Denise
Subject: LFCS proposed site

Good day, Mr. Russell,

I am FOR having the LFCS proposed site approved. Literacy First Charter Schools has had a positive impact on my life. I have 2 kids, one with autism, that attend LFCS. They are well educated and have high moral standards.

This school cares about kids, education, and the community. I want to have our Liberty Charter High School in my community. I realize that this will add to the traffic. But the benefits far out way the negative impacts. The student that Literacy First Charter Schools are helping to raise are kids of character. They are kids that will be our neighbors and our business leaders in the near future. They are kids that participate in sports after school so they will not all be getting out of school at the same time. These are kids who are well educated with high ambitions and strong moral character. This kids will be well educated voters in the future, and they will all vote because they care what happens to our community. We actually need MORE schools like the Literacy First Charter School in our neighborhoods, because they raise kids who are well educated and have high moral values.

Please let the Liberty Charter High School property be approved.

Caralyn Blaker
1268 Shari Way
El Cajon, CA 92019

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Cordova, Lorena

From: Jen Burr <jmburr7@gmail.com>
Sent: Friday, January 10, 2020 4:06 PM
To: Russell, Denise
Subject: Literacy First Charter School/Liberty Charter High School

Good afternoon!

My name is Jennifer Burr, and I have two sons who attend Literacy First Charter School. One is in 3rd grade, and one is in 7th grade. I am writing to let you know how pleased I have been with my children's experience at Literacy First, and how hopeful I am that we are able to build the High School in El Cajon.

Both of my sons have attended Literacy First since Kindergarten, and I am continually amazed at how the teachers in every grade make learning creative, fun, and in depth. Students are equally taught excellent academics (our school has top scores on state testing), as well as the importance of good character. Every day they learn math, language arts, and the other subjects, while also learning the importance of integrity, honesty, and making wise choices. I could say so much more about how wonderful our experience is, and how this school is helping our sons to grow into well rounded young men!

47-A

My oldest son, being in 7th grade, is starting to think about where he wants to go to High School. He would really like to go to Liberty Charter High School. He likes the idea of continuing in a smaller school environment, where the expectations and character development are a continuation of what he has experienced and enjoyed to this point. He also likes the classes and extra curricular options that are offered. We live in El Cajon, and having the school built here would save quite a bit of drive time and fuel cost for our family. Also, since our younger son will still be at one of the other campuses, it would be much easier to get from one school to the other at drop-off and pick-up times if the High School was in El Cajon. The new expanded facilities (the current location does not have a track or football field, along with some other facility options that the new High School would have) would also allow for greater involvement and an expanded learning environment.

Thank you for your consideration! I appreciate you taking the time to read my input.

Have a wonderful afternoon!

Jennifer Burr

Cordova, Lorena

From: Adrian Caminos <abcaminos@gmail.com>
Sent: Friday, January 10, 2020 7:21 AM
To: Russell, Denise
Subject: LFCS High School

Hello Ms. Russell,

I am wanted to send a note to you regarding Literacy First High School.

Both of our children have attended LFCS. It has been a great experience and a blessing in particular for our son as he has gone thru the school from K to now 12th grade. The environment at Literacy is exactly what he needed and he is thriving in part due to the school personnel and their style of education!

Please consider these things and allow the permanent high school to be built so that more families can benefit from this school!

Sincerely
Adrian Caminos

--

Thanks,
Adrian

"He who wants to finds a way.
He who doesn't finds an excuse."

Cordova, Lorena

From: Cox <meddery@cox.net>
Sent: Friday, January 10, 2020 5:04 PM
To: Russell, Denise
Cc: Debbie Beyer
Subject: LFCS Highschool

Hello,

49-A My family has been a part of the Literacy First Charter School family for 13 years. My children have had a top notch education in academics, arts, sports, and character. They have done this in an environment in which they are known and cared for by wonderful staff. As a family we are so thankful for the opportunity to send our children there. I have been a teacher for 27 years, and I am truly happy to see my children receive an education that encompasses the development of the whole child. Their curriculum allows the staff to utilize the children's strengths to help them grow and succeed. One of our boys will be going to the high school the year after next and another eventually as he starts kindergarten in the fall of 2020. We are excited at the thought of our children being able to attend in El Cajon at the new high school. We are thrilled at the thoughts of what this new school site and buildings will enable the staff to offer our children and the children of so many other families at LFCS.

Thank you for your time and thoughts on this new school site, Melissa Eddery

Sent from my iPhone

Cordova, Lorena

From: Marianne Forman <forman.marianne@hotmail.com>
Sent: Friday, January 10, 2020 10:36 AM
To: Russell, Denise
Subject: Proposed LCHS location

Hello Ms. Russell,

50-A We have been a Literacy First Charter School Family for eight years now, and our oldest will be soon heading to high school, with three more to follow after her. Our kids have excelled and thrived at Literacy First and we would greatly appreciate your support for the new proposed high school location. As you know, all of the traffic reports and environmental impacts have been carefully researched and presented in the proposal. Thank you for considering that school choice is truly best for California students and their families, and I believe our school's performance is proof of that.

Sincerely,
Marianne Forman

Sent from my iPhone

Cordova, Lorena

From: Jenny Gillespie <jennigi@cox.net>
Sent: Friday, January 10, 2020 5:48 PM
To: Russell, Denise
Subject: Liberty Charter High School

Good Afternoon,

51-A I am writing as a parent of 2 students of Liberty Charter High School. My girls (Freshman and a Junior) has been with the Literacy First Charter School system for their entire school career, 11 years. I am so thankful for LFCS and LCHS as they have helped my daughters develop not only academically but in character as well. In fact, I was a parent that fell so much in love with the school, I went to school to be a teacher at this school. I feel so passionate that all the students should have access to continue their education with the other students they have grown up with. Having a high school in the proposed location allows these students to continue onto High School at a greater rate since it is in close proximity to the other campuses. Our administration is very mindful of your concerns about traffic and noise, however our school is a small school and the impact will be minimal. Please allow us to have a school home of our own and vote in favor of our new location.

Thank you,
Jennifer Gillespie

Cordova, Lorena

From: Dina Godby <dinagodby@gmail.com>
Sent: Friday, January 10, 2020 3:07 PM
To: Russell, Denise
Subject: Liberty Charter High School

Hello Denise-

We are writing today to express our love of Literacy First Charter School/Liberty Charter High School. This our 14th year at Literacy. We are the parents of 2 girls. that at the time were just looking for an alternative to standard public school. What we found was far beyond. Our eldest daughter started at Literacy in kindergarten. She graduated last year from Liberty Charter and is now attending GCU on an academic scholarship. Literacy is trying to do something different and is succeeding. It is place our children go and feel comfortable and safe, where education is not the only goal. It is a place where they are known and learn to be effective members of society.

52-A

Before your latest meeting on the subject of Liberty being built, I read a posting on Facebook. The writer stated "we don't need another high school in this area. If they want a charter they can go to Steele Canyon or Helix." That is true they could, but they wouldn't get the same experience. Liberty is not the same type of school. How many schools do you know of that at graduation the vice principal can stand at the bottom of the stage and call each student that crossed the stage by their full name, without any note cards or help?

Traffic and parking may be a concern of yours. It is a concern of the Literacy staff also. On the existing campuses the staff continue everyday to be on site making sure that all traffic rules and agreed upon traffic flow are followed. Not only are we teaching are students to be good citizens, we as parents are trying to good citizens also. We remain aware and courteous to the residents around our campuses to ensure the least amount of disruption to their lives.

When making your decision on this matter, please consider the students that are lifelong learners at Literacy. Please allow them the opportunity to finish their education strong. Having this campus will allow them the opportunity to have most things that most high school have. A gym they don't have to drive to. A field they can run on. This campus allows for most activities to stay on campus. It is such an exciting time for the families at Literacy and hope we can break ground soon.

Thank you for your time,
Dina & Tom Godby

Cordova, Lorena

From: Stacey Harrell <staceyharrell36@gmail.com>
Sent: Friday, January 10, 2020 8:14 AM
To: Russell, Denise
Subject: Literacy First Charter Schools High School

Being aware of Literacy First Charter Schools from its beginning due to the placement of my college students there for fieldwork hours, I have seen the impact the school has had on students and their families. Being able to offer a complete program grades K-12 would allow students to continue their educational path through high school by attending a Literacy First High School in El Cajon. Having a local high school for LFCS students is vital for this process because the majority of the students come from the East County Area, specifically El Cajon.

I know there is a concern with the traffic impact the high school would have. The traffic plan I have read addresses all of these concerns regarding traffic paths, lights, parking, environmental impact and architecture that the County Planners have had. During the 5 years the school has been working on this project I know many impact studies have been done and addressed. Adequate parking is part of that plan. It is my understanding that most of the high school students do not drive, or have access to a vehicle for school transportation therefore parking needs are limited. Also, I understand that they will have a staggered schedule, so not all of the students will be coming or leaving at the same time.

The high school is a closed campus, and has been since the beginning of the high school. The students are not allowed to leave campus during the school hours. The school is built to include community service, has a prescribed dress policy, have parents that have chosen to put their kids there which has created a different type of environment, different from the typical high school, which is all part of the reason I have placed so many college students there.

Thank you for taking the time to read my concerns.

Sincerely,
Dr. Stacey Harrell

Cordova, Lorena

From: MANAR LAYTH <manar_1728@yahoo.com>
Sent: Friday, January 10, 2020 12:13 PM
To: Russell, Denise
Subject: Lfcs school

54-A I have two kids in this school system. When my oldest was approved to enter middle school of LFCS, I saw my son being very polite and kind because of the staff and teachers that work in LFCS. My son became more social and happy when he entered LFCS. His grades are very high now. I hope and pray for our high school to get built. I feel more happy to put my kids in a uniform school because I don't have money to buy other clothes and it is very good thing for their ages. I'm praying for the high school to get built because it's gonna be close to my house. Please can you approve our school?

Thank you so much.

[Sent from Yahoo Mail on Android](#)

Cordova, Lorena

From: Maclean, Leta <Leta.Maclean@aecom.com>
Sent: Friday, January 10, 2020 10:19 AM
To: Russell, Denise
Subject: LFCS high school in El Cajon

Denise,

Both of my children came up through the Literacy First Charter School system in El Cajon. My son is currently a junior at the high school, which is in Lemon Grove. Although it is convenient for us (we live in Casa do Oro), the location was not convenient for a number of his friends, and as a result they ended up going to other high schools that were closer to where they lived (Santee, Lakeside, El Cajon, Alpine). Having a high school in the same vicinity as the other campuses would be extremely beneficial for the families who would like the continuity of the charter school system.

I've lived in East County since 1982 and am very familiar with the location that is proposed for the new high school. I don't think there will be impacts to traffic, as we're not seeing significant impacts to traffic at the current high school location.

55-A

I think you will find that the planning documents put together by LFCS should address all of your concerns about parking and traffic. The location will allow many students who live in the area to walk or bike to school.

I love the LFCS school system - the quality curriculum, the high caliber of teachers, the variety of field trips, sports program, clubs, before and after school care programs, and the study skills programs. My son is on the autism spectrum and was able to mainstream into the school starting with kindergarten. There is seamless communication with San Diego Regional Center – we have annual IEP meetings and regular follow-up appointments. I truly believe my son would have slipped through the cracks if we had put him in public school. I appreciate that we have the option for charter schools in our county, and I honestly believe that LFCS is the top-ranking school.

I appreciate your consideration for the new high school.

Regards,

Leta Maclean, CHMM
Project Manager/Senior Chemist
D 619.610.7753 M 619.206.9109
leta.maclean@aecom.com

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Please consider the environment before printing this e-mail.

Cordova, Lorena

From: Paul Miller <paulmiller78@hotmail.com>
Sent: Friday, January 10, 2020 10:19 AM
To: Russell, Denise
Subject: Liberty charter high school

To whom it may concern.

I am the parent of a Liberty high school sophomore and a 8th grader who will be attending next year.

Both of my children have attended this charter school since kindergarten. I have watched them both flourish in this school. We are convinced that the new high school facility will be an overwhelmingly positive thing for the community.

56-A

We know the students and staff will be great neighbors as I've seen for 10 years how they not only teach about character and citizenship but they demonstrate it from the top down.

We hope the long track record of outstanding results by Literacy Charter And Liberty High will be considered and this project be approved

Thanks

Paul Miller

Cordova, Lorena

From: Robinson, Steve <steve.robinson@lfcinc.org>
Sent: Friday, January 10, 2020 7:02 AM
To: Russell, Denise
Subject: Liberty Charter High School on Chase Ave

Ms. Russell,

I attended the Valley De Oro Planning Commission meeting this past Tuesday night regarding the construction of LCHS on Chase Ave. Residents of the area expressed concerns mostly about traffic, parking, and safety.

57-A

Many of their concerns were based upon ill informed assumptions and equating LCHS, a school of 450 under close supervision, to Valhalla, a school of thousands under loose supervision.

Residents concluded our parking was inadequate as they assumed all 450 students drive to school. Over the past nine years, 10% or less of our student body drives a car to school and parks it on our site. Our home athletic games generate 30-100 spectators. Our parking is ample. They asked if we have assigned parking for students and plan for student parking and monitor it. We do and have successfully for 8 years.

57-B

Residents concluded that traffic will be unbearable as they assumed all 450 students arrive and are picked up at the same time. Parents drop students off from 7:00a to 8:15 am. For 8 years at our current site in Lemon Grove, we have never backed up on the street during drop off time. At the Chase site, we also plan to have our seniors, one fourth of the student population, start later in the morning. With between 100-150 students involved in after school extra-curricular activities, this will stagger afternoon pick up traffic. We also plan to use the parking lot to stage cars waiting to pick up students at the end of the school day. Currently, with 340 students, our pick up lines are clear of the cars who have arrived early (typically around 3:00pm) to wait for their student in 7 minutes from dismissal at 3:30pm and parents can drive onto campus, pick up their student, and drive out without waiting after 3:37pm. Also, some of our students use public transportation which will also ease the traffic burden. Residents assumed our drop off and pick up times are the same as Valhalla. We start and end later than Valhalla.

57-C

Lastly, residents assumed our students will be running across Chase, parents dropping students off across the street, and students accessing the contents of the 7-11, Vape, liquor store, and massage parlor of the nearby strip mall, including their trash cans. We currently have a convenience store / gas station 1/4 of a mile from our current campus. A handful of students walk down to that store before school starts without incident and have been for 8 years. We operate a closed campus and will continue to do so. I have personally monitored drop off and pick up for 12 years, 8 years at our current site. I watch our students properly use crosswalks every day and parents safely dropping students off in the designated locations. Once in 8 years we had a complaint about a student improperly crossing the street. Fear of illegality or irresponsible behavior is not a rational or fair basis to object to a project.

57-D

At LCHS, we take our responsibility as neighbors very seriously. Our neighbors at our current site in Lemon Grove can attest to that. We have had very few complaints from our current neighbors over the past 8 years. Each year we intentionally conduct community beautification projects and clean ups. Despite their reservations, I think our future neighbors will find us a welcome addition to their community.

--
Sincerely,

Steve Robinson

Vice Principal
Liberty Charter High School
8425 Palm Street
Lemon Grove, CA 91945
p:619.668.2131 f:619.668.2133
A Literacy First Charter Schools School

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Cordova, Lorena

From: Sarah Rodriguez <sdrhansen@gmail.com>
Sent: Friday, January 10, 2020 1:07 PM
To: Russell, Denise
Cc: steve.robinson@lfcinc.org
Subject: Literacy First Charter High School

Hi Denise,

58-A I am a parent of a student at Literacy First Charter School and resident of El Cajon. I am writing to urge you to allow the building of the Literacy First High School in Rancho San Diego. This school has provided such a caring school where my daughter is excited to go every day. The current high school campus is far from the elementary and middle schools and it may not be feasible to continue through high school if this new campus is now allowed to open. In my experience I have seen that this school is very considerate of their neighbors and all traffic concerns are addressed, as evidenced in their plans for this high school. They have thoughtfully addressed every concern brought up by neighbors in Rancho San Diego and I know that this school will continually address any additional concerns that may come up. Please allow this great school to build their high school so they can continue to partner with parents to raise great young adults.

Thank you,
Sarah Rodriguez

Cordova, Lorena

From: Rita Youkhana <rita.youkhana@yahoo.com>
Sent: Friday, January 10, 2020 3:02 PM
To: Russell, Denise
Cc: steve.robinson@lfcinc.org
Subject: Liberty Charter High School

Hi Mr. Russell,

My name is Rita Hanna, I live in El Cajon and would like to get an update on the status of proposed Liberty Charter High School. Our family is so lucky to currently have three kids in the Literacy First Charter School, my oldest son will be in the middle school next year and our family would appreciate any opportunity to continue his education in a permanent Liberty Charter high school at corner of Chase Ave and Jamach Rd which is very close to us.

I know process of getting a new school is lengthy, however schools' required documents have been initiated five years ago, do you have an estimated time for this process to be completed?

Do you have any idea when construction work will take a place?

Expediting this process is highly appreciated.

Thanks for your time and response.

Respectfully,

Rita Hanna

Cordova, Lorena

From: Elesha Bacong <ebacong@icloud.com>
Sent: Saturday, January 11, 2020 3:58 PM
To: Russell, Denise
Subject: Our support for LFCS HS

60-A My daughter is currently in 2nd grade with LFCS and would benefit greatly from the HS, her future HS. Both my husband and I attended Valhalla and we do not want our children attending the HS. We love LFCS and fully support the building of the HS. They have addressed every concern as it relates to traffic, location, etc. We just know that this location would be so much better than the Lemon Grove location. I believe with the existence of the new HS location, she and my son to follow will have a more positive learning experience.

Thank you,
Elesha Bacong

Sent from my iPhone

Cordova, Lorena

From: Debbie and Kevin Bailey <kevdebbailey@gmail.com>
Sent: Saturday, January 11, 2020 9:31 AM
To: Russell, Denise
Cc: steve.robinson@lfcsinc.org
Subject: LFCS High School

Good morning,

61-A I am reaching out this morning in regards to an LFCS high school in El Cajon. It is of the utmost importance to us that our children continue their current education at a high school that is aligned with the education and experience they are currently encompassed by. We feel this location in El Cajon is ideal for the high school as it is near all other Literacy First Charter Schools locations. We are well aware that the proposed location addresses all of the County's concerns regarding environmental impact, parking, traffic and architecture that the planners have had concerns about. As proud parents of students attending LFCS and also living in El Cajon, we would like nothing more than having a LFCS high school in El Cajon for our students to attend. We believe this school will have a very positive impact on our community!

Thank you for your time,
Debra Bailey

Cordova, Lorena

From: Noel Elliott <noellelliott87@gmail.com>
Sent: Saturday, January 11, 2020 9:58 AM
To: Russell, Denise
Subject: LCHS

Good morning,

I just wanted to let you know what a positive addition to the community LCHS would be. Both of my daughters attended the regular public school in our area. My older daughter still does. She is in 7th grade. She is behind in her education due to slipping through the cracks like a lot of kids do. I had asked for an intervention however did not receive one. While I believe her teachers do the best they can, the standards are low and it shows. On the other hand, my younger daughter got into Literacy First Charter school this past August! She is in 3rd grade. She too was behind. Without me having to advocate for her, the school got her the intervention she needs and continues to catch her up. LFCS academics are a huge step up from the public school. Not only is the education great, they are HUGE on character, patriotism, and community service. This is where the public schools are lacking. Please take it from someone who has seen both sides and know this high school would not only be a huge positive for my family but a great addition for the community.

Thank you
Noel

Cordova, Lorena

From: Nancy Faulkner <doyoulikecheese@gmail.com>
Sent: Saturday, January 11, 2020 2:47 PM
To: Russell, Denise
Subject: LFCS new high school

Good afternoon,

63-A I can't say enough positive things about our experience at Literacy First Charter. We were lucky to learn about the school from another parent in our co-op preschool program, then applied, and got a spot. My son, currently in 6th grade, has been very successful and so happy there. He is autistic. My sister, a Special Education teacher, has accompanied me to his annual IEP meetings, and was beyond impressed with the thorough, knowledgeable accommodations. It is vital that we be able to continue his great experience after 8th grade! Please help make the new high school a reality.

Thank you,

Nancy Faulkner

Cordova, Lorena

From: Jeni Fondaw <jeni.fondaw@gmail.com>
Sent: Saturday, January 11, 2020 7:38 AM
To: Russell, Denise
Subject: LCHS

Good morning!

64-A We are so excited for the possibility of having a Literacy First High school in our Rancho San Diego community. Our kids would be in walking distance of this wonderful school that promotes not only academics but outstanding character education as well. We live nearby and having seen the plans, we feel assured that traffic will be accommodated by the proposed development and expansion of Chase Avenue.

What an fantastic opportunity it will be to have our kids attend k-12 in a system that we know and trust. We trust your support in this process to allow LCHS to build and grow and make our community even better.

Thank you!

Jeni and Bart Edwards

Cordova, Lorena

From: Lacy Garlow <lacygarlow@gmail.com>
Sent: Saturday, January 11, 2020 8:53 AM
To: Russell, Denise
Cc: steve.robinson@lfcinc.org
Subject: LCHS

Ms. Russell,

I'm writing to you to express my whole hearted support of Liberty Charter High School's new location plan.

My husband and I have three sons in the LFCS/LCHS program and our experiences have been nothing less than exceptional! This school has offered our children a learning environment like no other! In fact when friends share difficult experiences in their children's schools my answer is always, how sorry I am that all schools aren't like ours. The intentional focus on character, accountability, each child being known and supported and being a positive influence in our community is truly setting my kids up to know exactly how to be productive and helpful members of society! This creates a learning environment where every child can succeed. This school has fostered a love for learning in my children and I couldn't be more grateful for it's existence.

The administrators, teachers and staff have my full and complete trust. Having an opportunity to have a new High School built in El Cajon, in close proximity to the K-8 schools, will allow greater continuity between the schools for our kids. The team that has been working on this project for 5 years are people of exceptional integrity. Their efforts have been exhaustive in assessing and addressing concerns and impacts on the community. In this very long and arduous journey the administrators and team have worked tirelessly in the best interest of my children and their peers! As a parent and member of this community, I'd ask that you trust the results of their hard work and allow us the opportunity to prove what a positive impact we can have!

Sincerely,
Lacy Garlow

Sent from my iPhone.

Cordova, Lorena

From: Tasha Smith Godinez <harpbytashasmith@gmail.com>
Sent: Saturday, January 11, 2020 11:32 PM
To: Russell, Denise
Cc: steve.robinson@lfcsinc.org
Subject: Literacy First Charter School

Dear Denise Russell,

I am writing to share with you why my family loves Literacy First Charter School and why we need the High School.

We have two sons at LFCS. One in 7th grade and another in 3rd grade. Our youngest son will start kindergarten at LFCS in a couple years. At LFCS our children have excelled academically. The teachers and leadership at the school are top rate and I don't think there is a better place for our children to learn! The leadership at LFCS works hard to not only teach our children to succeed academically but also to instill strong character traits, patriotism, grit. We LOVE LFCS and are convinced it is where our children need to be for their K-12 education!

Thank you for taking a moment to read this email and for your consideration of the building project for the new high school campus.

Best wishes,
Tasha

Tasha Smith Godinez
Professional Harpist and Music Educator
San Diego Harp Academy
University of San Diego - Applied Music Instructor, Harp
(619) 300.6228
<http://www.tashasmithgodinez.com>

Cordova, Lorena

From: Mary Hermosillo <mscm79@cox.net>
Sent: Saturday, January 11, 2020 9:34 PM
To: Russell, Denise
Subject: LCHS

67-A Hi, my name is Mary and have two daughters in LFCS, one in Elementary and another in Junior, they've been there since kindergarten and they love it here, it is a second home to them and the staff is more like family. It is our purpose to get them in LCHS next, my daughter in Junior is very excited about going to the new High School. We are extremely happy with the program and would like for them to continue it. It would make a difference for us to have the High School in El Cajon and it would save us time and stress with having kids in different campuses. At LFCS and LCHS kids learn more than Math, Science and so; they learn to be good citizens, to be responsible and caring to others, it is just so much more than meets the eye. Having a permanent location for LCHS will be a blessing to our family.

Best regards,
Mary Hermosillo
Sent from my iPhone

Cordova, Lorena

From: Natalie Howard <nataliemtwp@gmail.com>
Sent: Saturday, January 11, 2020 9:06 AM
To: Russell, Denise
Subject: Literacy First Charter School

68-A Literacy First has been such a blessing to our family. The staff strive to give each child the attention and education they deserve. We love this school.

Sent from my iPhone

Cordova, Lorena

From: Abel Juarez <abjrz1985@yahoo.com>
Sent: Saturday, January 11, 2020 10:28 AM
To: Russell, Denise
Subject: LFCS program

69-A I am a parent of two LFCS students, a seventh grader and a kindergartener. I am writing this e-mail in support of the LFCS community and the wonderful staff they employ. As a father who wants a better future for my children and the best opportunity to provide it, I discovered LFCS schools. During some parent/teacher conferences, I have learned how to ensure my children have a great learning environment at home. A permanent high school with this program would be a dream come true, and would help my children as much as it would the youth in the community. Thanks for taking the time to read this e-mail.

[Sent from Yahoo Mail on Android](#)

Cordova, Lorena

From: Alba Katz <lilyvalley2@yahoo.com>
Sent: Saturday, January 11, 2020 8:55 AM
To: Russell, Denise
Subject: LFCS High School (Liberty Charter HS)

Hello Ms. Russell,

70-A My name is Alba Katz and I have lived in El Cajon for the past 34 years. My two children attend LFCS and the education they have received is invaluable. They are challenged every day by caring teachers and supported by all staff. The plan they have for a new high school in El Cajon is so important for us. I drive my kids from rural El Cajon to two different campuses. One of them is not in El Cajon and it has been difficult to drive my older son so far away from where we live. LFCS scores have always been higher than the country's median and it is the best school for my children. It is important for me to have a high school in El Cajon that has the programs that continue their K-8 education. Please support us to have this school built where we live. It would be a great impact on our children's education, finances, and daily commute. The plan for the new high school details impact on parking, traffic, architecture, and the environment that would be of concern. A wonderful team has been working on this plan for five years in order to address any worries that the community or county might have. Please support us to have this school built where we live.

Respectfully,

Alba Katz, BS, RN, RCSN

[Sent from Yahoo Mail on Android](#)

Cordova, Lorena

From: Rihab Matti <rihabmatti@yahoo.com>
Sent: Saturday, January 11, 2020 7:57 AM
To: Russell, Denise
Subject: Lfcs lfhs location

To whom it may concern,

Please we need a literacy first high school in El Cajon city. Our kids really want to have nearby.

As parents we are looking for that too because we like the way they educate our kids, and most of the students lives in El Cajon.

Thank you

[Sent from Yahoo Mail for iPhone](#)

Cordova, Lorena

From: Carol Michael <camshines@cox.net>
Sent: Saturday, January 11, 2020 1:08 PM
To: Russell, Denise
Subject: New High School

To whom this may concern!

This actually concern us,all of us at Literacy First Charter School El Cajon,the best schools ever around. Please consider us please understand that we are a school that sticks together as a family we desire a new high school for our children, so we don't plead with you but we ask you to please consider this new high school for our kids we appreciate all that you could do to make this happen for us. Our children don't only learn great academics skills but Literacy First Charter School's also teach life skills and the importance of their community of their state and their country. Our children from very young age starting in kindergarten learn great skills about how to treat people and how to be a decent human being,kind ,and generous, thoughtful. Literacy first has done that with our children. There is a great generation of young children being raised up from literacy first charter schools.

Thank you
Carol Michael

Sent from my iPhone

Cordova, Lorena

From: ginamikhail09@gmail.com
Sent: Saturday, January 11, 2020 7:37 AM
To: Russell, Denise
Subject: LFCS & LCHS

73-A

My kids have been going to LFCS for 8 years now and I never had problems with anything. Specially with me a single mom having two jobs. school has great values and teaching for our kids great connections between kids and teachers. Every student is known there. Please help us build the new high school for better future for our kids.

Thank you,
Gina Mikhail

Sent from my iPhone

Cordova, Lorena

From: reven namo <revennamo@yahoo.com>
Sent: Saturday, January 11, 2020 8:21 AM
To: Russell, Denise

74-A From my family we highly recommend building hlfc first hi school in elcajon, my two kids currently going to middle school

[Sent from Yahoo Mail for iPhone](#)

Cordova, Lorena

From: Leslie Giampa <lesliegiampa@yahoo.com>
Sent: Saturday, January 11, 2020 9:18 AM
To: Russell, Denise
Subject: New location for Liberty High school

Dear Ms. Russell

75-A My name is Leslie Noriega and my children attend Literacy First Charter School(LFCS). We have been part of LFCS for 5 years, and I am so impressed with the quality of education my children are receiving. They are not only learning reading, writing, and arithmetic but they are also learning about the importance of character. My 4th grader loves LFCS and has told me that she is looking forward to attending Liberty High School. I am excited that LFCS is in the process of purchasing land in El Cajon for a new high school. The majority of our students live in East County and our 3 other campuses are also located in El Cajon. It makes sense that the high school would also be located there. I am hopeful that the County will approve the purchase of this land for the building of our new high school. I appreciate your time and consideration.

Sincerely,

Leslie Noriega
619-885-7830

[Sent from Yahoo Mail on Android](#)

Cordova, Lorena

From: Kyle Oliphant <clomok@gmail.com>
Sent: Saturday, January 11, 2020 4:12 PM
To: Russell, Denise
Subject: Liberty charter high school - too much traffic

76-A As a resident that lives on Monument Hill Rd, less than 1/8th of a mile from the proposed site of liberty charter high school, I am completely against it because of its negative effects to traffic. Traffic is already bad with Valhalla and Hillsdale. My street has only one entrance and exit; in the morning and afternoon, when school traffic is at its peak, it can easily take 10 minutes for it to be clear enough to be able to get out of the street.

My family and I are 100% against this new school and it's added traffic to Chase Ave.

Kyle Oliphant
619.438.0368

Cordova, Lorena

From: arrw arrw <arrw2001@yahoo.com>
Sent: Saturday, January 11, 2020 10:54 AM
To: Russell, Denise
Subject: LFCS high school in El Cajon

77-A It is important to our family to have an LFCS high school in El Cajon.
it is important for our child to continue their high school years in a school
where the programs are aligned with their K-8 experiences.
We would be greatly appreciated

Best Regards
Amir Oraha

Sent from [Mail](#) for Windows 10

Cordova, Lorena

From: Elizabeth Perez <mrslizzyperetz@yahoo.com>
Sent: Saturday, January 11, 2020 10:40 AM
To: Russell, Denise
Subject: Literacy First High School

Hello Denise,

78-A As a parent of a 4th grader and Kindergartner at LFCS we feel that is so important that we have a new high school for our kids when they are ready to go. LFCS has been an excellent school for our boys and they are thriving so much. The teachers, curriculum, families and resources are above and beyond. We really hope this moves forward and just know that it would be a huge blessing. Thank You!!

Sincerely,
Elizabeth Perez

Sent from my iPhone

Cordova, Lorena

From: Angelica Retano <retano.a@gmail.com>
Sent: Saturday, January 11, 2020 10:24 AM
To: Russell, Denise
Subject: High school for charter LTCS

79-A Hi my name is Angelica retano, I currently have 2 children enrolled in LTCS, I have a total of 4 children my two oldest also attended this wonderful school . I would like to express how magnificent it would be if we can have our permanent high school!

Cordova, Lorena

From: Verhoye, Carolyn <carolyn.verhoye@lfcsinc.org>
Sent: Saturday, January 11, 2020 8:53 AM
To: Russell, Denise
Subject: LFCS Liberty High School

Dear Ms. Russell,

I hope this message finds you well.

I am an educator at LFCS Junior Academy and wish to voice my enthusiastic support for the proposed location of our future charter high school. I understand that the plans have been thoroughly examined for many years, taking all factors into consideration. LFCS is an amazing body of outstanding leaders, educators and families. Our organization's integrity and solid moral values emanate into the community and have produced thousands of conscientious, educated, patriotic citizens.

I urge you to support the planning and construction of this fine institution. Thank you for your kind consideration.

Gratefully yours,

Carolyn Verhoye

--

Carolyn Verhoye
Intervention Teacher
LFCS Junior Academy

Cordova, Lorena

From: Arkan Alnagar <lediaalnagar@icloud.com>
Sent: Sunday, January 12, 2020 5:53 PM
To: Russell, Denise
Subject: High school

81-A I have two daughters one attend literacy first and the second one attends junior academy middle school and I'm hoping to have a high school opened in El Cajon. It would could be convenient for my two daughters.. I love charter school system and they work so hard hand in hand with the staff, teachers n parents. I would be gladly appreciated for them to continue their high school education.

Ledia Alnagar
Thank you.
Sent from my iPhone

Cordova, Lorena

From: Timothy Bottenberg <timcan327@gmail.com>
Sent: Sunday, January 12, 2020 6:53 PM
To: Russell, Denise
Subject: LCHS move to Chase Ave.

Ms. Russell

82-A

Hello, I am a Junior at Liberty Charter High School and I feel very fortunate to go to a Charter School. I would really appreciate if you would consider to approve the moving of Liberty Charter.

Thanks,
Tim Bottenberg

Cordova, Lorena

From: suolleen mansor <soulleen@yahoo.com>
Sent: Sunday, January 12, 2020 3:34 AM
To: Russell, Denise
Subject: About Ifcs high school location

83-A We need charter high school here in El Cajon because it is very important for our kids to continue their education in literacy first charter schools in El Cajon near us

[Sent from Yahoo Mail for iPhone](#)

Cordova, Lorena

From: Balsam Mikha <balsammikha@yahoo.com>
Sent: Sunday, January 12, 2020 11:23 PM
To: Russell, Denise
Subject: LFCS High School

to whom it may concern,

84-A My daughter started Kindergarten in LFCS, and I would like her to continue her high school years in the same program and school she started with. LFCS is one of the best schools in the county, and having a high school in El Cajon is going to be great for our kids and very helpful to the parents. please take it in consideration and help our students to continue what they started.

thank you

Cordova, Lorena

From: amal shattah <amalshattah69@yahoo.com>
Sent: Sunday, January 12, 2020 11:11 PM
To: Russell, Denise

85-A Hi, I am Ewan's mother, I highly recommend and prefer the location of LFCS high school to be in El Cajon because for us, Ewan's parents, it will save a lot of time driving Ewan to school in the morning and picking him up in the afternoon. it will also make it much easier for us to manage our time as students and employees.
We definitely prefer that Ewan continues his education in this school and we are so impressed by the level of education, disciplines, propriety and precision that this school is providing the students with.

[Sent from Yahoo Mail for iPhone](#)

Valle de Oro Community Planning Group
3755 Avocado Blvd #187
La Mesa, CA 91941

January 12, 2020

County of San Diego
Department of Public Works
Transportation Division
5510 Overland Ave., Suite 410, Room 470
San Diego, CA 92123-1239

RE: LIBERTY CHARTER HIGH SCHOOL MND PDS2015-MUP-15-027

Dear Denise Russell,

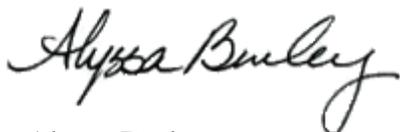
The Valle De Oro Community Planning Group (VDO CPG) has approved the following motion with regards to the Liberty Charter High School (PDS2015-MUP-15-027) Mitigated Negative Declaration (MND).

86-A With a unanimous vote for approval (11 yea, 0 nay, 3 absent, 1 vacant), the VDO CPG Literacy First/Liberty Charter High School Ad Hoc Subcommittee Chair Stephen Bickley motioned "to object to the Mitigated Negative Declaration and ask for a full Environmental Impact Report (EIR) based on concerns brought up at [the January 7, 2020 VDO CPG meeting] about community character and benefit, safety, aesthetics, and traffic by the public, the Grossmont Union High School District, and our Board."

86-B More specifically, the VDO CPG's concerns about the MND include:

- Only one mention of Valhalla and 2,230 students and staff 3,500 feet away from the proposed project
- Air Quality - emissions during construction
- Public Service - MND does not address proximity of Valhalla nor impacts on students or educational programs
- Traffic - no analysis of 850 vehicle trips given proximity to Valhalla
- General - (a) no analysis of potential conflicts with neighboring businesses, specifically liquor store, vape shop, hookah lounge, massage spa and restaurant with a liquor license, (b) appropriate EIR would allow further/better mitigation of issues.

Respectfully Submitted,



Alyssa Burley
Valle De Oro Community Planning Group
Chairperson

Cordova, Lorena

From: Brianne Walker <brianne.walker@gmail.com>
Sent: Sunday, January 12, 2020 8:46 PM
To: Russell, Denise
Subject: LFCS High School

Hi Denise,

I hope you have heard from many LFCS families. Our school is a very special place and the families that attend this school all have the same mission and goal: to allow our kids a safe place to learn not only academics but incredible character. We feel so blessed to be able to be a part of LFCS and dream about our kids continuing on to high school at LFCS HS one day!

Please listen to the hearts of these families. We need more schools in our city that are investing in our future by raising up honorable kids!

Please allow LFCS to build this dream campus for more families to experience the greatness of our school family!

Thanks,
Brianne Walker
Parent of 3rd, 1st and kindergartener

Cordova, Lorena

From: Blake Woods <bandbwoods1999@gmail.com>
Sent: Sunday, January 12, 2020 10:21 PM
To: Russell, Denise
Subject: Re: LFCS/LCHS Future Campus in El Cajon

Dear Ms. Russell,

I write this email on behalf of my family, and fellow parents and staff of LFCS and Liberty Charter High School. We are very, very excited that our Literacy First Family are getting closer to having a brand new high school in El Cajon. My daughter has attended Literacy First from Fifth Grade through Eighth. We unfortunately made the decision to send our daughter to another high school this year, due to it being closer to our home. We have been less than impressed with the high school she attends now and look forward to putting our daughter back in the LFCS community this next school year and watch her succeed in an environment that is rich in character education, as well as having high academic standards for our children.

88-A

I know that the LFCS administration have been working very hard over the past five years to make sure that the new high school in El Cajon, which is slated for Chase and Jamacha Road, will be a good fit for this community. They have addressed all of the County's concerns regarding traffic, parking, environmental impact and architecture that the Planners have had concerns about. It will offer an incredible opportunity to current LFCS/LCHS students, as well as future generations. It is a place that has become a second home to many of our kids. It is a place where they are known, by name. They are a part of a community where everyone is valued and cherished.

My daughter has seen the difference in going to a regular public high school in east county vs. the LFCS/LCHS and wants nothing more than to return to a school where she feels safe and valued. LFCS/LCHS instills a desire to learn and succeed in all their students. Teachers devote their time to their students. They are not just a number!

We need Liberty Charter High School in El Cajon. More students need to experience a quality education with rock solid teachers, who truly care for our kids. LCHS students are goal-oriented, compassionate kids....Let's make this opportunity available to more kids in the East County, by opening the El Cajon location, as soon as possible!

Thank you for hearing our comments and concerns!

Blake Woods
Parent/Teacher

Cordova, Lorena

From: revan zuhair <reavan78@yahoo.com>
Sent: Sunday, January 12, 2020 9:15 AM
To: Russell, Denise
Subject: LFCS

89-A LFCS is an amazing school, and we need high school located in El cajon.

Regards

[Sent from Yahoo Mail on Android](#)

Cordova, Lorena

From: Merna Alkatib <merna71.alkatib@gmail.com>
Sent: Monday, January 13, 2020 10:23 PM
To: Russell, Denise

90-A My concern for the high school project is for many reasons. One of them is the high volume of cars on chase rd starting from Anza up to Jamacha. It's only one lane. The shopping center parking lot would get used by the students families before and after school which would affect the businesses parking area. The parking inside the school is only 161 lots so people would wait at the shopping centers parking lot making it crowded and chaotic. People leaving home using chase rd, they are going to be surprised with the heavy traffic from the school because chase rd is the only rd that is connected to highway 8 from Rancho San Diego and Jamul area. The parking is also too tight for that many cars. It is a very tight area for a school like that. The traffic study for the school was made on 2014 and we are in 2020 which means that the number of cars got doubled when they used chase rd. Chase already does not have a walking safety side for people to walk through because the road is so tight from Anza all the way down to Jamacha.

-Mahir Alkatib

Cordova, Lorena

From: James Wells <James@triplek.com>
Sent: Monday, January 13, 2020 9:36 AM
To: Russell, Denise
Cc: steve.robinson@lfcsinc.org; debbie.beyer@lfcsinc.org
Subject: Liberty First Charter High School Construction

Ms. Russell,

I would like to formally express my support for the approval of plans to proceed with construction of the Liberty First Charter High School project your team now has under consideration. I express my support as both a parent and an immediate neighboring homeowner to this project.

91-A

I welcome seeing this undeveloped land built into what I consider one of the most successful education endeavors in our County. This project will be a wise use of the property and an asset to both neighboring children and the residential community in the vicinity and beyond.

Thank you for your support of this project,
James

James Wells
Vice President of Sales & Marketing
Triple K Manufacturing Co.
2222 Commercial St.
San Diego CA 92113
619-232-2066



Cordova, Lorena

From: Tina Wells <tinacwells@gmail.com>
Sent: Tuesday, January 14, 2020 8:09 AM
To: Russell, Denise
Subject: LCHS

Dear Ms. Russell,

92-A We are in our 11th year with this charter starting in kindergarten at LFCS and now in high school at LCHS. I am truly grateful for the exceptional education that my children are receiving here. It has been a wonderful blessing to be involved in a choice/charter school. Our children are known and greeted each morning by name. They are thriving academically, and immersed in a culture that encourages growth in the whole person as literate, articulate, life long learners. They have deep friendships and a community of teachers and parents that care for them.

Education is not "one size fits all," and I am pleased to have an alternative, small high school. It is my sincere desire to see the high school move to a permanent home at Chase/Jamacha.

Thank you for your participation in this process.

Sincerely,

Tina Wells

Sent from my iPhone

Christie Haigh
11711 Shadow Valley Rd, El Cajon, CA 92020
15 Jan 2020

Denise Russell
Planning & Development Services Project Processing Counter
5510 Overland Ave, Suite 110, San Diego, CA 92123

Subject: Comments on draft General Plan Amendment and draft Mitigated Negative Declaration for Liberty Charter High School; PDS2015-MUP-15-027; PDS2015-ER-15-14-010

Dear Ms. Russell,

93-A

I am writing to express my deep discomfort with the plan to add Liberty High School on the corner of Chase Avenue and Jamacha Blvd. While the initial study was completed in 2015, using 2014 data, that data is now old and outdated and should not be used to support this project in 2020. The basic data is no longer accurate, and it does not take into account community changes in this area. In addition to general increases in traffic since that initial study, almost 40 houses in the Fuerte Ranch Estates are scheduled to be built next to Fuerte Elementary on Fuerte Dr. Almost all of those houses will need to use Chase Ave to access Hillsdale Middle School, Valhalla High School, and jobs or activities. Fuerte Elementary has grown by over 10%, or 60+ families, and almost all of those families are not from the Fuerte area, which means that those students commute to Fuerte Elementary, and many of them access Fuerte Dr from the Chase Ave corridor.

Most of the rest of my concerns fall into several key areas:

SAFETY

93-B

- Current local students walking or biking to Valhalla will be placed in danger by the extreme increase in traffic trying to access Liberty High. Valhalla students will have to pass the Liberty entrance, while watching for traffic crossing from both sides of Chase. As it will be challenging for Liberty drivers to access the parking entrance due to traffic numbers and traffic speeds, drivers are likely to be in a hurry to access Liberty and are not likely to pay attention to students biking or walking down Chase.

93-C

- You are looking to put 450 teenagers directly next door to a small strip mall that includes bars, a liquor store, a hookah lounge and a vaping store. Many of these stores have been there for years and there would be neighborhood uproar if you were to move them. Albert's – a Mexican restaurant with a bar – has been a strong community supporter for a decade. Vaping is already a serious issues for today's teens and, when they don't even need to drive to access supplies, it's likely to get worse.
- A 7-11 on the opposite corner will tempt teens to cut across Chase for snacks, vaping, cigarettes or liquor.

93-D

- Liberty representatives said that most of the current students come from El Cajon city, which means students and staff would commute to an unincorporated County location. For a completely commuter campus, which has absolutely no street parking on Chase or Jamacha, parking is woefully insufficient. Carpooling is often not used and some percentage of the Junior/Senior students (approximately 50% of students, or up to 225 teens will be old enough to

drive) will want to drive to school. There will be 161 parking spaces, which include staff parking (33 estimated teachers and staff), and dedicated vanpool, handicapped, and electric car parking spots. Liberty High representatives use neighborhood school statistics for parking requirements, but this is not a neighborhood school. There is absolutely no street parking on either Chase or Jamacha for blocks (most neighborhood school statistics rely on students parking, or parents dropping off, in the neighborhood surrounding the schools; none of that is available to this campus). If only 50% of the Junior/Senior population drives, that will be over 100 cars, just from driving students. With less than 130 parking spots (once you removed faculty/staff parking), some of these students will be looking for spots to park. Since there are no legal ones, teens may make faulty and dangerous decisions on where to leave their car and how to quickly get to campus.

93-E

- Only 28 cars fit in the drop off zone before traffic backs up the hill on Chase. Faced with cars backing up on both Chase and Jamacha (current traffic turning left from Jamacha to Chase often backs up well past the turn lane), parents will be tempted to pull into the stores on the north side of Chase and have their students cross Chase. Many of them will not take the time to go down to the crosswalk.

93-F

- School attendees do not access or exit school once per day. They come and go various times, for sports or after school activities. Games or activities often include visitors from other schools. If Liberty cannot handle parking for their own students, where are students/supporters from other schools supposed to park as there is not a single legal neighborhood parking space within blocks of the school? Next to the liquor store? In the vaping supplies parking lot? When they see the tow notice in the store parking lots, will they try to park illegally on Chase or Jamacha and run across the street, illegally, because they can't find a legal option to park? Most likely. And, due to the speeds on those roads, that decision is inevitably end in tragedy. Even one student killed is one too many and store owners all have stories of horrible accidents in this area.

93-G

- In addition, school statistics for any school in Grossmont Unified High School District shows that schools often end up exceeding their initial enrollment estimates. What happens when Liberty decides to increase to 500 students down the road? Or even 475? There is absolutely no room to expand the parking lot.

TRAFFIC

93-H

- There are currently multiple times each day where eastbound Chase backs up to the crest of the hill on Chase. With only 28 dropoff cars allowed before traffic backs up onto Chase, this problem will get much worse. Valhalla High School has students starting at approximately 7:30am and 8:30am. Anyone heading to Valhalla from Chase will impacted by staff and students trying to access Liberty. Cars turning from Jamacha to Chase, and trying to access the left turn lane into Liberty (which will only hold a few cars), will back up the huge amount of traffic which accesses westbound Chase to get to the 8 Freeway (this route is often much faster than continuing north up Jamacha). Once the 28 cars fill the drop off zone, traffic will back up in both directions. This will keep cars from progressing to Jamacha north, crossing Chase to Valhalla or the neighborhoods, or turning south onto Jamacha to access Hillsdale Middle. With no lights, and way too many cars reliant upon this intersection, road rage accidents will likely implode.

93-I

- In addition, traffic turning onto westbound Chase has to cross traffic to access the school. This alone is dangerous, but this is currently a very busy area of Chase. The left turn lane into Liberty

comes right after a merge on westbound Chase. The eastbound turn from Chase to Liberty is right before Chase splits to turn left onto Jamacha, straight across Chase (to access Valhalla or neighborhoods), or right onto Jamacha (to access Hillsdale Middle or Rancho/94W to 125). There is already a lot going on, before adding this school. With the school, it will become much more chaotic, which will result in more accidents. Is this really where we want to add 450 teenagers? Do we care that little about them?

IMPACT ON COMMUNITY

93-J

- The Chase Corridor is the primary access point for hundreds of houses on both sides of Chase Blvd. With the current traffic level (that is, well before the addition of this potential high school), it can take 5-10 minutes to turn onto Chase Blvd or to turn from Chase Blvd to one of the feeder roads. If this school is added, it will take even longer to get out of our homes, to school or work.
- There is a 1 ½ miles stretch from the Chase/Jamacha light before you hit the next light on Chase at Anza. Cars pick up speed, especially as they hit the crest of Chase Ave (which is also the main access point for several communities, including Chase Lane, Fuerte Knolls, Chase Terrace, Bernita, Grove), before they barrel down towards Jamacha. Cars often travel 50-60 miles/hour in this stretch. They will have very little time to slow down before they hit the cars backed up on Chase, trying to access the drop off at Liberty.
- There are many senior residents who are already uncomfortable with the speed on Chase and Jamacha. Adding the potential for hundreds of teen drivers is incredibly upsetting and incredibly dangerous for them.

93-K

- GUHSD is not at capacity. There is no need for an additional school here. There are already good Charter School options within the district. This is an unnecessary addition which will greatly increase density in a rural area. People bought here to have space and quiet. It is hard to imagine anything on this space that would have a great negative impact than a high school.
- The people who support this school are primarily NOT neighborhood residents. El Cajon City is very different from El Cajon County. El Cajon County already has very good Charter School options within GUHSD. This school is unnecessary and will create all sorts of local problems. Alpine has been asking for a high school for years. Wouldn't that be a much more appropriate place for this endeavor?

This project has not been thoroughly vetted and I believe that the County will come to greatly regret adding such a high density project to this area.

Sincerely,

Christie Haigh

Cordova, Lorena

From: Abdul Hakim Mateen <abdulhakim_mateen@yahoo.com>
Sent: Wednesday, January 15, 2020 6:29 PM
To: Russell, Denise
Subject: Literacy First Charter high school in El Cajon

Dear Russell,

I am writing this email for you in support of having literacy first charter high school here in El Cajon.
I am Abdul Hakim Mateen a resident of El Cajon CA, my children are currently studying at Literacy First Charter school.
As a resident we have the right to have access to better education to serve better our great country and community, therefore I feel having Literacy First Charter high school could be a part of that, and is essential for our children future. I fully support and ask please help our kids and our future to make it happen.
Appreciated for consideration,
Abdul Hakim Mateen

94-A

Cordova, Lorena

From: Lesli Navarro <lesli1608@hotmail.com>
Sent: Wednesday, January 15, 2020 10:49 PM
To: Russell, Denise
Subject: Literacy First High School

95-A As a mother, it is very important that this LFHS project be carried out for the future of my children, good education and values. I ask you to think about our children and accept this wonderful project.

Obtener [Outlook para Android](#)

Subject: Proposed Liberty HS
From: Karen <klbeck@cox.net>
Date: 1/16/2020, 11:28 PM
To: alyssa@alyssaburley.com

96-A

I'd like to state for the record that I am totally AGAINST this high school

Thank you

DEPARTMENT OF TRANSPORTATION

DISTRICT 11
4050 TAYLOR STREET, MS-240
SAN DIEGO, CA 92110
PHONE (619) 688-6075
FAX (619) 688-4299
TTY 711
www.dot.ca.gov



*Making Conservation
a California Way of Life.*

January 16, 2020

11-SD-125
PM 10.5
Liberty Charter High School
SCH# 2019129039
MND

Ms. Denise Russell
Land Use/Environmental Planner
County of San Diego
Department of Planning and Development Services
5510 Overland Avenue, Suite 110
San Diego, CA 92123

Dear Ms. Russell:

97-A

Thank you for including the California Department of Transportation (Caltrans) in the Mitigated Negative Declaration (MND) review for the proposed Liberty Charter High School located on Chase Road near Jamacha Road (State Route 54 (SR- 54) in the community of Valle de Oro. The mission of Caltrans is to provide a safe, sustainable, integrated and efficient transportation system to enhance California's economy and livability. The Local Development-Intergovernmental Review (LD-IGR) Program reviews land use projects and plans to ensure consistency with our mission and state planning priorities.

Caltrans has the following comments:

Traffic Impact Study

A traffic impact study (TIS) is necessary to determine this proposed project's near-term and long-term impacts to the State facilities – existing and proposed – and to propose appropriate mitigation measures.

97-B

- Chase Road and Jamacha Road (SR-54). The geographic area examined in the TIS should also include, at a minimum, all regionally significant arterial system segments and intersections, including State highway facilities where the project will add over 100 peak hour trips. State highway facilities that are experiencing

Ms. Denise Russell
January 16, 2020
Page 2

noticeable delays should be analyzed in the scope of the traffic study for projects that add 50 to 100 peak hour trips.

97-C

- A focused analysis may be required for project trips assigned to a State highway facility that is experiencing significant delay, such as where traffic queues exceed ramp storage capacity.

97-D

- In addition, the TIS could also consider implementing vehicles miles traveled (VMT) analysis into their modeling projections.

97-E

- Any increase in goods movement operations and its impacts to State highway facilities should be addressed in the TIS.

97-F

- The data used in the TIS should not be more than 2 years old.
- Please provide Synchro Version 10 files.
- Early coordination with Caltrans is recommended.

If you have any questions, please contact Mark McCumsey at (619) 688-6802 or by email at mark.mccumsey@dot.ca.gov.

Sincerely,



MAURICE EATON, Branch Chief
Local Development and Intergovernmental Review Branch

DEPARTMENT OF TRANSPORTATION

DISTRICT 11, DIVISION OF PLANNING

4050 TAYLOR ST, M.S. 240

SAN DIEGO, CA 92110

PHONE (619) 688-6960

FAX (619) 688-4299

TTY 711

www.dot.ca.gov



*Serious Drought.
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November 17, 2015

11-SD-54
SR-54, PM T13.72
Liberty Charter High School
PDS2015-MUP-15-027

Planning & Development Services
County of San Diego
5510 Overland Avenue, Suite 110
San Diego, CA 92123

Dear Sir or Madam:

Thank you for including the California Department of Transportation (Caltrans) in the application review process for the project referenced above. We have reviewed the major use permit and have the following comments to offer.

Traffic Study

A traffic impact study (TIS) is necessary to determine this proposed project's near-term and long-term impacts to the State facilities – existing and proposed – and to propose appropriate mitigation measures. The study should use as a guideline the *Caltrans Guide for the Preparation of Traffic Impact Studies*. Minimum contents of the traffic impact study are listed in Appendix "A" of the TIS guide.

The *TIS Guide* is available at the following website address:
http://dot.ca.gov/hq/tpp/offices/ocp/igr_ceqa_files/tisguide.pdf

The geographic area examined in the traffic study should include as a minimum all regionally significant arterial system segments and intersections, including State highway facilities where the project will add over 100 peak hour trips. State highway facilities that are experiencing noticeable delays should be analyzed in the scope of the traffic study for projects that add 50 to 100 peak hour trips.

A focused analysis may be required for project trips assigned to a State highway facility that is experiencing significant delay, such as where traffic queues exceed ramp storage capacities. A focused analysis may also be necessary if there is an increased risk of a potential traffic accident.

The data used in the TIS should not be more than 2 years old.

Cordova, Lorena

From: Shawn Dennison <sdennison@cox.net>
Sent: Thursday, January 16, 2020 4:29 PM
To: Russell, Denise
Subject: Liberty Charter High School PDS2015-MUP-15-027 PDS2015-ER-15-14-010

Dear Denise,

98-A I just wanted to let you know that I am totally against this Charter School going in at the corner of Chase and Jamacha. Traffic in this area is already hell as it is! Valhalla High School is just a stones throw away as well as Vista Grande Elementary. By placing a school at that location it will make it virtually impossible to get out of the area. Just pull up the numbers of accidents we have at that intersection now, I see emergency vehicles heading there virtually every day.

Please do not allow this school to be built it is will contribute to an already dangerous situation.

Sincerely,

Shawn Dennison
Rancho San Diego Resident

Subject: Liberty Charter site

From: Cathy Jezierski <cjezierski@cox.net>

Date: 1/16/2020, 9:10 PM

To: Alyssa@alysaburley.com

99-A I am writing to send my opinion on the Liberty Charter site on the corner of Chase and Jamacha Road in El Cajon. I am never against schools, but this is not the place for a school. The traffic coming down Chase is horrible at all times of the day and people come flying down the hill and many times run the red light at the intersection of Chase and Jamacha. Having a school at that corner will only add to the congestion that we already have.

99-B Also, we have two high schools near this site. Valhalla High School is less than a mile down the road and not full with students. Steele Canyon High School is a few miles down Jamacha towards Spring Valley. Why add another high school when we have those two close and even Granite Hills High School is about the same distance as Steele Canyon High School, so you already have three highs in a small distance apart.

I understand that they need to stay in the Grossmont School District, but with Alpine not having a high school, why not have them set up camp there?

99-C Also, as you know there is a liquor store and vaping store in the shopping center right in front of where the school would be built. I don't think this is something that should be so close to a school.

Thank you for your consideration and my vote would be "no" to Liberty Charter in Rancho San Diego.

Cathy Jezierski
1651 Monarch Ridge Circle
El Cajon, CA 92019

Cordova, Lorena

From: Susan Kellstrom <saki2001@sbcglobal.net>
Sent: Thursday, January 16, 2020 10:14 AM
To: Russell, Denise
Subject: Liberty Charter High School Feasibility Study

Hello Ms. Russell!

100-A How are things going? My wife and I are residents in the area of unincorporated East El Cajon that will be adversely affected by the increased traffic generated by the creation of Liberty Charter High School. We live a few blocks away from the Jamaica/Chase area and can attest to the increased activity every day that now occurs due to the Valhalla High School traffic. Any increase to this amount of already existing traffic would be catastrophic in the face of emergencies, such as wildfires. We have lived here for over two years and have noticed the inadequacy of the local roads to accommodate emergency traffic trying to evacuate fire areas. Adding more traffic at least twice daily would certainly exacerbate this situation to the point of extreme human loss if a fire occurred during school hours.

Please consider this in your planning? We don't think widening the roads can alleviate this situation. The students and parents at Valhalla High and the future students of Liberty Charter really deserve better. Further, a sand mining project that is proposed for the Cottonwood Golf Course, in the near future, if approved, would further exacerbate the potential for a traffic nightmare of disastrous proportions.

Thank you for your prompt attention to this matter!

Respectfully submitted,
John R. Wittenbel and Susan A. Kellstrom
2014 Darlington Ct., El Cajon 92019
619-456-4762

Cordova, Lorena

From: Donna McKenzie <handsmacd@gmail.com>
Sent: Thursday, January 16, 2020 4:49 PM
To: Russell, Denise
Subject: Liberty Charter High School

I am very much opposed to the location on Chase and Jamacha for Liberty Charter High School !!!!!

The traffic congestion would be unbearable and the safety of the students would be a huge concern . I have heard that someone from the school stated , “the students would not cross Chase to get to the 7/11.....”. i would really like to know how long the person has been working with high school students , as that is a totally ridiculous statement .

Please do not put the students, or residents in this area in an unsafe situation !!!! This is the WRONG location for a school ‘

D. McKenzie
Sent from my iPhone

Cordova, Lorena

From: Linda Shoemaker <ljshoemaker@cox.net>
Sent: Thursday, January 16, 2020 5:57 PM
To: Russell, Denise
Cc: ljshoemaker@cox.net; rdshoemaker@cox.net
Subject: Liberty Charter School
Attachments: Llbrty Charter School.docx

Please accept our feedback on this project by the required 1\17\2020 date. We attended the Cajon De Oro planning meeting Jan 7. I included the email text below in a Word Doc attached.

To: Valle de Oro planning group/County planning commission
 Re: The proposal for Liberty First Charter School in Rancho San Diego
 To whom it may concern,

Our family has lived at 1893 E Chase Ave since 1988; approximately 30 years. Our children attended Fuerte Elementary School, Hillsdale Middle School and Vahalla High School. Our home borders the property for the proposed school on the South West corner of the lot. So, we know, first-hand, about the traffic patterns in this area. We do not believe that this location is appropriate for the use intended by another high school and will create public safety problems that cannot be mitigated to this location. Our concerns are:

- Chase Ave is one lane in each direction west of the proposed development and is impacted by traffic volume regularly by students attending Valhalla High School, Hillsdale Middle School, and Fuerte Elementary school during the normal morning and afternoon drop off and pick up times. The bike lane\shoulder in front of our driveway is regularly used as a second lane when the traffic backs up at the traffic light at Jamacha, and drivers use the shoulder to turn right at Jamacha.

- Chase Ave is a main thorough fare between Jamacha connecting Hwy 94 and Interstate 8. It has already been identified as a road impacted by traffic congestion
- Traffic backs up regularly in the mornings and afternoons from Jamacha to Chase lane and can go even farther at the peak times.
- The development appears to propose 2 unprotected turn lanes on Chase Ave as a mitigation but, this will significantly impact traffic on Chase Ave. in both directions and make the problem worse.
- Driveways along Chase will be significantly impacted during the drop off and pick up times of the proposed development. Driveways will be used as turnouts and drop off points by cars hoping to avoid turning into the parking lot
- A current and comprehensive traffic study on Chase Ave should be required. (the last study was done in 2014) The proposed traffic mitigation from this study should address the traffic on Chase Ave that would be increased by the proposed development
- This school project must address the traffic impact at the intersections of Chase\Jamacha and Fuerte\Chase
- The project proposes approximately 140-160 parking spaces for an enrollment of 400 plus students and 33 staff. This does not seem realistic. The suggestion that minimal impacts to traffic on Chase Ave and the intersections at Jamacha and Fuerte seems unrealistic when the additional stated traffic volume is 800 trips to this school per day

ON site parking appears to be inadequate for 400 students daily (800 auto trips). Another concern is when there are special events at the school such as sporting events and parent nights. There is **NO OFF SITE PARKING ANYWHERE** near the vicinity to accommodate overflow or special events. How will this be addressed? County standards related to the number of parking spaces appear to be at a minimum and not at all adequate for this type of facility.

We do not believe students and parents that arrive to drop off or pick up students will pull into the parking lot but use driveways and streets close to the school. A BIG public safety concern with this project is students jay walking across Chase Ave. This will be unavoidable and we would like to know how this project will address this concern? Another concern is that students will not use the crosswalks but will run across Chase Ave to shop at the 7 Eleven before or after school creating very hazardous conditions for them. There is also the concern as a neighbor of the school, that our driveways will be blocked when we are coming and going places

102-D Another concern that we have is that several business adjacent to the proposed school have liquor licenses such as the 7-11, the liquor store and the Alberts Mexican food restaurant. How will the terms\conditions\restrictions of their licenses be affected by the placement of a school within 300 ft of these establishments? Will this adversely affect these businesses? There is also a vaping shop and Hookah lounge which should be a concern so close to a school.

102-E There was an estimate given of noise levels exceeding 70db at times at the school. This will significantly affect neighbors. For a rural neighborhood with sound levels much lower than average city volumes, the change would be very difficult to adjust to. The sound increase to neighbors appears to be a magnitude greater than what our rural community can cope with.

102-F This type of land use\development is incompatible with the immediate and surrounding area and is not in the public interest. How is this proposal justified? Text book minimum county standards are insufficient when citing a project that will rezone property from residential to school use.

102-G How will the visual impacts of the lighting of the parking lots and the facility at night and during sporting events to the surrounding neighbors be addressed?

102-H The on line documents related to biological resources seems to downplay the hawk population by citing a visual report of 1 hawk. We regularly see 4 adults plus juveniles annually. They nest in the palm trees on the West side of the subject property. They regularly catch squirrels, gophers and rabbits in around my property. We also see them in the California pepper trees on my property. There are also coyotes that gather often in the property at night. Their habitat has been almost erased by all of the development in the area.

102-I We are also asking that this project be required to follow the full Environmental Impact Review (EIR) process to fully identify the impacts to the environment and identify appropriate mitigation to the impacts at this location. In closing, there are many things to be concerned about with this project. Please address them seriously before moving forward. We do not believe that this is the right location for this school.

Respectfully,

Rich & Linda Shoemaker
1893 E Chase Ave
El Cajon Ca 92020
ljshoemaker@cox.net

Subject: Liberty Charter school

From: "Claudia ." <nohemi_19@hotmail.com>

Date: 1/17/2020, 3:37 PM

To: "alyssa@alyssaburley.com" <alyssa@alyssaburley.com>

Valle del Oro planning Group,

103-A

I am writing to register my opposition to the proposed constructions of Liberty Charter High School near the intersection of Chase Ave and Jamacha Rd. I reside at 2044 George Maria Way and we take Chase ave every day and see the congestion, is a school is built that would only get worse.

Thank you,
Claudia Ayala

Sent from my iPhone

January 17, 2020

County of San Diego
 Planning & Development Services
 Denise Russell, Land Use/Environmental Planner
 5510 Overland Avenue, Suite 310
 San Diego, CA 92123
denise.russell@sdcounty.ca.gov

RE: Record ID: PDS2015-MUP-15-027
 Environmental Log No.: PDS2015-ER-15-14-010
Project Title: Liberty Charter High School

Dear Ms. Russell:

The following submit this public comment in opposition to the Liberty Charter High School proposed project due to its many violations of the California Environmental Quality Act, the substantial hardship and impact to local businesses, and the extraordinary dangers and high likelihood of loss of life of children it will cause:

Gordon and Marisa Page – 1891 E Chase Avenue, El Cajon, California
 Spanish Immersion – 1530 Jamacha Road, Suite W, El Cajon, California
 Jamacha Liquor Market – 1530 Jamacha Road, Suite O, El Cajon, California
 El Cajon Center Inc – 1530 Jamacha Road, El Cajon, California

104-A

The Proposed Project Must Be Based Upon CEQA And Fact

This is not a determination to be based upon whether the applicant, Literacy First Charter Schools, Inc. is like or disliked or operates good schools. This decision must be made on whether this project has complied with the environmentally important requirements of CEQA in providing the County and the public with the required level of accurate and current information upon which to base a decision. We do not believe that the Applicant and the County have sufficiently identified, analyzed, investigated, and reported the environmental impacts of the proposed project. Normally a project such as this would be required to perform an environmental impact report (“EIR”); however, in its place, is a Mitigated Negative Declaration (“MND”), which does not adequately address the myriad of impacts the proposed project will have on the Rancho San Diego/El Cajon community – or the children and family who attend Liberty Charter High School. Without having full, accurate and current information that could only be learned through a proper EIR, no lawful decision can be made on the proposed project.

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104-B

Rancho San Diego/El Cajon and the area proposed for this build is a unique rural residential neighborhood. People moved here for its natural, rural, and quiet environment. The very lot that is the site for the proposed project offers such natural open space to the neighbors, community and those who visit it. Not only people who enjoy the quiet sanctity that the sweet wooden swing gently offers when one needs to ponder, but the hawks who have used that same tree as their nesting space for the last few years, the red fox who can be seen trotting through, and the bunnies and other wildlife who have made this lot their home. The proposed project seeks to eradicate that natural sanctuary and the wildlife who grace it with a gated, noisy, flood-lighted, traffic nuisance and cut it off from the neighborhood – to be used by family *who do not even live in the neighborhood!*

104-C

There is no benefit to the community in building another high school just 4000 feet from an underpopulated high school. The Applicant has stated it intends to exclude the local community from all access and that the high school and its grounds will be used only by the attending students who are predominantly not from the surrounding neighborhood. In addition to offering no benefit and in fact taking away the benefits of that current natural sanctuary, the risk to the neighborhood of the proposed project and its associated traffic is the highest risk of all: loss of life! The local community is still grieving the irreparably devastation of losing two young teens whose lives were ripped from them in traffic accidents on their way to/from their high schools – both within 5 miles of this proposed project. The traffic dangers of those schools pale in comparison to the planned project. No parent – or anyone who honestly cares about the lives of children would endanger such children the way this proposed project does. Not only the children and families proposed to attend this high school, but Chase Avenue is a main thoroughfare for students attending Steele Canyon, Valhalla, or Christian. All those students trying to get to their respect high schools by 8:00 a.m. every weekday morning – and more. Currently, every morning and afternoon eastbound traffic backs up from Jamacha all the way to Chase Lane, blocking driveways and causing drivers to do crazy things – like cross the center divide and the narrow walkways to get around the traffic congestions. What's worse, students run across the street through these traffic hazards every single morning – right at the proposed build site. The few actual proposed project traffic mitigations are unreasonable and do not account for all the students and parents who will cross the center divide above Fuerte to get past the lined up 400+ cars heading to Liberty in order to get to Valhalla, Steele Canyon, Hillsdale, and Christian.

104-D

In addition to the traffic dangers, there are bars, liquor stores, vape shops, and a hookah lounge (and their easily accessible trash receptacles) within in 100 feet of the proposed project, which all offer their own enticements and dangers to curious teens. The MND offers no analysis or mitigation or the necessary public resources that will be needed to policy and keep the students and these businesses and patrons safe.

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The Use Of A Mitigated Negative Declaration Is Very Limited And An Exception To The Rule

In evaluating the significant of environmental effects under CEQA, “the lead agency *shall* consider direct physical changes to the environment which may be caused by the project and reasonably foreseeable indirect physical changes in the environment which may be caused by the project.¹ The reasonably foreseeable impacts of this proposed project can only be truly understood and evaluated with a CEQA Initial Study that accurately identifies *all* affected environmental factors and the completion of an EIR, which must adequately consider all potential impacts and alternatives for the proposed project.

104-E

CEQA only excuses the preparation of an EIR and allows the preparation of a negative declaration in its place in two very narrow instances:

- (1) When the initial study identifies potentially significant effects on the environment:
 - a. But revisions in the project plans would avoid the effects or mitigate the effects to a point where clearly no significant effect on the environment would occur: and,
 - b. There is no substantial evidence that the project, as revised may have a significant effect on the environment.²
- (2) When the initial study shows there is no substantial evidence that the project may have a significant effect on the environment.³

Here, both prongs of this test are violated. The Initial Study acknowledges that the proposed project would result in significant impacts in the areas of Biological Resources, Utilities & Service Systems and Air Quality, but it claims those impacts are mitigable. We contend that these impacts are substantially understated and are not mitigated to a level of insignificance – as mandated by law.

104-F

More critically, there is substantial evidence that the Initial Study fails to identify all the environmental factors potentially affected by the proposed project – including Aesthetics, Noise, Recreation, Greenhouse Gas Emissions, Land Use & Planning, Transportation, and Public Services. Because the Initial Study fails to accurately identify these impacted environmental factors, all these environmental impacts are not mitigated in the offered MND. Additionally, the offered mitigation for the traffic is wholly insufficient – indeed, there is no viable mitigation so the MND suggests the Applicant pay a few into a state fund. How much money is a child’s life worth? Not only will not of those mitigation fees go to a cure to the traffic problems and dangers

¹ 14 Code of California Regulations (“CCR”) § 15064(d).

² Public Resources Code (“PRC”) § 21064.5; 14 CCR § 15064(f)(2); 14 CCR § 15070.

³ 14 CCR § 15070.

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that will unequivocally occur as a direct result of this proposed project – but none will go to the victims that will be injured or worse – by the traffic impacts. Courts have not hesitated to reject an MND and require an EIR when the requirements of CEQA have not been met.⁴ In determining whether an EIR is needed, there is “a low threshold for initial preparation of an EIR.”⁵ “Since the preparation of an EIR is the key to environmental protection under CEQA, accomplishment of the high objectives of that Act require the preparation of an EIR whenever it can be “fairly argued” on the basis of substantial evidence that the project may have significant environmental impact.”⁶

“A ‘significant effect on the environment’ means a substantial, or potentially substantial, adverse change in any of the physical conditions within the area affected by the project including land, air, water, minerals, flora, fauna, ambient noise, and objects of historic or aesthetic significance.”⁷

Substantial evidence as used in CEQA means facts, a reasonable assumption predicated upon facts, or expert opinion supported by facts.⁸ Substantial evidence means enough relevant information, and reasonable inferences from this information, that a fair argument can be made to support a conclusion that there may be a significant effect on the environment, even though other conclusions might be reached.⁹ Expert testimony or evidence is not required, statements by members of the public may constitute substantial evidence if based on relevant personal observations on non-technical subjects where special expertise is not required.¹⁰

Under the “fair argument” standard, the County is not to weigh competing evidence to determine who has a better argument concerning the likelihood or extent of a potential environmental impact, as the County’s job is not to resolve conflicts in the evidence but to determine only whether substantial evidence exists to support the fair argument being made.¹¹ Even if the County can point to substantial evidence supporting a determination that no significant impact

⁴ See, e.g. *Mejia v. City of Los Angeles* (2005) 130 Cal.App.4th 322 (rejecting MND based on evidence of possible impacts to wildlife and traffic hazards); *Chamberlin v. City of Palo Alto* (1986) 186 Cal.App.3d 181 (rejecting MND where neighborhood traffic control plan might increase vehicle pollution and noise, limit effectiveness of policy and fire protection, etc.); *Friends of “B” Street v. Hayward* (1980) 106 Cal.App.3d 988, 1003 (rejected MND where project would lead to vehicle emissions, business disruption, loss of wildlife habitat, increased traffic, noise, parking problems, and loss of neighborhood character of area.).

⁵ *Architectural Heritage Ass’n v. County of Monterey* (2004) 122 Cal.App.4th 1095, 1110.

⁶ *No Oil, Inc. v. City of Los Angeles* (1974) 13 Cal.3d 68, 75.

⁷ 14 CCR § 15382.

⁸ PRC § 2080(e).

⁹ 14 CCR § 15384(a).

¹⁰ *Citizens Ass’n for Sensible Dev. V. County of Inyo* (1985) 172 Cal.App.3d 151, 173 (owner of adjacent property may, based on personal observations, testify to existing traffic conditions).

¹¹ *Id.*

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will occur, a negative declaration will not be upheld if the record contains other substantial evidence to the contrary.¹²

The Supreme Court has stressed “the importance of preparing an EIR in cases ... in which the determination of a project’s environmental effect turns upon the resolution of controverted issues of fact and forms the subject of intense public concern.”¹³ There is no doubt, as seen as the Valle de Oro Community Planning Group meeting held on January 7, 2020, that the residents of Rancho San Diego/El Cajon expressed overwhelming and intense concern about the many impacts of this proposed projects – especially traffic—give real life observations of the already dangerous and problematic traffic along Chase Avenue and Jamacha Avenue – including recounts of many accidents within 100 feet of the proposed project site, involving complete shutdown of Chase Avenue – the only thoroughfare of this community to Interstate 8 – for hours with emergency vehicles and dozens of accidents at the intersection of Chase and Jamacha in the last year alone. For these reasons, as well as many others, there is a confirmed statutory preference for resolving doubts in favor of the preparation of an EIR.¹⁴

104-F

There Is A Fair Argument, Based On Substantial Evidence, That The Following Environmental Factors Will Be Affected By The Proposed Project That Have Not Been Mitigated Or Have Not Been Mitigated To A Level Of Insignificance:

Traffic	Described above and at the Valle de Oro Community Planning Group meeting
Parking	Inadequate parking on sight and no overflow parking available
Lighting	The football field land parking lighting will substantially impact surrounding neighbors
Noise	The noise from sporting events, band practice, and general teen behavior will substantially impact surrounding neighbors
Wildlife	The hawks, foxes, and other wildlife will be displaced with no mitigation for where.
Public Resources	The neighborhood costs will be the increased by the resulting increase in use of police to control traffic, respond to teens loitering and illegal pursuance of controlled substances sold by surrounding establishments, and increased emergencies requiring response by emergency personnel.
Greenhouse Gas Emissions	Insufficient analysis and mitigation for the impact of more than 800 increased idling vehicles every day. ¹⁵

104-G

¹² *Friends of “B” Street v. Hayward* (1980) 106 Cal.App.3d 988, 1002; *Architectural Heritage Ass’n v. County of Monterey* (2004) 122 Cal.App.4th 1095, 1110.

¹³ *Architectural Heritage Ass’n v. County of Monterey* (2004) 122 Cal.App.4th 1095, 11122.

¹⁴ *Id.*

¹⁵ 14 CCR 15064.4; 14 CCR 15126.2

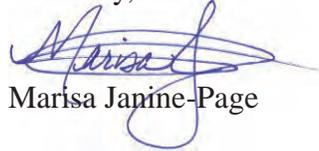
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Other: There are two bars and one liquor store within 600 feet of the proposed project. The California Department of Alcoholic Beverage Control refuses to issue retail licenses for premises within 600 feet of schools.

Conclusion

Without the above referenced issues being properly addressed and studied in an EIR, an informed decision on the proposed project cannot legally be made. Therefore, we respectfully request the County reject the MND and require than EIR be prepared.

Cordially,



Marisa Janine-Page

1 - 345

Subject: Charter school

From: Helen Dagnino <treadag@cox.net>

Date: 1/17/2020, 3:41 PM

To: alyssa@alyssaburley.com

105-A

I oppose a new charter school on Chase and Jamacha. Please find another location. I live on Hidden Mesa Road. Building a school on this location would add too much traffic to the area. Please think wisely.

Sent from my iPhone

Subject: Opposed to Liberty Charter School

From: Leslie Didier-Paquin <ldidier_paquin@centricsoftware.com>

Date: 1/17/2020, 10:38 AM

To: "Alyssa@alyssaburley.com" <Alyssa@alyssaburley.com>

To whom it may concern,

The Didier-Paquin household is adamantly opposed to the proposed location for the Liberty Charter School. We live on Penasco Rd. and will be negatively impacted dramatically by the increased traffic. The intersection is already extremely congested and dangerous without possible school traffic.

Leslie Didier-Paquin | Customer Support Specialist



655 Campbell Technology Parkway

Suite 200

Campbell, CA 95008 | e: ldidier_paquin@centricsoftware.com

p: 408.574.7802 ext. 563

Cordova, Lorena

From: alyssa <alyssa@alyssaburley.com>
Sent: Friday, January 17, 2020 4:00 PM
To: Russell, Denise; Smith, Ashley
Subject: FW: Opposition to the Proposed site of Liberty Charter High School

Hi Denise and Ashley,

I just received another letter from the community regarding the Liberty Charter High School. See below.

Alyssa Burley
Chairperson
Valle de Oro Community Planning Group

----- Original message -----

From: Steve Dillingham <depmar@aol.com>
Date: 1/17/20 3:58 PM (GMT-08:00)
To: alyssa@alyssaburley.com
Subject: Opposition to the Proposed site of Liberty Charter High School

Dear Ms. Burley,

107-A I'm writing you today to record my opposition to the negative declaration presented to the the committee by Liberty Charter High School. Further, I am opposed to the building of this charter school on the Chase avenue/ Jamacha road intersection. This school will put an undue burden on the current residents while serving mostly students that do not live in the immediate area. Valhalla High School is 1/2 mile away and Steele Canyon Charter is approximately 3 miles from the proposed site, not to mention Granite Hills High. The residents in opposition would like more time to consider the plans and chart a course of action. Most residents in the area had no idea Liberty Charter was planning on building a
107-B high school. The community was not duly informed about your committee hearing until the night before! Thanks to social media, word has gotten out and the negative opinions on the site of this school are overwhelming. Please consider making any further meetings concerning Liberty well known throughout the community you serve.

Thank you,

Steven Dillingham
1900 Avocado Ranch Rd
El Cajon, CA 92019

Sent from my iPad

Cordova, Lorena

From: RICHARD FOSSETT <richardfossett@gmail.com>
Sent: Friday, January 17, 2020 2:16 PM
To: Russell, Denise
Subject: Comments on Proposed Liberty High School (PDS2015-MUP-15-027)(PDS2015-ER-15-14-010)

Dear Denise,

I just wanted to comment on the Liberty Charter High School proposal. I live one block away from this location and the traffic is already very bad - especially in the afternoons.

The only access to the proposed driveway would be from Chase Avenue, on the eastbound side, and I can't imagine how much worse it will be with an additional 450 students and 33 faculty and staff driving to this location every day.

Traffic already backs up in a long line up the hill on Chase Avenue (where the proposed driveway would be located) as people wait their turn at the traffic signal located at the junction of Chase Avenue and Jamacha Road.

Thank you for taking my comments into account concerning this project.

Sincerely,

Richard Fossett
1669 Hollow Place
El Cajon, CA, 92019
(619) 316-5235

Cordova, Lorena

From: Alyssa Burley <alyssa@alyssaburley.com>
Sent: Friday, January 17, 2020 4:24 PM
To: Russell, Denise; Smith, Ashley
Subject: Fwd: Opposition to proposed building of Liberty Charter School at Chase & Jamacha

Hi Denise and Ashley,

The letters about the Liberty Charter High School keep coming. Here is another one. See below.

Alyssa Burley
Chairperson
Valle De Oro Community Planning Group
(619) 851-5961
alyssa@alyssaburley.com
www.alyssaburley.com

----- Forwarded Message -----

Subject:Opposition to proposed building of Liberty Charter School at Chase & Jamacha
Date:Fri, 17 Jan 2020 16:11:38 -0800
From:Stacie Hoover <audreysmom@aol.com>
To:Alyssa@alyssaburley.com

Dear Alyssa:

I am writing to voice my concern regarding the Notice to Adopt a Mitigated Negative Declaration regarding the building of a Liberty Charter High School near the intersection of Chase & Jamacha in El Cajon. I attended the meeting recently held at the Otay Water District office and spoke on my concerns regarding the added pollution and traffic this will burden the neighborhood with. I'm sure you will recall that there were nearly a hundred people there to voice their opposition to this project as well.

109-A Twenty years ago I purchased my home here in this pleasant quiet community, less that a mile east from the proposed site. For every one of those years I have had to travel west on Chase to get to work as it is the only road that leads to highway 8. I also travel that road home. The majority of the homeowners in our community must use Chase to travel to and from work. Adding 500 cars to that intersection 2-3 times a day is a thought that I just cannot bear. I also cringing at the thought of those kids getting hit by a car while running to 7/11.

I'd also like to say that I'm not opposed to kids having a school, I'm just opposed to the location and the pollution and problems it will bring to the community.

What do we need to do to protect our community and stop this project?

Thank you, and the other members of the planning committee, very much for your help.

Best regards, Stacie Hoover
(619)316-5335
Sent from my iPhone

Subject: Liberty Charter School proposal

From: terry <terrymenchaca@gmail.com>

Date: 1/17/2020, 7:36 AM

To: "Alyssa@alyssaburley.com" <Alyssa@alyssaburley.com>

110-A I am writing to voice my concern and objection regarding the Liberty Charter School building a high school at Chase Rd and Jamacha Blvd in RSD. I am a resident of RSD and travel that intersection daily, as I live off Vista Grande Rd and travel Chase/Hillsdale Rd to come and go. The traffic already backs up on Chase and the congestion is getting worse. Additionally, I am concerned for the children's safety in that area. I can easily see a child getting hit by a car as the intersection is already dangerous, without additional traffic and pedestrians.

I believe this is the wrong place for a high school and should be voted against.

Thank you

Terry Oneill

Cordova, Lorena

From: Ardawanna Starks <astar221@yahoo.com>
Sent: Friday, January 17, 2020 10:14 PM
To: Russell, Denise
Subject: Liberty Charter High School Location

Hi,

I would prefer Liberty Charter High School location to remain at the same place or in the same area. I started searching for a high school for my son when he was still in elementary school. At that time, I was undecided whether to enroll him in another charter or have his education continue at a public school. After seeing him struggle during his last two years in elementary in a public school; I decided to enroll him in another charter. When I start researching any school, I look at test scores, parent or staff reviews, what the school offers, the neighborhood the school is in (if it is safe)/location, the size of the school, parking, etc. (not specifically in that order). Then I further my research by contacting the school with a list of questions and I write down their answers.

111-A

In middle school, the campus was a small comfortable size and around 12- 15 minutes away from my parents house and mine (depending on traffic). However, I decided for high school to find a charter school in Lemon Grove; which is around 10-12 minutes away from my parents house and mine (depending on traffic). After all the bullying, fights, student deaths, school shootings and bomb threats over the years; I find it of the utmost importance for my son to be close to home while at school.

If the school is relocated to El Cajon it will take double to triple the amount of time for me to get him dropped off and picked up depending on the flow of traffic. It will take even longer for my parents when they help me. I am basing the commute time on the interstate they live closer to in comparison to the one I live closer to.

I understand the public bus or trolley is an option but not for my child. I have viewed videos and read too many violent and sad stories regarding peoples action on the buses and trolleys. I work very hard to maintain having an automobile to make sure my son never has to ride public transportation. Nor would I ever have him ride in an Uber, Lyft, etc.; because that is extremely dangerous based on the news reports and stories I have read. Therefore, my only option is to have my immediate family members or myself transport him.

I personally found the process of getting him into Liberty Charter High School a little stressful because of the lottery. However, I was extremely happy when his name was called during the lottery process and my family was as well when I contacted them. My son settled into the high school great. Before the lottery he was concerned about once again being at a new school and not knowing anyone. Then he learned one of the students from the middle school had enrolled and would be attending as well. Furthermore, I really like the school, the staff and the location. I am looking forward to my son continuing his high school education at Liberty Charter High School and graduating from it but at the same location or in Lemon Grove.

Respectfully,

Ms. Starks

[Sent from Yahoo Mail on Android](#)

Cordova, Lorena

From: lajan talkou <lataalkou@gmail.com>
Sent: Saturday, January 18, 2020 11:34 AM
To: Russell, Denise
Subject: Re: LCHS El Cajon High School

On Sat, Jan 18, 2020 at 11:30 AM lajan talkou <lataalkou@gmail.com> wrote:

Dear Denise,

112-A LFCS has provided our children with experiences in academic levels and character. They have provided a learning experience the most schools don't often give. They are caring and kind towards all students and treat them more than just some kids. The school cares more than just who has the highest grades and cares for the character inside. The school cares more than just who has the highest grades and cares for the character inside. Kids are provided in education K through eight in El Cajon, but when they move onto high school it is very far away. Neighbors have said that the commotion of the school can be very disturbing but LCHS needs another high school to help provide for the many students to go there. They are a respectable school and don't cause much trouble. Parents have had many troubles because of the Long way to LCHS. The neighbors have also said that it could cause a lot of traffic around their area of living. However, LCHS is a smaller high school compared to schools like Valhalla. Also, traffic only wouldn't range for too long being that they are a smaller school. Please help consider our school for the benefit of the children.

Very best,
Lajan Talkou
lataalkou@gmail.com

Cordova, Lorena

From: Lenny Coppenrath <lennyc@cox.net>
Sent: Sunday, January 19, 2020 10:30 AM
To: Russell, Denise
Subject: Proposed site for Liberty Charter HS

Hi there,

113-
A

I am a local resident of the area proposed for the Liberty Charter HS and a board member of the Hidden Mesa Estates HOA. Traffic is the main concern we have for this proposal. Before, and after school hours this intersection at Chase and Jamacha, and the entire length of Chase from west of Vahalla is congested. This is due to the comings and goings of parents and students commuting to the various schools in the area. The addition of another school will only make that worse.

113-
B

I understand the proposal includes a widening of Chase in an attempt to accommodate this additional traffic, but we as a community see this as an inadequate solution since we truly understand the increasing traffic problems in this area, not to mention the dangerous high speeds drivers on both Chase and Jamacha at all times of the day.

For the record:
Leonard J Coppenrath
1335 Hidden Mt Dr
El Cajon Ca 92019
619 916 7019

Cordova, Lorena

From: Danielle Nunez <dmnunez81@gmail.com>
Sent: Sunday, January 19, 2020 8:35 AM
To: Russell, Denise
Subject: New LFCS High School

Hello-

It's imperative that we utilize this space in our neighborhood for the new school.

114- LFCS is a wonderful place to educate kids, and mine have benefited from this program for the last 8 years.
A

I know traffic is a concern, but the benefits to the community outweigh the costs of that. LFCS is a good neighbor and makes a point of educating our families about proper procedures and processes concerning our facilities.

It will be a great addition to the community and it will give parents more choice in their children's education.

Thank you.

Danielle Nuñez
LFCS parent

Cordova, Lorena

From: Chance Agnew <litechance@gmail.com>
Sent: Monday, January 20, 2020 6:56 PM
To: Russell, Denise
Cc: Jacob, Dianne
Subject: Liberty Charter High School Concerns

Good evening Denise,

I live on Fuerte which is .7 miles from the proposed development site of Liberty Charter High School. My address is 12108 Fuerte Dr. My teen and child attend Valhalla HS and Fuerte Elementary. My biggest concern with the location of the school is traffic at that intersection, Chase, and Fuerte Drive which is a light emitter road. Fuerte Drive is currently used as a short cut or thruway between Avocado and Chase. Over the last three years, it seems the amount of traffic on Fuerte has doubled. Fuerte Elementary already brings families from all over into this neighborhood under ESSA and there is a new development of 36-70 houses going in at the Chicken Ranch. The intersection of Fuerte and Chase will be a nightmare if the school locates at it's proposed location. I'm repeatedly told there is a shortage of CHP resources that have jurisdiction over Fuerte Dr. Let's buy this land back and plan for a CHP/Sheriff Branch office to help protect our community. We have plenty of schools in our immediate area.

Very Respectfully,

Chance Agnew
619-302-7173

Sent from my iPhone

115-
A

MEETING MINUTES: Tuesday, January 7, 2020

Location

Otay Water District Headquarters Training Room, Lower Level
2554 Sweetwater Springs Blvd
Spring Valley, CA 91978

Meeting called to Order: 7:00 PM, Alyssa Burley, Chair

A. Roll Call

Present: 11 (Baillargeon, Bickley, Burley, Feathers, Hermann, Herron, Johnston, Kossman, Myers Tinsley-Becker, Zimmerman)

Absent: 3 (Gerhart, Schuppert, Steele)

Vacant: 1

B. Approval of Previous Meeting Minutes

- **Minutes to Approve:** December 3, 2019 VDO CPG Meeting
- **Motion to approve:** Burley
- **Second:** Not recorded
- **Yea: 9** (Bickley, Burley, Feathers, Hermann, Herron, Johnston, Kossman, Myers, Tinsley-Becker)
- **Nay:** (None)
- **Abstain: 2** (Baillargeon, Zimmerman)
- **Absent: 3** (Gerhart, Schuppert, Steele)
- **Vacancy: 1**
- The CPG adopted the minutes from the December 3, 2019 meeting

C. Public Communication

1. Lori Myers - Wanted to make a correction to the information on the flyer handed out at the GMIA walk.
2. Gary Westergren - Applying for the vacancy on the board. Serves on the Casa de Oro Alliance. Experience in planning.
3. Art Patoff - Concerned about the widening of Damon Lane. Deferred to the agenda item E3 for further discussion.
4. Stacy Hoover: Where are the public documents relating to the projects that Myers mentioned? Can be found on PDS website.
5. Bryan Sesko: Member of Lakeside Planning Group, Candidate for 2nd Supervisor District. Wants to have Board of Supervisors come out to community to vote in front of the public, not at 9 am in downtown.

D. Information Items

None

E. Action Items

Item E1:

- **Start Time: 7:13pm**
- **Item issue Summary:** Liberty Charter High School - CEQA Public Review (MUP-15-027) E Chase Avenue west of Jamacha Road APN: 498-330-39-00

- **CPG Presenter:** Bickley
- **Applicant Speakers / Representatives:** Mark Linemen, Liberty Charter High School
- **County Representative:** Denise Russell
- **Abstentions or Disqualifications:** (None)
- **Public Comment:**

Giberson: Kids go to Fuerte. Feels like turning onto Fuerte is similar to living in Los Angeles. Can take up to 15 minutes to turn on to Fuerte/ Chase.

Milley: Resident up above Chase/ Fuerte of 22 years. 3-4 major accidents per year. 7:30 AM traffic can cause 15 minute delays.

Espindle: Resident of Granite Hills. Parent of LFCS students. Wife drives from El Cajon and adds to the congestion already.

Morey: Shadow Knolls resident. Rush hours can back up the street. Doesn't believe that the traffic report is accurate. Supports the school but believes the location is inappropriate. Liquor stores and bars too close. Traffic is too bad already.

Page: Practicing in CEQA law. Represented the County, cities, and proponents. In reference to public code 210645. Negative Declaration is allowed in very specific cases. There is a substantial impact on the environment. The lead agency shall prepare gas emissions plan. Extra 400-500 cars emitting. Streets unsafe and impassable at school time. Traffic reports are timed out. Teenagers will jay-walk to get to 7-Eleven. Valhalla is already noisy.

Petrarca: Resident of neighborhood. Was not notified for this project. Wants better communication.

Cunningham: Resident since 1982. Seen neighborhood grow. More lanes and lights. There are problems with the project but it is better than a strip mall. Traffic isn't as bad as Valhalla or Steele. Whole school is less than graduating class.

Kister: Not enough room for school. Traffic is bad.

Bendall: Driveway opens onto Chase. Echoes traffic concerns. Accidents, bloodshed. Concerned that students will run across Chase. Believe traffic report should be updated. Concerned about traffic drop off on Brayton. Loves high schools. students, administrators. Believes this location is particularly bad.

Steiger: Member of GUHSD finance oversight committee, speaks on private accord. Other schools in the district have student fatalities because of inadequate planning.

Hoover: Moved into neighborhood to be in a quiet place. Doesn't hold anything against school but thinks light pollution and traffic is a major concern.

Gregoire: Moved in 11 years ago. Drunk driver crashed into house. Cannot walk on Chase with daughter. Too many emergency vehicles/ accidents and crashing.

Greg: East Chase resident and echoes traffic concerns. Traffic is impossible. State was supposed to build SR-54. Loves idea of charter schools but this location is bad.

Kriebel: Doesn't believe community is against school but bad location. Left turn into school not addressed. Also doesn't understand where parking will take place for stadium. Students don't walk to school anymore.

Christie: Traffic studies outdated. Fuerte Elementary has increased by 10%. Not good stores directly next to store. No parking if lot fills up.

Johnston: Only found out about the project 3 days ago. Students will go to the 7-Eleven.

Garzon: Owns a shop that sells alcohol. Will they be grandfathered in regardless of the school?
- **CPG Discussion Points:**

Kossmann: Over a dozen homes on Brayton.

Burley: What percentage of students live within schools radius. Assumptions living in El Cajon, but going to K-12. How many students allowed to drive to school?

Lineman: LFCHS examined their existing lot and included those numbers in the traffic report.
 Parking is adequate and up to code.

Zimmerman: What grades are the students?

Lineman: K-12

Myers: Applicants have been respectful, but the concerns raised do not have to do with charter schools, but the location.

Herron: If traffic can be mitigated, we can support it. If not, then we might need to reconsider.

Hermann: If traffic problem can be mitigated, would be in support.

Baillargeon: Is this within traffic code?

Traffic engineer: Deficiencies in the traffic code pave the way for future improvements.

Johnston: Is this a benefit to the community? Well... no. Students are from out of neighborhood.

Valhalla is close and not at full capacity. Therefore, the land can be put to better use.

Bickley: See attached.

Tinsley-Becker: Traffic analysis is flawed. Problems presented have never been considered. We don't know where new students will come from. There are 5 major projects that will drastically affect the community. The County traffic planning is faulty and the Rancho San Diego neighborhood is under siege. Environmental Impact Review is necessary and Mitigated Negative Declaration is insufficient.

Feathers: EIR is necessary. If traffic can't be mitigated, the project should not go in.

- **Motion: Motion to object to the Mitigated Negative Declaration and ask for a full Environmental Impact Review based upon concerns brought up tonight (community character and benefit, safety, benefits, aesthetics, traffic) by the public, Grossmont Union High School District, and our board. Bickley.**
- **Second: Myers**
- **Yea: 11** (Baillargeon, Bickley, Burley, Feathers, Hermann, Herron, Johnston, Kossman, Myers Tinsley-Becker, Zimmerman)
- **Nay: 0**
- **Absent: 3**
- **Vacancy: 1**
- **Abstain: 0**

Item E2:

- **Start Time: 9:30PM**
- **Item issue Summary: PDS2019-STP-19-032 (Site "D" Designator) for a proposed one and two story single family dwellings on Fuerte drive at the former 'Chicken Ranch' property 11739 Fuerte Drive. APN: 498-153-01-00**
- **CPG Presenter:** Burley
- **Applicant Speakers / Representatives:** Sarah Morrell
- **County Representative:** Sean Oberbauer
- **Abstentions or Disqualifications:** Myers
- **Public Comment:**
 Lynn Patoff: Lives on Damon Lane. Thought she lived across from a meadow. There will be significant visual impact.
 Oberbauer: Permit is for wall and architectural.

Art Patoff: Valle de Oro rejected the project 9 years ago because of community character. Map is different that what Board of Supervisors approved. Traffic as discussed tonight is incredibly relevant to this project.

Van Antwerp: Tract homes are inconsistent with Fuerte Farms. Traffic on Fuerte is bad, has to back into driveway to be able to pull out effectively.

Parrott: Lives 10 feet from project and was not notified. Grew up next door. Wells overflow. Feels that applicant broke promises.

Killiam: We are over developing and exhausting our resources.

Patoff (2): Firewall would cause trap.

- **CPG Discussion Points:**

Herron: In terms of Mt. Helix, this is out of character, but not as for VDO in its entirety.

Baillargeon: Adjacent community character clusters transition into other areas.

Herron: It might be time for a change.

Baillargeon: Uses community plan and walks the vicinity. That helps to understand to understand the community.

Kossmann: Rancho San Diego is Rancho San Diego and Mt. Helix is Mt. Helix, this neighborhood is not RSD.

Hermann: There is a lot of work that needs to be done on roads before development can persist.

Johnston: Siding? Landscaping? Creek?

Sized for the 100 year storm.

Baillargeon: Meets criteria, but still not individualized.

Bickley: Homeowners Association?

Yes.

Bickley: Solar? Tiles?

Can be considered.

Tinsley-Becker: Disagrees that it is like RSD. It is not. There are some important considerations for character.

Landscaping will be done by homeowner, certain parts maintained by HOA.

Feathers: Leed homes?

No. Meets title 24 standards.

- **Motion to approve. Burley.**
- **Second: Tinsley-Becker.**
- **Yea: 8** (Baillargeon, Bickley, Burley, Hermann, Herron, Johnston, Myers Tinsley-Becker)
- **Nay: 2** (Feathers, Kossmann)
- **Absent: 4** (Gerhart, Schuppert, Steele, Zimmerman)
- **Vacancy: 1**
- **Abstain: 0**
- **Motion doesn't pass because quorum wasn't reached.**

Item E3:

- **Start Time: Not recorded**
- **Item issue Summary:** PDS2019-VAC-19-003 (Discretionary Permit) for the exchange of open space through dedication and vacation. Fuerte Heights lane & Jamacha Road. APN: 498-371-29-00
- **CPG Presenter:** Herron
- **Applicant Speakers / Representatives:** Lawrence Cole
- **County Representative:** (None)

- **Abstentions or Disqualifications:** (None)
- **Public Comment:** (None)
- **CPG Discussion Points:** (None)

- **Motion: Recommend Approval: Burley**
- **Second: Not recorded.**
- **Yea: 10** (Baillargeon, Bickley, Burley, Feathers, Hermann, Herron, Johnston, Kossman, Myers Tinsley-Becker)
- **Nay:** (None)
- **Absent: 4**
- **Vacancy: 1**
- **Abstain: 0**

Item E4: Skipped.

Item E5:

- **Start Time: Not recorded**
- **Item issue Summary:** Request for Site Plan Waiver/ Sign Approval. Proposed 7- Eleven signage at 9716 Campo Road. APN: 5001922900
- **CPG Presenter:** Tinsley-Becker
- **Applicant Speakers / Representatives:** Tim Searman.
- **County Representative:** (None)
- **Abstentions or Disqualifications:** (None)
- **Public Comment:** (None)
- **CPG Discussion Points:**
Tinsley-Becker: We have approved exemption before. Only basis to approve.
It conforms to signage requirements.

- **Motion: Recommend Approval: Tinsley Becker**
- **Second: Johnston**
- **Yea: 10** (Baillargeon, Bickley, Burley, Feathers, Hermann, Herron, Johnston, Kossman, Myers Tinsley-Becker)
- **Nay:** (None)
- **Absent: 4**
- **Vacancy: 1**
- **Abstain: 0**

F. GROUP BUSINESS:

Start Time: 10:48 PM

F1: Administrative - Project assignments and subcommittees.

F3: Subcommittee Update – Short-Term Rentals Subcommittee Update; Kossman
(No update)

F4: Subcommittee Update – Skyline Church Project Subcommittee Update; Feathers

Board of Supervisors Hearing January 29th.

F5: Subcommittee Update – Estrella Park Project Subcommittee Update; Myers- 3 storm drains filtrate some water then look at ADA.

F6: Subcommittee Update – Casa de Oro Business Corridor; Herron, Tinsley-Becker
Hoping to meet soon.

F8: Subcommittee Update – Literacy First Charter High School; Bickley

F9: VDO CPG Liaison Update – Fire Safe Council; Hermann/ Feathers
CWPP is underway.

F10: Subcommittee Update - Parklands Dedication Ordinance (PLDO); Feathers/ Myers
Lonnie Brewer Park conceptualized.

F11: Subcommittee Update - Cottonwood Sand Mine Project; Myers
(No update)

G. ADJOURNMENT: 11:00 p.m. Alyssa Burley, Chair

Submitted by: Kyle Hermann

**VOLUNTARY SPEAKER CARD FOR PUBLIC COMMENT
County of San Diego**

Valle de Oro Community Planning Group

Please complete and submit this card to the Group Secretary if you wish to speak on any agenda item. Please keep your comments brief while still communicating your concerns. As a general guideline, it is suggested that you please limit your comments to two (2) minutes if you are speaking as an individual, and five (5) minutes if you are speaking for an organized group. Please provide the following information for the purposes of recognizing your comments.

Name: Jay Steise

Agenda Item No.: C

Comment Summary: Introduction as
VBOCG applicant

The Group Chair will recognize speakers in the order in which the speaker cards were submitted. All comments shall be directed to the Chair. The Chair reserves the right to close public comment if comments have become repetitive or non-productive. Speakers may cede their time to another speaking as part of an organized presentation / response to an agenda item.

Going First

17

**VOLUNTARY SPEAKER CARD FOR PUBLIC COMMENT
County of San Diego**

Valle de Oro Community Planning Group

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Name: Ann Morey

Agenda Item No.: Liberty Charter Sec.

Comment Summary: _____

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County of San Diego**

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Name: Dr. Tim Glover

Agenda Item No.: 6-7

Comment Summary: Liberty Charter

The Group Chair will recognize speakers in the order in which the speaker cards were submitted. All comments shall be directed to the Chair. The Chair reserves the right to close public comment if comments have become repetitive or non-productive. Speakers may cede their time to another speaking as part of an organized presentation / response to an agenda item.

(16)

VOLUNTARY SPEAKER CARD FOR PUBLIC COMMENT
County of San Diego

Valle de Oro Community Planning Group

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Name: MARISA JANINE-PAGIE

Agenda Item No.: E-7 (LIBERTY HIGH SCHOOL)

Comment Summary: Oppose on grounds of
Deficient Traffic Study; Greenhouse
Gas EMISSIONS & LOSS OF LIFE;
REQUIRES AN EIR

The Group Chair will recognize speakers in the order in which the speaker cards were submitted. All comments shall be directed to the Chair. The Chair reserves the right to close public comment if comments have become repetitive or non-productive. Speakers may cede their time to another speaking as part of an organized presentation / response to an agenda item.

①

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County of San Diego

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Name: Paula Miley

Agenda Item No.: E7

Comment Summary: oppose

The Group Chair will recognize speakers in the order in which the speaker cards were submitted. All comments shall be directed to the Chair. The Chair reserves the right to close public comment if comments have become repetitive or non-productive. Speakers may cede their time to another speaking as part of an organized presentation / response to an agenda item.

②

**VOLUNTARY SPEAKER CARD FOR PUBLIC COMMENT
County of San Diego**

Valle de Oro Community Planning Group

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Name: Christie Haugh

Agenda Item No.: ~~3~~^{E7} - Liberty High

Comment Summary: opposed to
Liberty location

The Group Chair will recognize speakers in the order in which the speaker cards were submitted. All comments shall be directed to the Chair. The Chair reserves the right to close public comment if comments have become repetitive or non-productive. Speakers may cede their time to another speaking as part of an organized presentation / response to an agenda item.

3

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County of San Diego**

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Name: Michelle Kister

Agenda Item No.: E7

Comment Summary: OPPOSED TO SCHOOL SITE

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4

VOLUNTARY SPEAKER CARD FOR PUBLIC COMMENT
County of San Diego

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Name: Eileen Bottenberg

Agenda Item No.: E 7

Comment Summary: Support Liberty.

The Group Chair will recognize speakers in the order in which the speaker cards were submitted. All comments shall be directed to the Chair. The Chair reserves the right to close public comment if comments have become repetitive or non-productive. Speakers may cede their time to another speaking as part of an organized presentation / response to an agenda item.

5

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County of San Diego**

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Name: Joshua Garber

Agenda Item No.: E7

Comment Summary: IN FAVOR
OF LIBERTY CHARTER

The Group Chair will recognize speakers in the order in which the speaker cards were submitted. All comments shall be directed to the Chair. The Chair reserves the right to close public comment if comments have become repetitive or non-productive. Speakers may cede their time to another speaking as part of an organized presentation / response to an agenda item.

②

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County of San Diego**

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Name: LEONARD COPPENHAGEN

Agenda Item No.: _____

Comment Summary: _____

Not in favor of
Liberty Charter HS Project

The Group Chair will recognize speakers in the order in which the speaker cards were submitted. All comments shall be directed to the Chair. The Chair reserves the right to close public comment if comments have become repetitive or non-productive. Speakers may cede their time to another speaking as part of an organized presentation / response to an agenda item.

7

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County of San Diego**

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Name: Scott Patterson

Agenda Item No.: E-7

Comment Summary: Liberty Charter

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9

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County of San Diego**

Valle de Oro Community Planning Group

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Name: KATY WRIGHT

Agenda Item No.: E7

Comment Summary: Concerns RE

Liberty Charter

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(10)

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County of San Diego

Valle de Oro Community Planning Group

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Name: Steve Dillingham

Agenda Item No.: E7

Comment Summary: opposition to
the Negative Declaration by
Liberty Charter High School

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(11)

VOLUNTARY SPEAKER CARD FOR PUBLIC COMMENT
County of San Diego**Valle de Oro Community Planning Group**

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Name: MARK FEALLOCKAgenda Item No.: E-7Comment Summary: this project, Liberty High School, is unfeasible and unsafe.

The Group Chair will recognize speakers in the order in which the speaker cards were submitted. All comments shall be directed to the Chair. The Chair reserves the right to close public comment if comments have become repetitive or non-productive. Speakers may cede their time to another speaking as part of an organized presentation / response to an agenda item.

14

VOLUNTARY SPEAKER CARD FOR PUBLIC COMMENT
County of San Diego

Valle de Oro Community Planning Group

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Name: Susan Britt

Agenda Item No.: E 7

Comment Summary: _____

The Group Chair will recognize speakers in the order in which the speaker cards were submitted. All comments shall be directed to the Chair. The Chair reserves the right to close public comment if comments have become repetitive or non-productive. Speakers may cede their time to another speaking as part of an organized presentation / response to an agenda item.

13

VOLUNTARY SPEAKER CARD FOR PUBLIC COMMENT
County of San Diego

Valle de Oro Community Planning Group

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Name: Angelo Nissan

Agenda Item No.: e7

Comment Summary: parking, noise, traffic,

The Group Chair will recognize speakers in the order in which the speaker cards were submitted. All comments shall be directed to the Chair. The Chair reserves the right to close public comment if comments have become repetitive or non-productive. Speakers may cede their time to another speaking as part of an organized presentation / response to an agenda item.

15

**VOLUNTARY SPEAKER CARD FOR PUBLIC COMMENT
County of San Diego**

Valle de Oro Community Planning Group

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Name: Eileen Tierney

Agenda Item No.: _____

Comment Summary: Charter School

I was against this when I was
on the Planning Group and I still am

The Group Chair will recognize speakers in the order in which the speaker cards were submitted. All comments shall be directed to the Chair. The Chair reserves the right to close public comment if comments have become repetitive or non-productive. Speakers may cede their time to another speaking as part of an organized presentation / response to an agenda item.

12

VOLUNTARY SPEAKER CARD FOR PUBLIC COMMENT
County of San Diego

Valle de Oro Community Planning Group

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Name: RICH STOCMAKER

Agenda Item No.: E 7

Comment Summary: OPPOSITION - LIBERTY CHARTER

COMPREHENSIVE TRAFFIC STUDY OF
CHASE AVE

The Group Chair will recognize speakers in the order in which the speaker cards were submitted. All comments shall be directed to the Chair. The Chair reserves the right to close public comment if comments have become repetitive or non-productive. Speakers may cede their time to another speaking as part of an organized presentation / response to an agenda item.



County of San Diego, Planning & Development Services
**COMMUNITY PLANNING OR SPONSOR
 GROUP PROJECT REVIEW**
 ZONING DIVISION

Record ID(s): PDS2015-MUP-15-027 PDS2015-ER-15-14010

Project Name: Liberty Charter High School

Project Manager: Denise Russell

Project Manager's Phone: (858) 694-2019

Scope of Review:

Board Policy I-1 states; "groups may advise the appropriate boards and commissions on discretionary projects as well as on planning and land use matters important to the community." Planning & Development Services (PDS) has received an application for the project referenced above. PDS requests your Group evaluate and provide comment on the project in the following areas:

- The completeness and adequacy of the Project Description
- Compatibility of the project design with the character of the local community
- Consistency of the proposal with the Community Plan and applicable zoning regulations
- Specific concerns regarding the environmental effects of the project (e.g., traffic congestion, loss of biological resources, noise, water quality, depletion of groundwater resources)

Initial Review and Comment:

Shortly after an application submittal, a copy of the application materials will be forwarded to the Chair of the applicable Planning or Sponsor Group. The project should be scheduled for initial review and comment at the next Group meeting. The Group should provide comments on planning issues or informational needs to the PDS Project Manager.

Planning Group review and advisory vote:

- A. **Projects that do not require public review of a CEQA document:** The Group will be notified of the proposed hearing date by the PDS Project Manager. The project should be scheduled for review and advisory vote at the *next Group meeting*.
- B. **Projects that require public review of a CEQA document:** The Chair of the Planning Group will be notified when an environmental document has been released for public review. The final review of the project by the Group, and any advisory vote taken, should occur *during the public review period*.

As part of its advisory role, the Group should provide comments on both the adequacy of any environmental document that is circulated and the planning issues associated with the proposed project. The comments provided by the Group will be forwarded to the decision-making body and considered by PDS in formulating its recommendation.

Notification of scheduled hearings:

In addition to the public notice and agenda requirements of the Brown Act, the Group Chair should notify the project applicant's point of contact and the PDS Project Manager at least two weeks in advance of the date and time of the scheduled meeting.

5510 OVERLAND AVE, SUITE 110, SAN DIEGO, CA 92123 • (858) 565-5981 • (888) 267-8770

<http://www.sdcountry.ca.gov/pds>



County of San Diego, Planning & Development Services
**COMMUNITY PLANNING OR SPONSOR
GROUP PROJECT RECOMMENDATION**
ZONING DIVISION

Record ID(s): PDS2015-MUP-15-027 PDS2015-ER-15-14-010

Project Name: Liberty Charter High School

Planning/Sponsor Group: Valle de Oro Planning Group

Results of Planning/Sponsor Group Review

Meeting Date: 2/4/2020

A. Comments made by the group on the proposed project.

B. Advisory Vote: The Group Did Did Not make a formal recommendation, approval or denial on the project at this time.

If a formal recommendation was made, please check the appropriate box below:

- MOTION:
- Approve without conditions
 - Approve with recommended conditions
 - Deny
 - Continue

VOTE: 10 Yes 0 No 0 Abstain 3/2 Vacant/Absent

C. Recommended conditions of approval:

Reported by: Wendy L. Tinsley Becker Position: V-Chair Date: 02/04/2020

Please email recommendations to BOTH EMAILS.

Project Manager listed in email (in this format): Firstname.Lastname@sdcounty.ca.gov and to CommunityGroups.LUEG@sdcounty.ca.gov

5510 OVERLAND AVE, SUITE 110, SAN DIEGO, CA 92123 • (858) 565-5981 • (888) 267-8770

<http://www.sdcounty.ca.gov/pds>



Russell, Denise

From: Chance Agnew <litechance@gmail.com>
Sent: Monday, January 20, 2020 6:56 PM
To: Russell, Denise
Cc: Jacob, Dianne
Subject: Liberty Charter High School Concerns

Good evening Denise,

I live on Fuerte which is .7 miles from the proposed development site of Liberty Charter High School. My address is 12108 Fuerte Dr. My teen and child attend Valhalla HS and Fuerte Elementary. My biggest concern with the location of the school is traffic at that intersection, Chase, and Fuerte Drive which is a light emitter road. Fuerte Drive is currently used as a short cut or thruway between Avocado and Chase. Over the last three years, it seems the amount of traffic on Fuerte has doubled. Fuerte Elementary already brings families from all over into this neighborhood under ESSA and there is a new development of 36-70 houses going in at the Chicken Ranch. The intersection of Fuerte and Chase will be a nightmare if the school locates at it's proposed location. I'm repeatedly told there is a shortage of CHP resources that have jurisdiction over Fuerte Dr. Let's buy this land back and plan for a CHP/Sheriff Branch office to help protect our community. We have plenty of schools in our immediate area.

Very Respectfully,

Chance Agnew
619-302-7173

Sent from my iPhone

Russell, Denise

From: Justin Bennett <justinb@mcwe.com>
Sent: Wednesday, January 22, 2020 5:14 PM
To: Russell, Denise
Subject: Liberty Charter High School

Dear Denise Russell,

I wanted to reach out to you, as a parent students from Liberty Charter High School. Both my girls have graduated and my son is currently enrolled. All three of them have been at the top of their classes. Their success is a direct result of what they have learned, experienced from this school. Liberty Charter works to achieve building character in our kids and I see that in action not only with my kids but how our athletes perform on and off the field. There is a great difference. I am a proud parent of the Literacy Charter Schools which include the Liberty Charter High School.

My oldest attended the high school at a church in La Mesa prior to the move to the current facility in Lemon Grove. I know how much it means to staff, parents and students to have their own school and much closer to home. It would be great to see my son, finish his high school year at the new location in El Cajon.

Having looked at the plans, the LCHS has done their due diligence in addressing the concerns of traffic, parking, and architecture. Would you please put your vote of approval and support this project to its completion.

Thank you!!

Justin Bennett

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DEPARTMENT OF TRANSPORTATION

DISTRICT 11
4050 TAYLOR STREET, MS-240
SAN DIEGO, CA 92110
PHONE (619) 688-6075
FAX (619) 688-4299
TTY 711
www.dot.ca.gov



*Making Conservation
a California Way of Life.*

April 9, 2020

11-SD-54
PM 10.5
Liberty Charter High School
TIS

Ms. Denise Russell
Land Use/Environmental Planner
County of San Diego
Department of Planning and Development Services
5510 Overland Avenue, Suite 110
San Diego, CA 92123

Dear Ms. Russell:

Thank you for including the California Department of Transportation (Caltrans) in the Focused Traffic Impact Study (TIS) review dated September 12, 2019 for the proposed Liberty Charter High School located on Chase Road near Jamacha Road (State Route 54 (SR- 54) in the community of Valle de Oro. The mission of Caltrans is to provide a safe, sustainable, integrated and efficient transportation system to enhance California's economy and livability. The Local Development-Intergovernmental Review (LD-IGR) Program reviews land use projects and plans to ensure consistency with our mission and state planning priorities.

Caltrans has the following comments:

Traffic Impact Study

1. Include the missing Table 4-1 and Figure 5-1 in TIS.
2. Only a one-day traffic count was provided, and the data was more than 4 years old. A one-day count is not a good representation of actual traffic movements. Provide more recent traffic counts which include minimum three-day mid-week traffic counts.
3. The Traffic Impact Fee (TIF) sub-section under Section 2.3 states that by participating in the County of San Diego's TIF program, the potential cumulative traffic impacts to County's circulation Element Roadways would

Ms. Denise Russell
April 9, 2020
Page 2

be mitigated to less than significant. The Traffic Impact Study's section 'Cumulative Impacts' agrees to participate in the county of San Diego Traffic Impact Fee (TIF) but fails to study the cumulative impacts the proposed project will incur to State's facilities. Provide traffic analysis for the Horizon year without Project (Cumulative) and Horizon year with Project (Cumulative) to verify if there are any traffic impacts to any of the State's facilities.

If you have any questions, please contact Mark McCumsey at (619) 688-6802 or by email at mark.mccumsey@dot.ca.gov.

Sincerely,

electronically signed by

MAURICE EATON, Branch Chief
Local Development and Intergovernmental Review Branch

Russell, Denise

From: Lenny Coppenrath <lennyc@cox.net>
Sent: Sunday, January 19, 2020 10:30 AM
To: Russell, Denise
Subject: Proposed site for Liberty Charter HS

Hi there,

I am a local resident of the area proposed for the Liberty Charter HS and a board member of the Hidden Mesa Estates HOA. Traffic is the main concern we have for this proposal. Before, and after school hours this intersection at Chase and Jamacha, and the entire length of Chase from west of Vahalla is congested.

This is due to the comings and goings of parents and students commuting to the various schools in the area. The addition of another school will only make that worse.

I understand the proposal includes a widening of Chase in an attempt to accommodate this additional traffic, but we as a community see this as an inadequate solution since we truly understand the increasing traffic problems in this area, not to mention the dangerous high speeds drivers on both Chase and Jamacha at all times of the day.

For the record:
Leonard J Coppenrath
1335 Hidden Mt Dr
El Cajon Ca 92019
619 916 7019

Russell, Denise

From: Smith, Ashley
Sent: Tuesday, January 21, 2020 7:12 AM
To: Russell, Denise
Subject: FW: Liberty Charter High School PDS2015-MUP-15-027 PDS2015-ER-15-14-010

-----Original Message-----

From: Shawn Dennison <sdennison@cox.net>
Sent: Thursday, January 16, 2020 5:12 PM
To: Smith, Ashley <Ashley.Smith2@sdcounty.ca.gov>
Subject: Liberty Charter High School PDS2015-MUP-15-027 PDS2015-ER-15-14-010

Dear Ashley,

I just wanted to let you know that I am totally against this Charter School going in at the corner of Chase and Jamacha. Traffic in this area is already hell as it is! Valhalla High School is just a stones throw away as well as Vista Grande Elementary. By placing a school at that location it will make it virtually impossible to get out of the area. Just pull up the numbers of accidents we have at that intersection now, I see emergency vehicles heading there virtually every day.

Please do not allow this school to be built it is will contribute to an already dangerous situation.

Sincerely,

Shawn Dennison
Rancho San Diego Resident

Russell, Denise

From: cindy <cenglish4@cox.net>
Sent: Thursday, February 06, 2020 12:32 PM
To: Russell, Denise
Subject: Proposed Liberty Charter school location

Just wanted to give my input to the proposed school on Chase and Jamacha.

Anyone that lives East of this area would confirm that the traffic is so congested with a two lane road and then adding a school site in that area would be horrendous. Traffic backs up during school hours as it is because of the elementary and high school. Traffic backs up during the working hour days as well.

It is difficult to drive out of the strip mall at the bottom of the hill right now. I see so many people frustrated to pull out of the driveway there that they make poor decisions to just pull out in front of cars coming down the hill.

I would not want to be living anywhere on the Chase area. Cars making a left or right turn out of the streets crossing Chase have a hard time pulling out safely. I have experienced a few times where they pull out in front of you and have to slam on the brake.

Overall this would be a nightmare if a school is built on that lot.

I have lived in this area for 30 years now and have noticed the changes in traffic congestion with all the new homes that have been added over the years.

I hope you would consider the input of concerned people that live in this area.

I did not attend the meeting as the time is in the evening hours and I now prefer not to drive at night so I did not attend.

Cynthia English
2071 Chardon Lane
El Cajon 92019
Sent from [Mail](#) for Windows 10

Russell, Denise

From: Amy Strider Harleman <ash@petersonprice.com>
Sent: Thursday, January 23, 2020 12:19 PM
To: Russell, Denise
Subject: Liberty Charter High School

I live just up the hill at 1617 Fuerte Hills Drive and I strongly object to this project. We bought a house on a large lot and private street because this area is quiet! A big high school visible from our peaceful deck would be a nightmare, not to mention the traffic.

Amy Strider Harleman | Shareholder
ash@petersonprice.com

PETERSON & PRICE APC
402 West Broadway, Suite 960 • San Diego, California 92101
Telephone: 619-234-0361, Ext. 235

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Russell, Denise

From: Danielle Nunez <dmnunez81@gmail.com>
Sent: Sunday, January 19, 2020 8:35 AM
To: Russell, Denise
Subject: New LFCS High School

Hello-

It's imperative that we utilize this space in our neighborhood for the new school.

LFCS is a wonderful place to educate kids, and mine have benefited from this program for the last 8 years.

I know traffic is a concern, but the benefits to the community outweigh the costs of that. LFCS is a good neighbor and makes a point of educating our families about proper procedures and processes concerning our facilities.

It will be a great addition to the community and it will give parents more choice in their children's education.

Thank you.

Danielle Nuñez
LFCS parent

Russell, Denise

From: M Patricia Welling <mpwelling@gmail.com>
Sent: Thursday, February 06, 2020 4:15 PM
To: Russell, Denise
Subject: Liberty Charter High School project

Ms. Russell,

I own a home in close proximity to this project and I would like to express my disapproval to this project moving forward. You should be aware that the intersection of Chase and Jamacha have been the site of numerous accidents. I can't imagine more congestion in this intersection, which will undoubtedly cause more car accidents and likely pedestrian accidents. The approval of this project will create a danger to the public. The increase danger to the public is not offset by community's need for an additional school. This community already has enough schools.

Respectfully,

Patricia Welling
1483 Hidden Mesa Trail
El Cajon, CA 92019

cell 619 995-1766



July 30, 2020,

San Diego County Planning Commission and Staff:

Jerry Keough asked me to describe daily life at LCHS. Please find that description below and some of my thoughts in regards to the specific concerns raised by a group of neighbors to the proposed new site of LCHS. I attended a recent Zoom meeting of this group, SOS^2 where they described their major concerns.

Daily Life at LCHS:

Drop off for 8:30am start time: Teachers begin trickling in as early as 6:00am. With an 8:30 start time, teachers are contracted to be on campus by 8:00am. Typically, 60% of teachers are on campus 45 minutes before the start of school with the other 40% arriving in the 15 minutes prior to when they are contracted to be on campus. 95% of parents will drop off their students from 7:30-8:45. Some parents will drop students off as early as 7:00 but that is rare and few (currently less than 10). Drop off is a slow steady flow of traffic 10-20 cars at a time over the 45 minute period before the start of class at 8:30. We have heavy supervision at drop off starting 30 minutes before school starts. Administrators are on campus one hour prior to the start of school and do keep an eye on the pick up area during this time frame. The vice principal greets every student as they walk through the gate by name each morning. About 30 students drive their own cars to school. This number has remained relatively constant over the past six years. Parking is one of the neighbor's concerns. They assume many more students drive cars to campus than actually do. I think we need to map out how cars will enter and exit and be able to show how many cars can be lined up in our parking lot during drop off as a major concern is traffic backing up into the street.

School day: 8:30-3:45: Regarding traffic, we have roughly 30 people stop by the office during any given school day. More so toward the last hour to pick students up early for appointments. Lunch is delivered in one truck around 10:30am. UPS and Fed Ex make frequent deliveries at about once per day each, usually in the middle of the day.

Pick up: 3:45-4:15: Currently, within 8 minutes our entry is cleared of cars and parents picking up students drive right up, pick up, and drive right out. Starting about half an hour before the school day ends, parents begin arriving to park in our long double wide entry way. Our entry is full at the end of the school day but clears out in 8 minutes or less. Like drop off, I think we need to map out how cars will line up on campus for pick up so we can show how many cars will fit on campus as the concern is that cars will back up on the street as our lot will be full and exiting cars will not be able to get out due to the traffic on Chase. Teachers are contracted to be on campus half an hour after school ends so they won't be leaving until 4:15.

A Literacy First Charter Schools School
8425 Palm Street • Lemon Grove, CA 91945
Phone: 619.668.2131 Fax: 619.668.2133
www.libertychs.org



After school activities: Fall: girls volleyball (JV and V), flag football, Cross Country (boys and girls). All these students will remain on campus until about 5:00p for practice. Participation on these three teams is close to 100 students total. Volleyball home games are usually scheduled for Tue and Thu and can run until 8:00pm. Volleyball practices and games will be conducted in the gym at the new site. Flag football plays their games on Saturday mornings. XC runs its races on Thursdays off campus and would have left campus around 2:30pm. Winter: boys and girls soccer, boys and girls basketball. This accounts for about another 100 students. They will either be on campus after school for practice or home games (Tue and / or Thu) or would have left campus around 2 to get to away games. Home soccer games (one per Tue or Thu) typically start at 3:15 and last 2 hours. If we have lights, we would start them later so students don't have to miss school. Game time would be 4:30 - 6:30. We expect lights to be out by 7:00pm, 8:00pm at the latest. Basketball games run until about 8:00pm depending on if there is one or two games to play on a given night. Soccer practices and games will be conducted on the field of the new site and basketball practices and games will be conducted in the gym of the new site. Spring: softball, baseball, boys volleyball, track and field, drama production. This accounts for about 125 students. Since we won't have softball or baseball fields, these teams will have to practice and play games off campus. We do anticipate having track meets on our site with lights expected to be out by 8:00pm. Track meets are typically on Thursdays. Boys home volleyball games are typically Tuesday and or Thursday nights. Boys volleyball practices and games will be conducted in the gym of the new site. We will have students rehearsing for our spring drama production on campus after school Mon-Fri as well from March through May.

We typically have about 20 students earn the ability to either arrive to school late or leave early depending on their schedule and grades. These are typically juniors or seniors.

Additional thoughts:

The opposition group (SOS^2) has expressed concern about the lights, noise, and parking. They assume we will be as loud as Valhalla with our lights on until 11:00pm at night and that we have nowhere near enough parking for the school day and after school events / games. They overestimate the number of people who attend games and the number of students who drive to school and park their car for the day. During a recent Zoom meeting held by this group, I did address the lights.

However, in my opinion these are minor concerns. What this group really cares about is traffic. To them, the existing traffic is unbearable and one more car is too many. We could have a parking garage of 800 spots, no lights, and no loudspeaker and they would still adamantly oppose construction. Their main issue is traffic, traffic, traffic. They are convinced we do not realize how bad it is now or we would never have considered building at that location. The additional turn lane, in their opinion, does little to nothing to improve the traffic and really nothing we could do will make the traffic better as we are adding roughly 800 cars per day in traffic. They also claim the traffic study is outdated and was conducted over Thanksgiving weekend and thus is inaccurate and invalid. They conducted their own traffic study (



members of the group counted cars from 8:00am-9:00am on a weekday and counted 1,000 cars. I think we need to be prepared to defend the traffic study.

Their secondary line of attack is CEQA. A CEQA attorney lives in the neighborhood and has been very vocal and explicit about attacking us along the lines that the County has not required us to do all that is necessary to follow CEQA guidelines, specifically to obtain the MND. She believes we have not met the threshold for a MND and that we must do a AIR. She is also convinced that if we did an AIR, we would not pass. I think we need to be prepared to defend our MND and the integrity of the process. In my opinion, she believes the County has been negligent in following CEQA for this project.

While I am convinced their main concern is traffic, they welcome the CEQA line of attack as additional grounds to argue against construction. We need to be prepared to address traffic and CEQA. In this groups' eyes, we may be able to convince them we have followed CEQA guidelines but I don't think anything we say will convince them that the traffic will be bearable. Fortunately for us, the County Planning Commission makes the decision and their opinion of the traffic may be different than that of this group. Our crucial audience is the County.

Sincerely,

Steve Robinson

Vice Principal
Liberty Charter High School
Steve.robinson@lfcsinc.org

**Attachment F – SERVICE
AVAILABILITY FORMS**



County of San Diego, Planning & Development Services
PROJECT FACILITY AVAILABILITY - SCHOOL
ZONING DIVISION

Please type or use pen
(Two forms are needed if project is to be served by separate school districts)
Literacy First Charter Schools, Inc. (619) 596-5905
Owner's Name Phone
1012 East Bradley Avenue
Owner's Mailing Address Street
El Cajon, CA 92020
City State Zip
ORG _____ ACCT _____ ACT _____ TASK _____ DATE _____
ELEMENTARY _____ HIGH SCHOOL _____ UNIFIED _____
DISTRICT CASHIER'S USE ONLY

Sc

SECTION 1. PROJECT DESCRIPTION TO BE COMPLETED BY APPLICANT

A. LEGISLATIVE ACT
B. DEVELOPMENT PROJECT
C. Residential, Commercial, Industrial, Other
D. Total Project acreage 8.83 Total number lots 1
Assessor's Parcel Number(s)
498-330-39
Thomas Guide Page 1272 Grid A2
1530 Jamacha Road, El Cajon, CA 92020
Project address Street
Valle De Oro 92020
Community Planning Area/Subregion Zip
Applicant's Signature: [Signature] Date: 10/21/15
Address: 1012 East Bradley Avenue, El Cajon, CA 92020 Phone: (619) 596-5905
(On completion of above, present to the district that provides school protection to complete Section 2 below.)

SECTION 2: FACILITY AVAILABILITY TO BE COMPLETED BY DISTRICT

District Name: Cajon Valley Union School District
If not in a unified district, which elementary or high school district must also fill out a form? Grossmont UHSD
Indicate the location and distance of proposed schools of attendance.
Elementary: Fuente Elem. miles: 1.5
Junior/Middle: Hillside Middle miles: 1.9
High school: miles:
Project is located entirely within the district and is eligible for service.
Authorized Signature: [Signature] Print Title: Planning Tech
Sve Miller Print Name
619. 588-3676 Phone
10/21/15
On completion of Section 2 by the district, applicant is to submit this form with application to: Planning & Development Services, Zoning Counter, 5510 Overland Ave. Suite 110 San Diego, CA 92123



1 - 397

County of San Diego, Planning & Development Services
PROJECT FACILITY AVAILABILITY - SCHOOL
ZONING DIVISION

Please type or use pen

(Two forms are needed if project is to be served by separate school districts)

Literacy First Charter Schools, Inc. (619) 596-5905
Owner's Name Phone
1012 East Bradley Avenue
Owner's Mailing Address Street
El Cajon, CA 92020
City State Zip

ORG _____
ACCT _____
ACT _____ ELEMENTARY _____
TASK _____ HIGH SCHOOL _____
DATE _____ UNIFIED _____

Sc

DISTRICT CASHIER'S USE ONLY

SECTION 1. PROJECT DESCRIPTION

TO BE COMPLETED BY APPLICANT

A. LEGISLATIVE ACT

- Rezones changing Use Regulations or Development Regulations
General Plan Amendment
Specific Plan
Specific Plan Amendment

Assessor's Parcel Number(s)
(Add extra if necessary)

Table with 2 columns and 4 rows containing parcel numbers, with '498-330-39' in the first row.

B. DEVELOPMENT PROJECT

- Rezones changing Special Area or Neighborhood Regulations
Major Subdivision (TM)
Minor Subdivision (TPM)
Boundary Adjustment
Major Use Permit (MUP), purpose: Charter High School
Time Extension...Case No.
Expired Map...Case No.
Other

Thomas Guide Page 1272 Grid A2
1530 Jamacha Road, El Cajon, CA 92020
Project address Street
Valle De Oro 92020
Community Planning Area/Subregion Zip

- Residential Total number of dwelling units
Commercial Gross floor area
Industrial Gross floor area
Other Gross floor area +/- 48,000 square feet

D. Total Project acreage 8.83 Total number lots 1

Applicant's Signature: [Signature] Date: 10/21/15

Address: 1012 East Bradley Avenue, El Cajon, CA 92020 Phone: (619) 596-5905

(On completion of above, present to the district that provides school protection to complete Section 2 below.)

SECTION 2: FACILITY AVAILABILITY

TO BE COMPLETED BY DISTRICT

District Name: Grossmont Union H.S. District
If not in a unified district, which elementary or high school district must also fill out a form?

Indicate the location and distance of proposed schools of attendance.

Elementary: _____ miles: _____
Junior/Middle: _____ miles: _____
High school: Valhalla High School _____ miles: _____

- This project will result in the overcrowding of the elementary junior/school high school. (Check)
Fees will be levied or land will be dedicated in accordance with Education Code Section 17620 prior to the issuance of building permits.
Project is located entirely within the district and is eligible for service.
The project is not located entirely within the district and a potential boundary issue may exist with the school district.

Authorized Signature: [Signature] 10/21/15
Planning Technician

Print Name: Rosa Rosselli
Phone: 619 644 8177

On completion of Section 2 by the district, applicant is to submit this form with application to:
Planning & Development Services, Zoning Counter, 5510 Overland Ave. Suite 110 San Diego, CA 92123





County of San Diego, Planning & Development Services
PROJECT FACILITY AVAILABILITY - SEWER
ZONING DIVISION

Please type or use pen

Literacy First Charter Schools, Inc. (619) 596-5905	ORG _____
Owner's Name Phone	ACCT _____
1012 East Bradley Avenue	ACT _____
Owner's Mailing Address Street	TASK _____
El Cajon, CA 92020	DATE _____
City State Zip	AMT \$ <u>75.00</u>

S

DISTRICT CASHIER'S USE ONLY

SECTION 1. PROJECT DESCRIPTION **TO BE COMPLETED BY APPLICANT**

A. Major Subdivision (TM) Certificate of Compliance: _____
 Minor Subdivision (TPM) Boundary Adjustment
 Specific Plan or Specific Plan Amendment
 Rezone (Reclassification) from _____ to _____ zone
 Major Use Permit (MUP), purpose: Charter High School
 Time Extension... Case No. _____
 Expired Map... Case No. _____
 Other _____

B. Residential Total number of dwelling units _____
 Commercial Gross floor area _____
 Industrial Gross floor area _____
 Other Gross floor area +/- 48,000 square feet _____

C. Total Project acreage 8.83 Total lots 1 Smallest proposed lot 8.83

D. Is the project proposing its own wastewater treatment plant? Yes No
 Is the project proposing the use of reclaimed water? Yes No

Assessor's Parcel Number(s)
 (Add extra if necessary)

498-330-39	

Thomas Guide Page 1272 Grid A2
 1530 Jamacha Road, El Cajon, CA 92020
 Project address Street
 Valle De Oro 92020
 Community Planning Area/Subregion Zip

Owner/Applicant agrees to pay all necessary construction costs and dedicate all district required easements to extend service to the project.
OWNER/APPLICANT MUST COMPLETE ALL CONDITIONS REQUIRED BY THE DISTRICT.

Applicant's Signature: [Signature] Date: 10/21/15
 Address: 1012 East Bradley Avenue, El Cajon, CA 92020 Phone: (619) 596-5905

(On completion of above, present to the district that provides sewer protection to complete Section 2 below.)

SECTION 2: FACILITY AVAILABILITY **TO BE COMPLETED BY DISTRICT**

District name Otay Water District Service area Must be annexed into Sewer Improvement I.D. 18

A. Project is in the District.
 Project is not in the District but is within its Sphere of Influence boundary, owner must apply for annexation.
 Project is not in the District and is **not** within its Sphere of Influence boundary.
 Project is not located entirely within the District and a potential boundary issue exists with the _____ District.

B. Facilities to serve the project ARE ARE NOT reasonably expected to be available within the next 5 years based on the capital facility plans of the district. Explain in space below or on attached. Number of sheets attached: _____
 Project will not be served for the following reason(s): _____

C. District conditions are attached. Number of sheets attached: 3
 District has specific water reclamation conditions which are attached. Number of sheets attached: _____
 District will submit conditions at a later date. Northeast side of Property fronts sewer main at Chase Ave and Jamacha Rd.

D. How far will the pipeline(s) have to be extended to serve the project? _____

This Project Facility Availability Form is valid until final discretionary action is taken pursuant to the application for the proposed project or until it is withdrawn, unless a shorter expiration date is otherwise noted.

[Signature] Vu Tran, Permit Tech. 619-670-224 10/30/15
 Authorized Signature Print Name and Title Phone Date

THIS DOCUMENT IS NOT A COMMITMENT OF FACILITIES OR SERVICE BY THE DISTRICT On completion of Section 2 by the district, applicant is to submit this form with application to: Planning & Development Services, Zoning Counter, 5510 Overland Ave. Suite 110 San Diego, CA 92123



County of San Diego, Planning & Development Services
PROJECT FACILITY AVAILABILITY - WATER
ZONING DIVISION

Please type or use pen
Literacy First Charter Schools, Inc. (619) 596-5905
Owner's Name Phone
1012 East Bradley Avenue
Owner's Mailing Address Street
El Cajon, CA 92020
City State Zip
ORG
ACCT
ACT
TASK
DATE
AMT \$ 75.00
DISTRICT CASHIER'S USE ONLY

SECTION 1. PROJECT DESCRIPTION TO BE COMPLETED BY APPLICANT
A. Major Subdivision (TM) Specific Plan or Specific Plan Amendment
Minor Subdivision (TPM) Certificate of Compliance:
Boundary Adjustment
Rezone (Reclassification) from to zone.
[X] Major Use Permit (MUP), purpose: Charter High School
Time Extension... Case No.
Expired Map... Case No.
Other
B. Residential Total number of dwelling units
Commercial Gross floor area
Industrial Gross floor area
[X] Other Gross floor area +/- 48,000 square feet
C. [X] Total Project acreage 8.83 Total number of lots 1
D. Is the project proposing the use of groundwater? Yes No
Is the project proposing the use of reclaimed water? Yes No
Assessor's Parcel Number(s)
498-330-39
Thomas Guide Page 1272 Grid A2
1530 Jamacha Road, El Cajon, CA 92020
Project address Street
Valle De Oro 92020
Community Planning Area/Subregion Zip
Owner/Applicant agrees to pay all necessary construction costs, dedicate all district required easements to extend service to the project and COMPLETE ALL CONDITIONS REQUIRED BY THE DISTRICT.
Applicant's Signature: [Signature] Date: 10/21/15
Address: 1012 East Bradley Avenue, El Cajon, CA 92020 Phone: (619) 596-5905
(On completion of above, present to the district that provides water protection to complete Section 2 below.)

SECTION 2: FACILITY AVAILABILITY TO BE COMPLETED BY DISTRICT
District Name: Otay Water District Service area Water ID 22
A. [X] Project is in the district.
Project is not in the district but is within its Sphere of Influence boundary, owner must apply for annexation.
Project is not in the district and is not within its Sphere of Influence boundary.
The project is not located entirely within the district and a potential boundary issue exists with the District.
B. [X] Facilities to serve the project ARE ARE NOT reasonably expected to be available within the next 5 years based on the capital facility plans of the district. Explain in space below or on attached. (Number of sheets)
Project will not be served for the following reason(s):
C. [X] District conditions are attached. Number of sheets attached: 3
District has specific water reclamation conditions which are attached. Number of sheets attached:
District will submit conditions at a later date.
D. How far will the pipeline(s) have to be extended to serve the project? North side of Property fronts existing water main at Chase Ave
This Project Facility Availability Form is valid until final discretionary action is taken pursuant to the application for the proposed project or until it is withdrawn, unless a shorter expiration date is otherwise noted.
Authorized Signature: [Signature] Print Name Vu Tran
Print Title Permit Tech. Phone 619-670-224 Date 10/30/15
NOTE: THIS DOCUMENT IS NOT A COMMITMENT OF SERVICE OR FACILITIES BY THE DISTRICT
On completion of Section 2 and 3 by the District, applicant is to submit this form with application to:
Planning & Development Services - Zoning Counter, 5510 Overland Ave, Suite 110, San Diego, CA 92123





...Dedicated to Community Service

2554 SWEETWATER SPRINGS BOULEVARD, SPRING VALLEY, CALIFORNIA 91978-2004
TELEPHONE: 670-2222, AREA CODE 619
www.otaywater.gov

Sent via electronic mail to: mjlinman@cox.net

October 29, 2015

Project No.: p1438-010000
Activity: 3111

Mark J. Linman
Land Use & Development Consulting
11316 Rolling Hills Drive
El Cajon, CA 92020

Subject: Liberty High School
Project Description: Proposed Charter High School
Location: 1530 Jamacha Road, El Cajon CA 92020
APN: 498-330-39-00

Dear Mr. Linman:

As per Section 62.01 of the Otay Water District's (District) Code of Ordinances (enclosed), "To provide for future line extensions, pipelines installed within public streets must be constructed to the subdivision boundary and pipelines not installed within a public street must be installed in a District easement or right-of-way and must extend across the frontage of the parcel or parcels to be served."

As provided to the District, the subject project (Project) consists of a proposed 48,000 square-foot, two-story high school. The Project also includes 22 classrooms, administrative office space, and a gymnasium. The District anticipates the ability to supply water to meet this proposed development.

The Project can be served by an existing 12-inch potable water main located on Chase Avenue and an existing 12-inch potable water main located on Jamacha Road. The Project can also be served by an existing 8-inch sewer main located on Jamacha Road. The developer is required to submit improvement plans for District approval and pay all fees including plan review, inspection, water meter installation, and capacity fees prior to any work.

In the event that the Project interferes with the use of any District facilities or requires relocation of facilities on the subject property, which facilities exist by right of easement or otherwise, the owner/developer will provide a substitute easement and pay any additional cost associated with its use, and/or bear the cost of such relocation, and provide the District with suitable replacement rights. Such costs and replacement rights are required prior to the performance of the relocation.

Mark J. Linman
Liberty High School
October 29, 2015
Page 2 of 2.

When a customer requests water service on a parcel of land with irrigated landscape equal to 5,000-square-feet or more, a separate meter will be required for irrigation purposes on the site. If service laterals do not exist for the Project, the applicant must pay to have the District install them.

Each service must have an approved reduced pressure principle backflow prevention device (R/P) purchased and installed by the developer. The developer should contact the Project's fire agency for any fire protection requirements. Water furnished for fire hydrant or fire sprinkler service shall be used only for fire protection purposes and shall be connected to a District water main. Where service is provided for fire hydrant or fire sprinkler service on privately-owned land, the service shall be provided by the District at the property line of land to be served.

Water availability is subject to all District requirements in effect at the time and you are strongly encouraged to adopt water conservation measures throughout the development.

The District's Engineering Public Services Department can be contacted at (619) 670-2241 or visit the webpage at www.otaywater.gov for further requirements regarding inspection services, water main extensions, service laterals, backflow devices, and/or meter costs (see enclosed sections of the District's Code of Ordinances), and any other conditions that may have arisen since this letter was written for this Project.

Also, returned herewith are the documents you forwarded with your review request.

Sincerely,
OTAY WATER DISTRICT



Dan Martin, P.E.
Engineering Manager

DM:mlc

Enclosures: Location map
Code of Ordinances Sections 23, 25, 27, 28, 38, 39, 60, 62
Documents submitted with review request



EXISTING 12 INCH PVC WATER MAIN
(PZ: 978 / AB: 157-03)

EXISTING 8 INCH PVC SEWER MAIN
(AB: 28-18)

SUBJECT PROPERTY
APN: 498-330-39-00

EXISTING 12 INCH PVC WATER MAIN
(PZ: 978 / AB: 164-20)

92020

92019

368

367

PENAS

IR VALLE

FAIR GLEN RD

JAMACHA RD

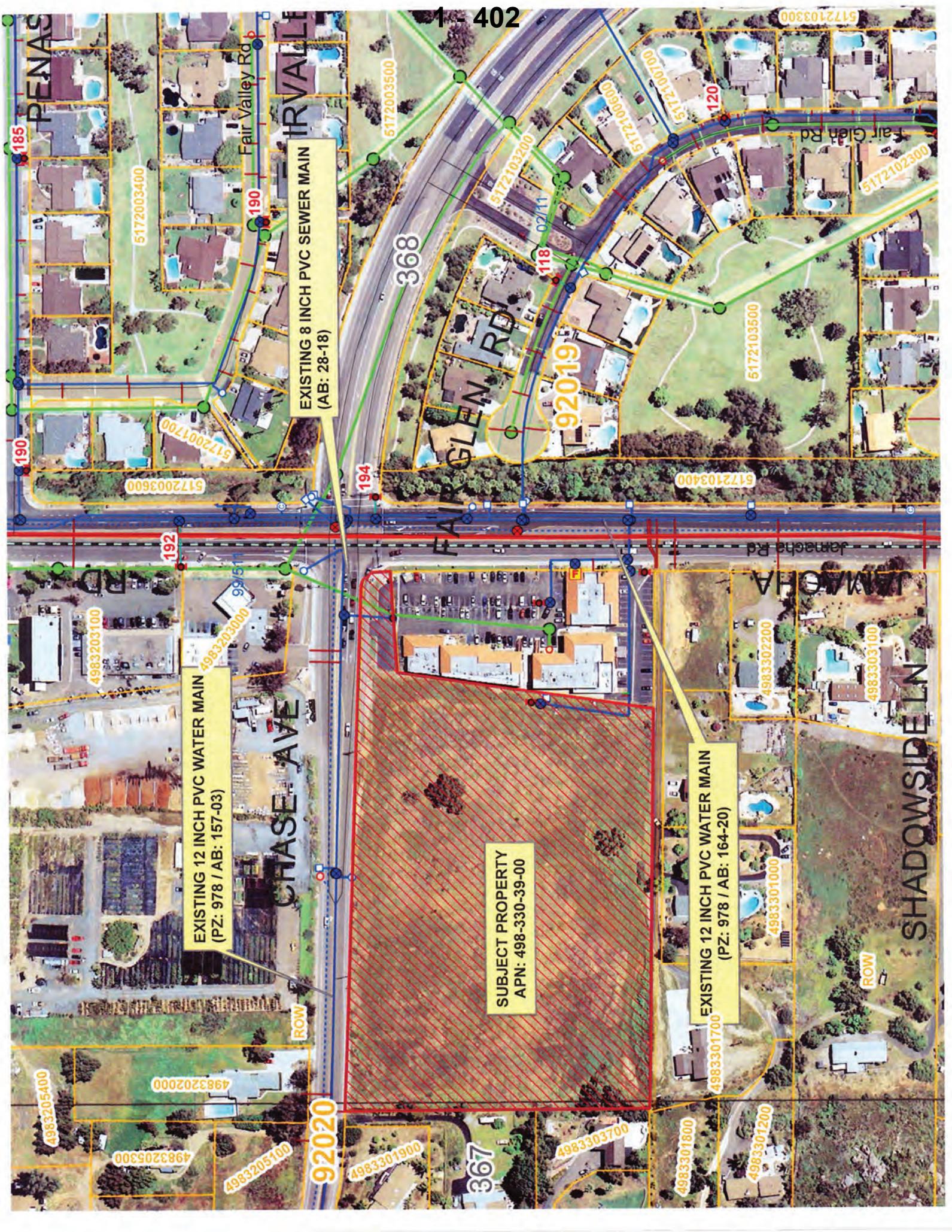
SHADOWSIDE LN

CHASE AVE

Fair Valley Rd

Fair Glen Rd

ROW





County of San Diego, Planning & Development Services
PROJECT FACILITY AVAILABILITY - FIRE
ZONING DIVISION

Please type or use pen

Literacy First Charter Schools, Inc. (619) 596-5905
 Owner's Name Phone
 1012 East Bradley Avenue
 Owner's Mailing Address Street
 El Cajon, CA 92020
 City State Zip

ORG _____
 ACCT _____
 ACT _____
 TASK _____
 DATE _____ AMT \$ _____

F

DISTRICT CASHIER'S USE ONLY

SECTION 1. PROJECT DESCRIPTION

TO BE COMPLETED BY APPLICANT

- A. Major Subdivision (TM) Specific Plan or Specific Plan Amendment
 Minor Subdivision (TPM) Certificate of Compliance: _____
 Boundary Adjustment
 Rezone (Reclassification) from _____ to _____ zone.
 Major Use Permit (MUP), purpose: Charter High School
 Time Extension... Case No. _____
 Expired Map... Case No. _____
 Other _____
- B. Residential Total number of dwelling units _____
 Commercial Gross floor area _____
 Industrial Gross floor area _____
 Other Gross floor area +/- 48,000 square feet
- C. Total Project acreage 8.83 Total lots 1 Smallest proposed lot 8.83

Assessor's Parcel Number(s)
 (Add extra if necessary)

498-330-39	

Thomas Guide. Page 1272 Grid A2
 1530 Jamacha Road, El Cajon, CA 92020
 Project address Street
 Valle De Oro 92020
 Community Planning Area/Subregion Zip

OWNER/APPLICANT AGREES TO COMPLETE ALL CONDITIONS REQUIRED BY THE DISTRICT.

Applicant's Signature: [Signature] Date: 10/21/15
 Address: 1012 East Bradley Avenue, El Cajon, CA 92020 Phone: (619) 596-5905
 (On completion of above, present to the district that provides fire protection to complete Section 2 and 3 below.)

SECTION 2: FACILITY AVAILABILITY

TO BE COMPLETED BY DISTRICT

- District Name: San Miguel Consolidated Fire Protection District
 Indicate the location and distance of the primary fire station that will serve the proposed project:
STA# 22 11501 Via Rancho SAN DIEGO, El Cajon 2 miles
- A. Project is in the District and eligible for service.
 Project is not in the District but is within its Sphere of Influence boundary, owner must apply for annexation.
 Project is not in the District and not within its Sphere of Influence boundary.
 Project is not located entirely within the District and a potential boundary issue exists with the _____ District.
- B. Based on the capacity and capability of the District's existing and planned facilities, fire protection facilities are currently adequate or will be adequate to serve the proposed project. The expected emergency travel time to the proposed project is 4 minutes.
 Fire protection facilities are not expected to be adequate to serve the proposed development within the next five years.
- C. District conditions are attached. Number of sheets attached: 1
 District will submit conditions at a later date.

SECTION 3. FUELBREAK REQUIREMENTS

Note: The fuelbreak requirements prescribed by the fire district for the proposed project do not authorize any clearing prior to project approval by Planning & Development Services.

- Within the proposed project 100 feet of clearing will be required around all structures.
 The proposed project is located in a hazardous wildland fire area, and additional fuelbreak requirements may apply. Environmental mitigation requirements should be coordinated with the fire district to ensure that these requirements will not pose fire hazards.

This Project Facility Availability Form is valid until final discretionary action is taken pursuant to the application for the proposed project or until it is withdrawn, unless a shorter expiration date is otherwise noted.

Authorized Signature: [Signature] Print Name and Title: Tony Morgan DFM Phone: 619-670-0500 Date: 10/27/15

On completion of Section 2 and 3 by the District, applicant is to submit this form with application to:
 Planning & Development Services - Zoning Counter, 5510 Overland Ave, Suite 110, San Diego, CA 92123





San Miguel Consolidated Fire Protection District

Serving the communities of Bostonia, Casa de Oro, Crest, Grossmont/Mt. Helix, La Presa, Rancho San Diego, Spring Valley, and unincorporated areas of El Cajon and La Mesa

October 27, 2015

San Diego County
Department of Planning and Land Use
5510 Overland Avenue, Suite 110
San Diego, CA 92123

Ref: MUP, APN 498-330-39, Charter High School

The San Miguel Consolidated Fire Protection District has the following requirements for the above referenced project:

- Fire Sprinklers: Structures shall have an automatic fire sprinkler system installed per NFPA 13 standards and San Miguel Consolidated Fire Protection District standards
- Fire Alarm System shall be installed per NFPA 72 Standards.
- Gates shall have a clear width of 24 feet, and a vertical clearance of 13 feet 6 inches.
- Provide a 24 foot wide fire lane for access to all sides of the school.
- Provide access from structures to the athletic field by means of ramp/stairs for emergency evacuation of students.
- Fire Hydrants: The location, number and type of fire hydrant connected to a water supply capable of delivering the required fire flow shall be provided on the public street or on the site of the premises or both to be protected as required and approved by the Chief.
- Additional Requirements: There may be further conditions applied to this project at a later date.

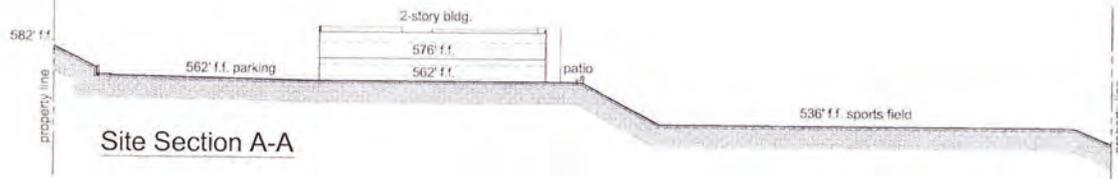
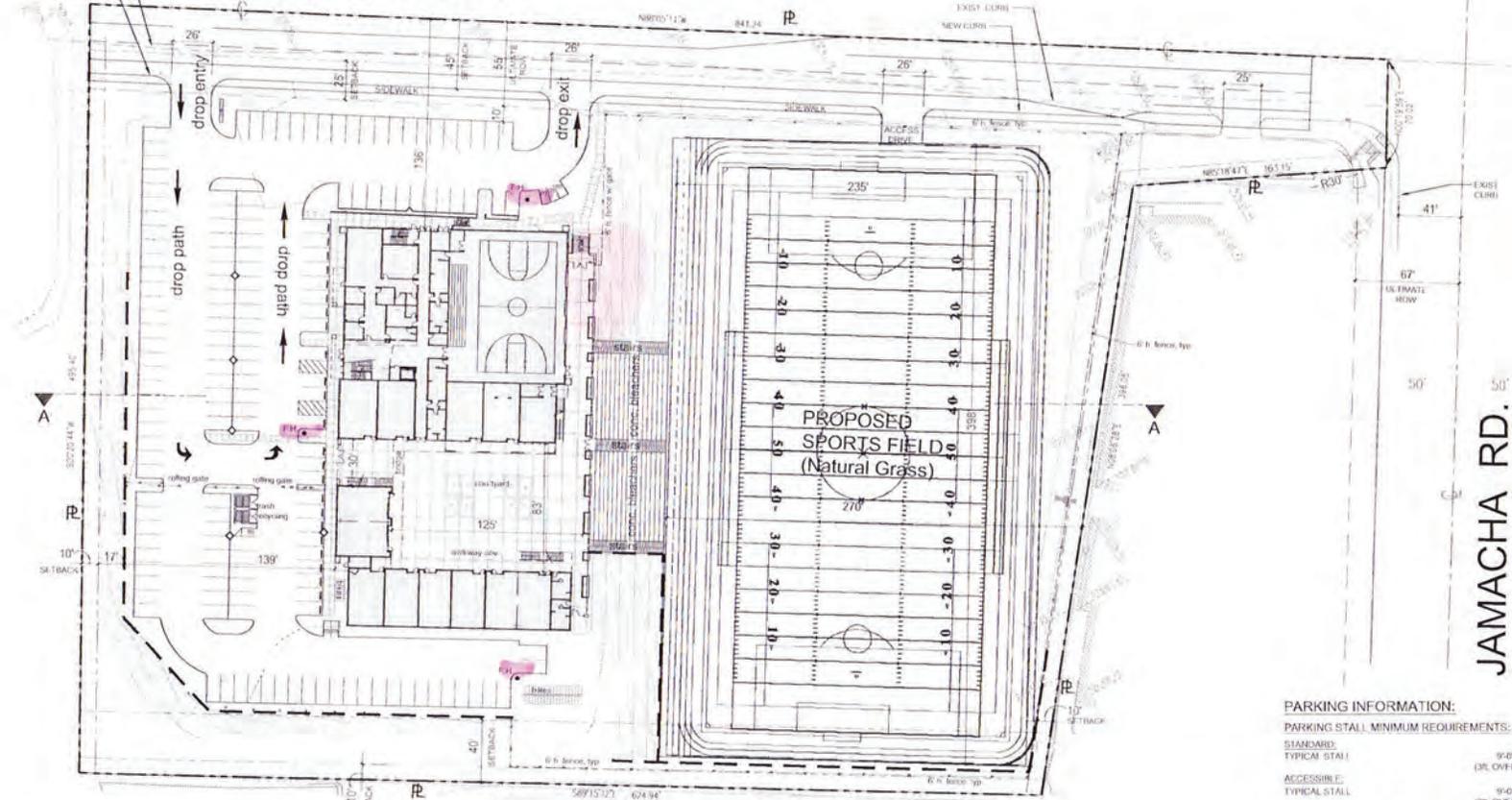
If you have any questions, please contact me at 619-660-5356.

Sincerely,

A handwritten signature in blue ink that reads "Tony Morgan". The signature is written in a cursive, flowing style.

Tony Morgan
Deputy Fire Marshal

CHASE AVE.



Site Section A-A



LIBERTY - LITERACY 1st.
CONCEPTUAL SITE PLAN

SCALE: 1"=30'-0"



PARKING INFORMATION:

PARKING STALL MINIMUM REQUIREMENTS:

STANDARD TYPICAL STALL	9'-0" WIDE x 18'-0" LONG (3% OVERHANG PERMITTED)
ACCESSIBLE TYPICAL STALL	9'-0" WIDE x 18'-0" LONG (2% OVERHANG PERMITTED)
ACCI-SG ABL L (NON-VAN)	5'-0" WIDE x 18'-0" LONG
ACCI-SG ABL L (VAN)	8'-0" WIDE x 18'-0" LONG

PARKING REQUIRED PER COUNTY OF SD:

1 - SPACE PER EACH EMPLOYEE	33 SPACES
0.25 - SPACES PER EACH STUDENT	450 STUDENTS = 113 SPACES
15 - SPACES FOR VISITORS	15 SPACES
TOTAL SPACES REQUIRED	161 SPACES

PARKING PROVIDED:

STANDARD ACCESSIBLE	157 SPACES (1 STANDARD + 1 VAN) = 4 SPACES
TOTAL PROVIDED	161 SPACES

MOTORCYCLE PARKING PROVIDED = 6 SPACES

BICYCLE PARKING PROVIDED (0.1 PER STUDENT) = 45 BIKE SPACES

SITE LIGHTING NOTE

ALL OUTDOOR LIGHTING FIXTURES SHALL BE SHADED ON TOP SO THAT ALL LIGHT IS DIRECTED DOWNWARD.



1405

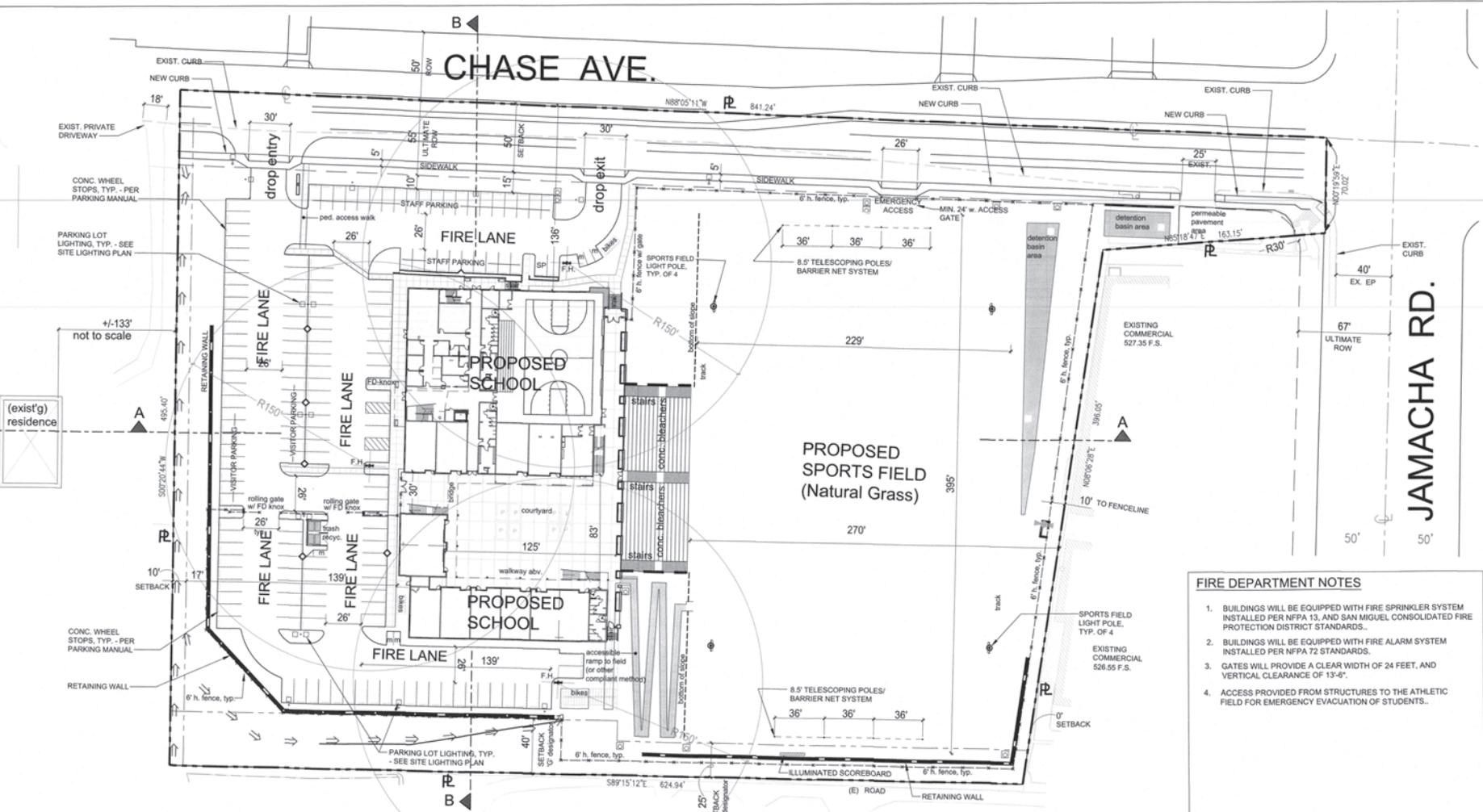
DATE ISSUED	PROJECT
04/15/15	LIBERTY 1ST
04/16/15	LIBERTY 1ST
04/17/15	LIBERTY 1ST

CONCEPTUAL PLAN
THIS PLAN IS A CONCEPTUAL SITE PLAN AND IS NOT TO BE USED FOR PERMITS OR CONTRACTS. IT IS SUBJECT TO CHANGE WITHOUT NOTICE. THE CLIENT ACCEPTS ALL RISKS AND LIABILITIES ASSOCIATED WITH THIS PLAN. THE ARCHITECT ASSUMES NO LIABILITY FOR ANY ERRORS OR OMISSIONS IN THIS PLAN.



Project No. 15-135
Sheet No. SITE PLAN

AS1.0



- FIRE DEPARTMENT NOTES**
1. BUILDINGS WILL BE EQUIPPED WITH FIRE SPRINKLER SYSTEM INSTALLED PER NFPA 13, AND SAN MIGUEL CONSOLIDATED FIRE PROTECTION DISTRICT STANDARDS.
 2. BUILDINGS WILL BE EQUIPPED WITH FIRE ALARM SYSTEM INSTALLED PER NFPA 72 STANDARDS.
 3. GATES WILL PROVIDE A CLEAR WIDTH OF 24 FEET, AND VERTICAL CLEARANCE OF 13'-0".
 4. ACCESS PROVIDED FROM STRUCTURES TO THE ATHLETIC FIELD FOR EMERGENCY EVACUATION OF STUDENTS.

FIRE MARSHAL
 San Miguel Consolidated Fire Protection District
 2002 Via Ortega Way, Eureka Valley, CA 95928
 (916) 870-0000

APPROVED

Subject to field inspection and required tests, including before, during, and after construction, and conforming with applicable regulations. The approval of these plans does not constitute a guarantee of the accuracy of the information provided hereon, and the contractor shall be held responsible for the accuracy of the information provided hereon.

7-30-19 *[Signature]*

CERTAIN FIRE CHECK APPROVAL PRIOR TO THE FOLLOWING INSPECTIONS:
 Foundation & Footing
 Framing
 Mechanical & Electrical

It is unlawful to make any changes or alterations on this set of plans without the approval of the Fire Marshal.



Date Issued	Remarks
06/15/15	CLIENT REVIEW
06/30/15	CLIENT REVIEW
09/11/15	M.P. RE-SUBMITAL
04/08/16	M.P. RE-SUBMITAL
02/10/17	M.P. RE-SUBMITAL
07/21/17	M.P. RE-SUBMITAL
02/08/18	M.P. RE-SUBMITAL
07/18/19	FD SITE PLAN EXHIBIT

CONCEPTUAL DRAWINGS

THIS DRAWING IS FOR CONCEPTUAL PURPOSES ONLY. IT IS NOT TO BE USED FOR PERMITS, CONTRACTS, OR ANY OTHER LEGAL PURPOSES. THE CLIENT AND CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS. ANY CHANGES TO THIS DRAWING SHALL BE MADE AT THE CLIENT'S RISK.

Proposed Project:
LIBERTY CHARTER HIGH SCHOOL
 - MAJOR USE PERMIT -



PCA Project No. 15-135

Sheet Title:
SITE PLAN FD EXHIBIT

Sheet No.
AS.FD

LIBERTY - LITERACY 1st.
CONCEPTUAL SITE PLAN-FD EXHIBIT
 SCALE: 1"=30'-0"



1 - 406

**Attachment G –
OWNERSHIP DISCLOSURE**



County of San Diego, Planning & Development Services
APPLICANT'S DISCLOSURE OF OWNERSHIP INTERESTS ON APPLICATION FOR ZONING PERMITS/ APPROVALS
ZONING DIVISION

Record ID(s) _____

Assessor's Parcel Number(s) _____

Ordinance No. 4544 (N.S.) requires that the following information must be disclosed at the time of filing of this discretionary permit. The application shall be signed by all owners of the property subject to the application or the authorized agent(s) of the owner(s), pursuant to Section 7017 of the Zoning Ordinance. NOTE: Attach additional pages if necessary.

A. List the names of all persons having any ownership interest in the property involved.

Blank lines for listing names of persons with ownership interest.

B. If any person identified pursuant to (A) above is a corporation or partnership, list the names of all individuals owning more than 10% of the shares in the corporation or owning any partnership interest in the partnership.

Blank lines for listing names of individuals with significant ownership in corporations or partnerships.

C. If any person identified pursuant to (A) above is a non-profit organization or a trust, list the names of any persons serving as director of the non-profit organization or as trustee or beneficiary or trustor of the trust.

Blank lines for listing names of persons serving in non-profit organizations or trusts.

NOTE: Section 1127 of The Zoning Ordinance defines Person as: "Any individual, firm, copartnership, joint venture, association, social club, fraternal organization, corporation, estate, trust, receiver syndicate, this and any other county, city and county, city, municipality, district or other political subdivision, or any other group or combination acting as a unit."

Handwritten signature of the applicant.

Print Name

Date

